



## **Town of Arlington, MA Redevelopment Board**

### **Agenda & Meeting Notice February 3, 2020**

The Arlington Redevelopment Board will meet Monday, February 3, 2020 at 7:30 PM in the **Town Hall Annex, 2nd Floor Conference Room, 730 Massachusetts Avenue, Arlington, MA 02476**

#### **1. Environmental Design Review Public Hearing**

7:30 p.m. - Board will open Special Permit Docket #3616 in accordance with the  
8:00 p.m. provisions of MGL Chapter 40A § 11, and the Town of Arlington Zoning Bylaw Section 3.4, Environmental Design Review. The applicant, Andy Liam, Taipei-Tokyo, at **434 Massachusetts Avenue**, seeks approval of signage that is already installed. The opening of the Special Permit is to allow the Board to review and approve the signage under Section 6.2, Signs.

- Applicant will be provided 10 minutes for updates from prior public hearing session.
- DPCD staff will be provided 5 minutes for updates.
- Members of the public will be provided time to comment.
- Board members will discuss docket and may vote.

#### **2. Environmental Design Review Public Hearing**

8:00 p.m. - Board will reopen Special Permit Docket #3504 in accordance with the  
8:30 p.m. provisions of MGL Chapter 40A § 11, and the Town of Arlington Zoning Bylaw Section 3.4, Environmental Design Review. The applicant, Kevin Flynn for Springboard Schools, Inc. at **93 Broadway**, Arlington, MA, seeks approval of an addition to the school on the adjacent vacant lot, and a request to amend the parking reduction under Section 6.1.5.

- Applicant will be provided 10 minutes for updates from prior public hearing session.
- DPCD staff will be provided 5 minutes for updates.
- Members of the public will be provided time to comment.
- Board members will discuss docket and may vote.

#### **3. Annual election of chair and vice-chair**

8:30 p.m. - • Board members will vote on positions  
8:35 p.m.

#### **4. Debrief and follow-up from joint meeting with Select Board on January 13, 2020**

8:35 p.m. - • Board will debrief meeting and discuss next steps.

8:45 p.m.

**5. Proposed Town Committee on Residential Development**

8:45 p.m. - • Board will discuss and may wish to submit correspondence to Select  
9:00 p.m. Board regarding proposal

**6. Selection of additional ARB meeting dates for Warrant Article public hearings**

9:00 p.m. - • Board and staff will discuss and select potential dates  
9:05 p.m.

**7. Lease Extension for Retirement Board**

9:05 p.m. - • Board members will review and may approve lease extension  
9:15 p.m.

**8. Final Broadway Corridor Report**

9:15 p.m. - • Staff will provide final report  
9:20 p.m.

**9. Meeting Minutes (12/2/19)**

9:20 p.m. - • Board members will review and may approve minutes  
9:25 p.m.

**10 Open Forum**

9:25 p.m. - • Except in unusual circumstances, any matter presented for  
9:45 p.m. consideration of the Board shall neither be acted upon, nor a decision  
made the night of the presentation. There is a three minute time limit to  
present a concern or request.

**11 Adjourn**

Estimated 9:45 p.m. – Adjourn

**12 Correspondence received:**

Correspondence received from:  
Don Seltzer (1/24/20)  
Barbara Thornton (1/27/20)  
John Worden (1/15/20)  
Christopher Loreti (1/27/20)



## Town of Arlington, Massachusetts

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### Environmental Design Review Public Hearing

#### Summary:

7:30 p.m. -  
8:00 p.m.

Board will open Special Permit Docket #3616 in accordance with the provisions of MGL Chapter 40A § 11, and the Town of Arlington Zoning Bylaw Section 3.4, Environmental Design Review. The applicant, Andy Liam, Taipei-Tokyo, at **434 Massachusetts Avenue**, seeks approval of signage that is already installed. The opening of the Special Permit is to allow the Board to review and approve the signage under Section 6.2, Signs.

- Applicant will be provided 10 minutes for updates from prior public hearing session.
- DPCD staff will be provided 5 minutes for updates.
- Members of the public will be provided time to comment.
- Board members will discuss docket and may vote.

#### ATTACHMENTS:

Type	File Name	Description
▢ Reference Material	Docket_#3616_434_Mass_Ave_Taipei_Tokyo_application_reduced.pdf	Docket #3616 434 Mass Ave. Taipei Tokyo application
▢ Reference Material	Agenda_Item_2_-_EDR_Public_Hearing_Memo_Docket_#3616_434_Mass_Ave_final.pdf	EDR Public Hearing Memo Docket #3616 434 Mass.-Final Ave.



TOWN OF ARLINGTON  
REDEVELOPMENT BOARD

Application for Special Permit In Accordance with Environmental Design  
Review Procedures (Section 3.4 of the Zoning Bylaw)

- Docket No. \_\_\_\_\_
- Property Address 434 MASSACHUSETTS AVE, Arlington  
Name of Record Owner(s) ARA K. Gechjian Phone 781-910-0822  
Address of Owner 432 MASS AVE., ARLINGTON, MA. (CAMERA)  
Street City, State, Zip 810P
  - Name of Applicant(s) (if different than above) ANDY LIAM  
Address 434 MASSACHUSETTS AVE Phone 781-267-5472  
Status Relative to Property (occupant, purchaser, etc.) \_\_\_\_\_
  - Location of Property 434 MASSACHUSETTS AVE.  
Assessor's Block Plan, Block, Lot No. \_\_\_\_\_
  - Deed recorded in the Registry of deeds, Book \_\_\_\_\_, Page \_\_\_\_\_;  
-or- registered in Land Registration Office, Cert. No. \_\_\_\_\_, in Book \_\_\_\_\_, Page \_\_\_\_\_.
  - Present Use of Property (include # of dwelling units, if any) Restaurant
  - Proposed Use of Property (include # of dwelling units, if any) RESTAURANT
  - Permit applied for in accordance with 3.4  
the following Zoning Bylaw section(s) 6.2  
section(s) title(s)
  - Please attach a statement that describes your project and provide any additional information that may aid the ARB in understanding the permits you request. Include any reasons that you feel you should be granted the requested permission.

(In the statement below, strike out the words that do not apply)

The applicant states that ANDY LIAM is the owner -or- occupant -or- purchaser under agreement of the property in Arlington located at 434 MASSACHUSETTS AVENUE (TAI-PEI TOKYO) which is the subject of this application; and that unfavorable action -or- no unfavorable action has been taken by the Zoning Board of Appeals on a similar application regarding this property within the last two years. The applicant expressly agrees to comply with any and all conditions and qualifications imposed upon this permission, either by the Zoning Bylaw or by the Redevelopment Board, should the permit be granted.

[Signature]  
Signature of Applicant(s)

434 MASSACHUSETTS AVE  
Address Arlington, Ma 02474

781-267-5472  
Phone



TOWN OF ARLINGTON  
REDEVELOPMENT BOARD

Petition for Special Permit under Environmental Design Review (see Section 3.4 of the  
Arlington Zoning Bylaw for Applicability)

For projects subject to Environmental Design Review, (see Section 3.4), please submit a statement that completely describes your proposal, and addresses each of the following standards.

- N/A
1. Preservation of Landscape. The landscape shall be preserved in its natural state, insofar as practicable, by minimizing tree and soil removal, and any grade changes shall be in keeping with the general appearance of neighboring developed areas.
  2. Relation of Buildings to Environment. Proposed development shall be related harmoniously to the terrain and to the use, scale, and architecture of existing buildings in the vicinity that have functional or visual relationship to the proposed buildings. The Arlington Redevelopment Board may require a modification in massing to reduce the effect of shadows on abutting property in an R0, R1 or R2 district or on public open space.
  3. Open Space. All open space (landscaped and usable) shall be so designed as to add to the visual amenities of the vicinity by maximizing its visibility for persons passing the site or overlooking it from nearby properties. The location and configuration of usable open space shall be so designed as to encourage social interaction, maximize its utility, and facilitate maintenance.
  4. Circulation. With respect to vehicular, pedestrian and bicycle circulation, including entrances, ramps, walkways, drives, and parking, special attention shall be given to location and number of access points to the public streets (especially in relation to existing traffic controls and mass transit facilities), width of interior drives and access points, general interior circulation, separation of pedestrian and vehicular traffic, access to community facilities, and arrangement of vehicle parking and bicycle parking areas, including bicycle parking spaces required by Section 8.13 that are safe and convenient and, insofar as practicable, do not detract from the use and enjoyment of proposed buildings and structures and the neighboring properties.
  5. Surface Water Drainage. Special attention shall be given to proper site surface drainage so that removal of surface waters will not adversely affect neighboring properties or the public storm drainage system. Available Best Management Practices for the site should be employed, and include site planning to minimize impervious surface and reduce clearing and re-grading. Best Management Practices may include erosion control and storm water treatment by means of swales, filters, plantings, roof gardens, native vegetation, and leaching catch basins. Storm water should be treated at least minimally on the development site; that which cannot be handled on site shall be removed from all roofs, canopies, paved and pooling areas and carried away in an underground drainage system. Surface water in all paved areas shall be collected at intervals so that it will not obstruct the flow of vehicular or pedestrian traffic, and will not create puddles in the paved areas.
- N/A
- In accordance with Section 3.3.4, the Board may require from any applicant, after consultation with the Director of Public Works, security satisfactory to the Board to insure the maintenance of all storm water facilities such as catch basins, leaching catch basins, detention basins, swales, etc. within the site. The Board may use funds provided by such security to conduct maintenance that the applicant fails to do. The Board may adjust in its sole discretion the amount and type of financial security such that it is satisfied that the amount is sufficient to provide for the future maintenance needs.
- N/A
6. Utility Service. Electric, telephone, cable TV and other such lines and equipment shall be underground. The proposed method of sanitary sewage disposal and solid waste disposal from all buildings shall be indicated.
  7. Advertising Features. The size, location, design, color, texture, lighting and materials of all permanent signs and outdoor advertising structures or features shall not detract from the use and enjoyment of proposed buildings and structures and the surrounding properties. Advertising features are subject to the provisions of Section 6.2 of the Zoning Bylaw.

(SEE ATTACHED)

# TOWN OF ARLINGTON

Dimensional and Parking Information  
for Application to  
The Arlington Redevelopment Board

Docket No. \_\_\_\_\_

Property Location 434 MASS AVE.

Zoning District \_\_\_\_\_

Owner: ARA K. GECHIJIAN

Address: 434 MASS AVE

Present Use/Occupancy: No. of Dwelling Units:

Uses and their gross square feet:

RESTAURANT

Proposed Use/Occupancy: No. of Dwelling Units:

Uses and their gross square feet:

RESTAURANT

	Present Conditions	Proposed Conditions	Min. or Max. Required by Zoning for Proposed Use
Lot Size	9,131		min.
Frontage	52'		min.
Floor Area Ratio			max.
Lot Coverage (%), where applicable			max.
Lot Area per Dwelling Unit (square feet)			min.
Front Yard Depth (feet)			min.
Side Yard Width (feet) right side			min.
left side			min.
Rear Yard Depth (feet)			min.
Height			min.
Stories	15'		stories
Feet			feet
Open Space (% of G.F.A.)			min.
Landscaped (square feet)			(s.f.)
Usable (square feet)			(s.f.)
Parking Spaces (No.)			min.
Parking Area Setbacks (feet), where applicable			min.
Loading Spaces (No.)			min.
Type of Construction			
Distance to Nearest Building			min.



## Appendix 2

Construction Documents are required for structures that must comply with 780 CMR 107. The checklist below is a compilation of the documents that may be required for this. The applicant shall fill out the checklist and provide the contact information of the registered professionals responsible for the documents. This appendix is to be submitted with the building permit application.

### Checklist for Construction Documents\*

No.	Item	Mark "x" where applicable		
		Submitted	Incomplete	Not Required
1	Architectural			
2	Foundation			
3	Structural			
4	Fire Suppression			
5	Fire Alarm (may require repeaters)			
6	HVAC			
7	Electrical			
8	Plumbing (include local connections)			
9	Gas (Natural, Propane, Medical or other)			
10	Surveyed Site Plan (Utilities, Wetland, etc.)			
11	Specifications			
12	Structural Peer Review			
13	Structural Tests & Inspections Program			
14	Fire Protection Narrative Report			
15	Existing Building Survey/Investigation			
16	Energy Conservation Report			
17	Architectural Access Review (521 CMR)			
18	Workers Compensation Insurance			
19	Hazardous Material Mitigation Documentation			
20	Other (Specify)			
21	Other (Specify)			
22	Other (Specify)			

\*Areas of Design or Construction for which plans are not complete at the time of application submittal must be identified herein. Work so identified must not be commenced until this application has been amended and the proposed construction document amendment has been approved by the authority having jurisdiction. Work started prior to approval may be subjected to *triple the original permit fee*.

### Registered Professional Contact Information

Name (Registrant)	Telephone No.	e-mail address	Registration Number
Street Address	City/Town	State    Zip	Discipline    Expiration Date
Name (Registrant)	Telephone No.	e-mail address	Registration Number
Street Address	City/Town	State    Zip	Discipline    Expiration Date
Name (Registrant)	Telephone No.	e-mail address	Registration Number
Street Address	City/Town	State    Zip	Discipline    Expiration Date

Vital Signs  
106 Massachusetts Ave.  
Arlington, MA 02474  
(781) 643-6327

[acarter@town.arlington.ma.us](mailto:acarter@town.arlington.ma.us)

To Whom It May Concern:

The signage is already up for TAIPEI-TOKYO, 434 Mass Ave, Arlington, Massachusetts, but will need the Arlington Historical Commission to review the application.

I have attached before-and-after photos of 434 Mass Ave signage.

Sincerely,

Charles Scacca,  
Vital Signs

## FABRICATION DESCRIPTION

ALL DISHPAN SIGNS ARE FABRICATED WITH 1"x1" TUBING (FRAME) OR TUBED FRAME. - .040 ALUMINUM BLACK METAL FACE W/ RIVETS INSTALLED TO FRAME AND FACE - WHEN FINISHED A .75" GOLD METALLIC MOLDING IS INSTALLED AROUND SIGN AS A FRAME. (SEE ATTACHED PICTURE.)

## INSTALLATION:

1"x3" - STAINLESS "L" BRACKETS SUPPORT THE SIGN, BOTH TOP & BOTTOM 6x PER SIGN AND 1.5" STAINLESS SELF DRILLING SCREWS ARE INSTALLED BOTH IN THE SIGN AND BUILDING FACADE.





# The Commonwealth of Massachusetts

## Department of Public Safety

Massachusetts State Building Code (780 CMR)

### Building Permit Application for any Building other than a One- or Two-Family Dwelling

(This Section For Official Use Only)

Building Permit Number: \_\_\_\_\_ Date Applied: \_\_\_\_\_ Building Official: \_\_\_\_\_

#### SECTION 1: LOCATION (Please indicate Block # and Lot # for locations for which a street address is not available)

434 ARLINGTON, MA. 02474  
No. and Street City /Town Zip Code Name of Building (if applicable)

#### SECTION 2: PROPOSED WORK

Edition of MA State Code used \_\_\_\_\_ If New Construction check here ☐ or check all that apply in the two rows below

Existing Building ☐ Repair ☐ Alteration ☐ Addition ☐ Demolition ☐ (Please fill out and submit Appendix 1)

Change of Use ☐ Change of Occupancy ☐ Other ☐ Specify: Sign

Are building plans and/or construction documents being supplied as part of this permit application? Yes ☐ No ☐

Is an Independent Structural Engineering Peer Review required? Yes ☐ No ☐

Brief Description of Proposed Work: 1-3'x 20' SIGN TO FACE W/ BLACK DIBOND  
FACE - RAISED 1/2" TEXT - 3'x12' - 2x REFACE ALSO 3'x12'  
RAISED TEXT. (will provide drawing)

#### SECTION 3: COMPLETE THIS SECTION IF EXISTING BUILDING UNDERGOING RENOVATION, ADDITION, OR CHANGE IN USE OR OCCUPANCY

Check here if an Existing Building Investigation and Evaluation is enclosed (See 780 CMR 34) ☐

Existing Use Group(s): \_\_\_\_\_ Proposed Use Group(s): \_\_\_\_\_

#### SECTION 4: BUILDING HEIGHT AND AREA

	Existing	Proposed
No. of Floors/Stories (include basement levels) & Area Per Floor (sq. ft.)		
Total Area (sq. ft.) and Total Height (ft.)		

#### SECTION 5: USE GROUP (Check as applicable)

A: Assembly A-1 ☐ A-2 ☐ Nightclub ☐ A-3 ☐ A-4 ☐ A-5 ☐ B: Business ☐ E: Educational ☐

F: Factory F-1 ☐ F-2 ☐ H: High Hazard H-1 ☐ H-2 ☐ H-3 ☐ H-4 ☐ H-5 ☐

I: Institutional I-1 ☐ I-2 ☐ I-3 ☐ I-4 ☐ M: Mercantile ☐ R: Residential R-1 ☐ R-2 ☐ R-3 ☐ R-4 ☐

S: Storage S-1 ☐ S-2 ☐ U: Utility ☐ Special Use ☐ and please describe below:

Special Use: \_\_\_\_\_

#### SECTION 6: CONSTRUCTION TYPE (Check as applicable)

IA ☐ IB ☐ IIA ☐ IIB ☐ IIIA ☐ IIIB ☐ IV ☐ VA ☐ VB ☐

#### SECTION 7: SITE INFORMATION (refer to 780 CMR 111.0 for details on each item)

<b>Water Supply:</b> Public <input type="checkbox"/> Private <input type="checkbox"/>	<b>Flood Zone Information:</b> Check if outside Flood Zone <input type="checkbox"/> or identify Zone: _____	<b>Sewage Disposal:</b> Indicate municipal <input type="checkbox"/> or on site system <input type="checkbox"/>	<b>Trench Permit:</b> A trench will not be required <input type="checkbox"/> or trench permit is enclosed <input type="checkbox"/>	<b>Debris Removal:</b> Licensed Disposal Site <input type="checkbox"/> or specify: _____
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**Railroad right-of-way:**  
Not Applicable ☐  
or Consent to Build enclosed ☐

**Hazards to Air Navigation:**  
Is Structure within airport approach area?  
Yes ☐ or No ☐

[MA Historic Commission Review Process:](#)  
Is their review completed?  
Yes ☐ No ☐

#### SECTION 8: CONTENT OF CERTIFICATE OF OCCUPANCY

Edition of Code: \_\_\_\_\_ Use Group(s): \_\_\_\_\_ Type of Construction: \_\_\_\_\_ Occupant Load per Floor: \_\_\_\_\_

Does the building contain an Sprinkler System?: \_\_\_\_\_ Special Stipulations: \_\_\_\_\_



①

SECTION 9: PROPERTY OWNER AUTHORIZATION

Name and Address of Property Owner

Ara K. Gechijian 434 MASS. AVE Arlington, MA. 02474  
Name (Print) No. and Street City/Town Zip

Property Owner Contact Information:

Landlord 781-910-0922 \_\_\_\_\_  
Title Telephone No. (business) Telephone No. (cell) e-mail address

If applicable, the property owner hereby authorizes

Andy Lian 434 mass ave Arlington, MA 02474  
Name Street Address City/Town State Zip

to act on the property owner's behalf, in all matters relative to work authorized by this building permit application.

SECTION 10: CONSTRUCTION CONTROL (Please fill out Appendix 2)

(If building is less than 35,000 cu. ft. of enclosed space and/or not under Construction Control then check here ☐ and skip Section 10.1)

10.1 Registered Professional Responsible for Construction Control

Name (Registrant)	Telephone No.	e-mail address	Registration Number
Street Address	City/Town	State Zip	Discipline Expiration Date

10.2 General Contractor

VITAL SIGNS  
Company Name  
106 MASS AVE NONE  
Name of Person Responsible for Construction License No. and Type if Applicable  
CHARLES SCALIA ARLINGTON MA. 02474  
Street Address City/Town State Zip  
781-643 6327 781-462 5208 RADENCO@Juno.com  
Telephone No. (business) Telephone No. (cell) e-mail address

SECTION 11: WORKERS' COMPENSATION INSURANCE AFFIDAVIT (M.G.L. c. 152, § 25C(6))

A Workers' Compensation Insurance Affidavit from the MA Department of Industrial Accidents must be completed and submitted with this application. Failure to provide this affidavit will result in the denial of the issuance of the building permit.

Is a signed Affidavit submitted with this application? Yes ☐ No ☐

SECTION 12: CONSTRUCTION COSTS AND PERMIT FEE

Item	Estimated Costs: (Labor and Materials)	Total Construction Cost (from Item 6) = \$ _____
1. Building (SIGNS)	\$ _____	Building Permit Fee = Total Construction Cost x _____ (Insert here appropriate municipal factor) = \$ _____.
2. Electrical	\$ _____	
3. Plumbing	\$ _____	
4. Mechanical (HVAC)	\$ _____	
5. Mechanical (Other)	\$ _____	
6. Total Cost	\$ <u>3,500</u>	Note: Minimum fee = \$ _____ (contact municipality)
		Enclose check payable to _____ (contact municipality) and write check number here _____

SECTION 13: SIGNATURE OF BUILDING PERMIT APPLICANT

By entering my name below, I hereby attest under the pains and penalties of perjury that all of the information contained in this application is true and accurate to the best of my knowledge and understanding.

Andy Lian owner 781-267-5472 10/1/19  
Please print and sign name Title Telephone No. Date  
434 MASS. AVE. Arlington MA. 02474  
Street Address City/Town State Zip

Municipal Inspector to fill out this section upon application approval: \_\_\_\_\_  
Name Date



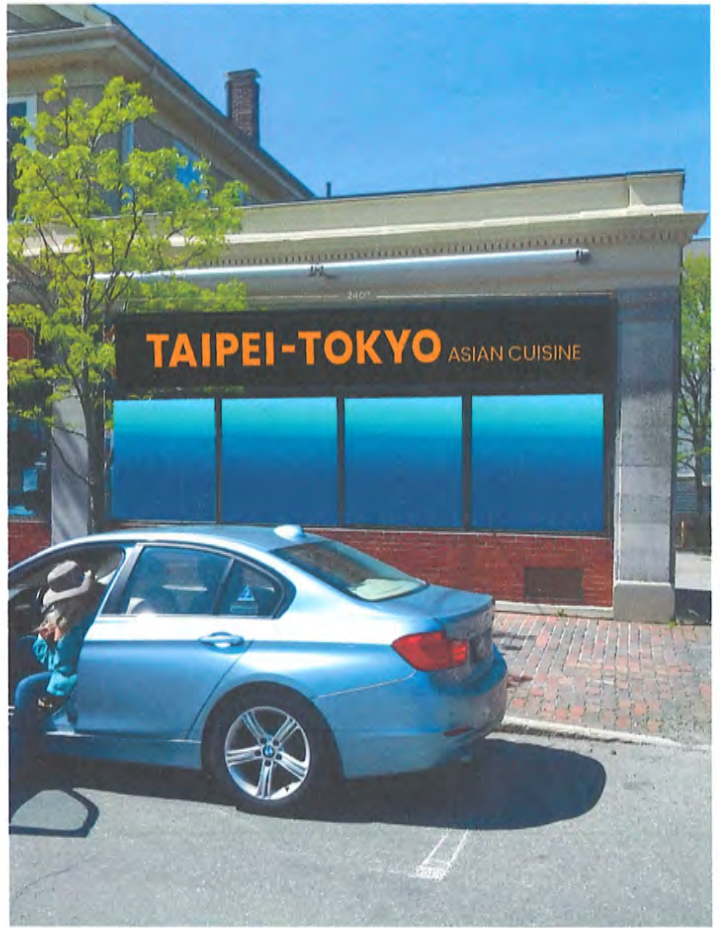
# TAI-PEI TOKYO SIGNS

Requires (2) Pictures Side by Side

## DISHPAN or BELT SIGN

1" x 1" - Aluminum Tubed Frame - .040 Aluminum Black Metal Factory Finish Face - 1" x .75" - gold Metallic Molding Frame

**Installation:** 1" x 3" Stainless Steel "L" Brackets (6) per sign - Big Sign (8) per sign - 2.5" Stainless Self-Drilling Screws.





# TAI-PEI TOKYO SIGNS

434 Massachusetts Avenue

37.5" x 146"

CHINESE FOOD ... 1/2" Exterior Komocel - Gold Metallic Letter Fastened to Black Aluminum DishPan Sign.

**CHINESE FOOD**  
**SUSHI BAR & LOUNGE**

Dining & Take-Out

781-646-6897

37.5" x 146"

TEMPLE Logo - 1/2" Exterior Komocel - Gold Metallic Letter Fastened to Black Aluminum DishPan Sign.



37.5" x 146"

TAIPEI-TOKYO - 1/2" Exterior Komocel - Gold Metallic Letter Fastened to Black Aluminum DishPan Sign.

**TAIPEI-TOKYO** ASIAN CUISINE

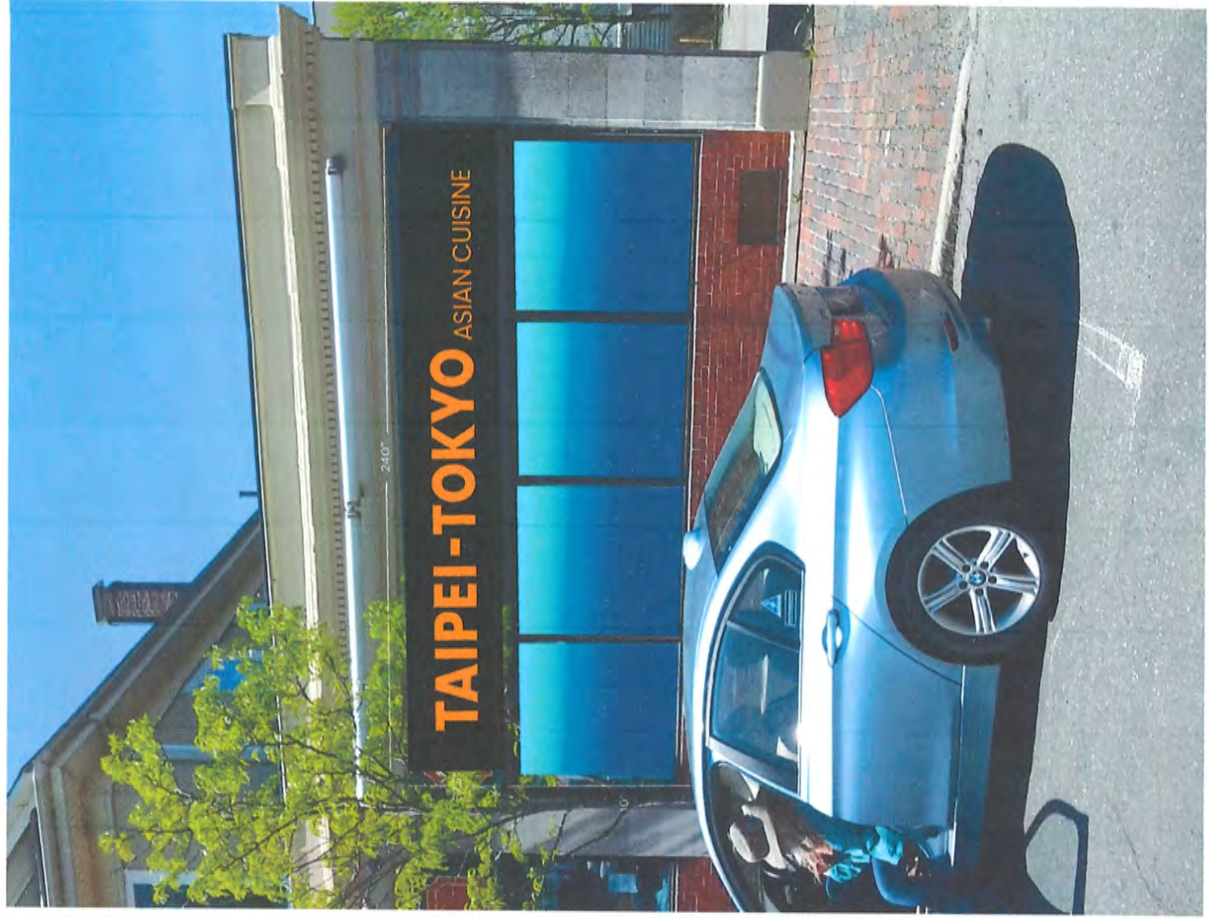


# TAI-PEI TOKYO SIGNS

Requires (2) Pictures Side by Side

**DISHPAN or BELT SIGN**

1" x 1" - Aluminum Tubed Frame - .040 Aluminum Black Metal Factory Finish Face - 1" x .75" - gold Metallic Molding Frame  
**Installation:** 1" x 3" Stainless Steel "L" Brackets (6) per sign - Big Sign (8) per sign - 2.5" Stainless Self-Drilling Screws.





# TAI-PEI TOKYO SIGNS

434 Massachusetts Avenue

37.5" x 146"

CHINESE FOOD - 1/2" Exterior Komocel - Gold Metallic Letter Fastened to Black Aluminum DishPan Sign.



37.5" x 146"

TEMPLE Logo - 1/2" Exterior Komocel - Gold Metallic Letter Fastened to Black Aluminum DishPan Sign.



37.5" x 146"

TAIPEI-TOKYO - 1/2" Exterior Komocel - Gold Metallic Letter Fastened to Black Aluminum DishPan Sign.



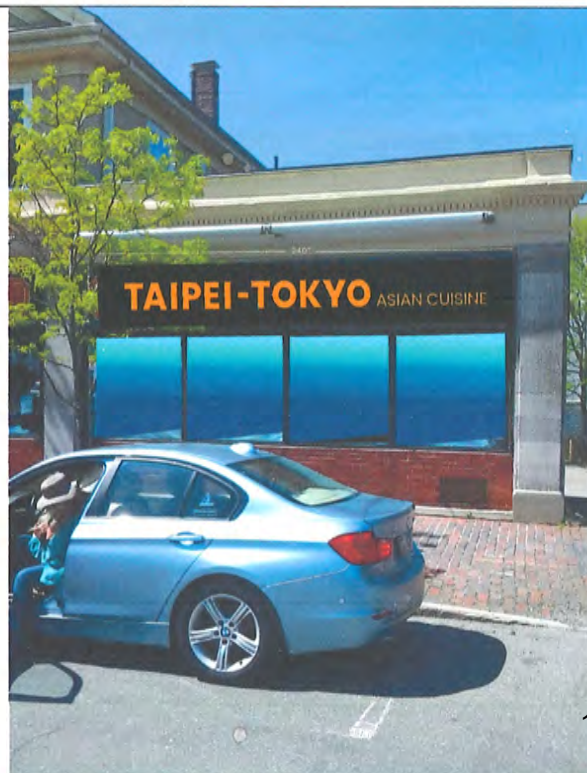
# TAI-PEI TOKYO SIGNS

434 Massachusetts Avenue

## DISHPAN or BELT SIGN (Requires (2) Pictures Side by Side)

1" x 1" - Aluminum Tubed Frame - .040 Aluminum Black Metal Factory Finish Face - 1" x .75" - gold Metallic Molding Frame

Installation: 1" x 3" Stainless Steel "L" Brackets (6) per sign - Big Sign (8) per sign - 2.5" Stainless Self-Drilling Screws.





# EXISTING SIGNS

434 Massachusetts Avenue









**Town of Arlington, Massachusetts**  
Department of Planning & Community Development  
730 Massachusetts Avenue, Arlington, Massachusetts 02476

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**Public Hearing Memorandum**

*The purpose of this memorandum is to provide the Arlington Redevelopment Board and public with technical information and a planning analysis to assist with the regulatory decision-making process.*

**To:** Arlington Redevelopment Board

**From:** Jennifer Raitt, Secretary Ex-Officio

**Subject:** Environmental Design Review, 434 Massachusetts Avenue, Arlington, MA  
Docket #3616

**Date:** January 27, 2020

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**I. Docket Summary**

This is an application by Andy Liam for Taipei-Tokyo, at 434 Massachusetts Avenue, Arlington, MA, 02476, for Special Permit Docket #3616 in accordance with the provisions of MGL Chapter 40A § 11, and the Town of Arlington Zoning Bylaw Section 3.4, Environmental Design Review. The applicant seeks approval of signage that is already installed. The opening of the Special Permit is to allow the Board to review and approve the signage under Section 6.2, Signs.

In this case, the applicant has already installed the signage at the restaurant, but exceeds the amount of signage that is allowed in Section 6.2, Signs, of the Zoning Bylaw. The regulations allow an applicant to seek a special permit to exceed the number of signs and size allowed by right.

Taipei-Tokyo was renamed Shanghai Village according to an updated business certificate filed with the Clerk's office on July 1, 2019 and a hearing with the Select Board to update their all alcohol license to reflect the name change on August 12, 2019. Shanghai Village had been operating in this location since 1991 until a fire damaged the restaurant in 2018. The use has been in the building since 1965. The proposed signage is a one-for-one replacement of signage that was previously in place at Shanghai Village since 1991. Two of

the proposed signs, which have already been installed, are smaller than the three 47.69 square foot signs that preceded them.

During the late summer of 2019, while owner Andy Lam was working to reopen his restaurant to the public, it came to the attention of the Department that the new signage had already been installed without a permit. Staff worked in conjunction with Lam, his contractor Vital Signs, and staff at the Inspectional Services Department to assure that they were allowed to reopen their restaurant only if they applied for a special permit with the Arlington Redevelopment Board.

Materials submitted for consideration of this application:

- Application for EDR Special Permit,
- Renderings of signage, and
- Photographs of installed signage.

## II. Application of Special Permit Criteria (Arlington Zoning Bylaw, Section 3.3)

### 1. Section 3.3.3.A.

**The use requested is listed as a Special Permit in the use regulations for the applicable district or is so designated elsewhere in this Bylaw.**

A restaurant is allowed in the B5 Central Business District Zoning District. The Board can find that this condition is met.

### 2. Section 3.3.3.B.

**The requested use is essential or desirable to the public convenience or welfare.**

A restaurant has operated in this location for many years, and is appropriately located in a major commercial district. The Board can find that this condition is met.

### 3. Section 3.3.3.C.

**The requested use will not create undue traffic congestion or unduly impair pedestrian safety.**

There are no exterior alterations proposed other than signage. The Board can find that this condition is met.

### 4. Section 3.3.3.D.

**The requested use will not overload any public water, drainage or sewer system or any other municipal system to such an extent that the requested use or any developed use in the immediate area or in any other area of the Town will be unduly subjected to hazards affecting health, safety, or the general welfare.**

A restaurant has operated in this location for years without overloading any public utilities. The Board can find that this condition is met.

**5. Section 3.3.3.E.**

**Any special regulations for the use as may be provided in the Bylaw are fulfilled.**

No special regulations are applicable to the proposal. The Board can find that this condition is met.

**6. Section 3.3.3.F.**

**The requested use will not impair the integrity or character of the district or adjoining districts, nor be detrimental to the health or welfare.**

The use does not impair the integrity or character of the neighborhood. The Board can find that this condition is met.

**7. Section 3.3.3.G.**

**The requested use will not, by its addition to a neighborhood, cause an excess of the use that could be detrimental to the character of said neighborhood.**

The use will not be in excess or detrimental to the character of the neighborhood. The Board can find that this condition is met.

**III. Environmental Design Review Standards (Arlington Zoning Bylaw, Section 3.4)**

**1. EDR-1 Preservation of Landscape**

**The landscape shall be preserved in its natural state, insofar as practicable, by minimizing tree and soil removal, and any grade changes shall be in keeping with the general appearance of neighboring developed areas.**

There are no changes to the landscape as there are no proposed exterior alterations. The Board can find that this condition is met.

**2. EDR-2 Relation of the Building to the Environment**

**Proposed development shall be related harmoniously to the terrain and to the use, scale, and architecture of the existing buildings in the vicinity that have functional or visible relationship to the proposed buildings. The Arlington Redevelopment Board may require a modification in massing so as to reduce the effect of shadows on the abutting property in an R0, R1 or R2 district or on public open space.**

There are no changes to the exterior of the building other than the new signage. The Board can find that this condition is met.



**3. EDR-3 Open Space**

All open space (landscaped and usable) shall be so designed as to add to the visual amenities of the vicinity by maximizing its visibility for persons passing by the site or overlooking it from nearby properties. The location and configuration of usable open space shall be so designed as to encourage social interaction, maximize its utility and facilitate maintenance.

There are no changes to open space. The Board can find that this condition is met.

**4. EDR-4 Circulation**

With respect to vehicular and pedestrian and bicycle circulation, including entrances, ramps, walkways, drives, and parking, special attention shall be given to location and number of access points to the public streets (especially in relation to existing traffic controls and mass transit facilities), width of interior drives and access points, general interior circulation, separation of pedestrian and vehicular traffic, access to community facilities, and arrangement of vehicle parking and bicycle parking areas, including bicycle parking spaces required by Section 6.1.12 that are safe and convenient and, insofar as practicable, do not detract from the use and enjoyment of proposed buildings and structures and the neighboring properties.

The existing circulation does not change. The Board can find that this condition is met.

**5. EDR-5 Surface Water Drainage**

Special attention shall be given to proper site surface drainage so that removal of surface waters will not adversely affect neighboring properties or the public storm drainage system. Available Best Management Practices for the site should be employed, and include site planning to minimize impervious surface and reduce clearing and re-grading. Best Management Practices may include erosion control and stormwater treatment by means of swales, filters, plantings, roof gardens, native vegetation, and leaching catch basins. Stormwater should be treated at least minimally on the development site; that which cannot be handled on site shall be removed from all roofs, canopies, paved and pooling areas and carried away in an underground drainage system. Surface water in all paved areas shall be collected in intervals so that it will not obstruct the flow of vehicular or pedestrian traffic and will not create puddles in the paved areas.

In accordance with Section 3.3.4., the Board may require from any applicant, after consultation with the Director of Public Works, security satisfactory to the Board to insure the maintenance of all stormwater facilities such as catch basins, leaching catch basins, detention basins, swales, etc. within the site. The Board may use funds provided by such security to conduct maintenance that the applicant fails to do.

**The Board may adjust in its sole discretion the amount and type of financial security such that it is satisfied that the amount is sufficient to provide for any future maintenance needs.**

There will be no changes to the exterior of the building or surface water run-off as a result of this proposal. The Board can find that this condition is met.

**6. EDR-6 Utilities Service**

**Electric, telephone, cable TV, and other such lines of equipment shall be underground. The proposed method of sanitary sewage disposal and solid waste disposal from all buildings shall be indicated.**

There will be no changes to the utility service as a result of this proposal. The Board can find that this condition is met.

**7. EDR-7 Advertising Features**

**The size, location, design, color, texture, lighting and materials of all permanent signs and outdoor advertising structures or features shall not detract from the use and enjoyment of proposed buildings and structures and the surrounding properties.**

Taipei-Tokyo has installed three wall signs on the façade of their restaurant. Two of the wall signs measure 38.02 square feet and the third wall sign measures 60 square feet, which exceeds the maximum allowed in both size and number. As noted in the introduction, the previous restaurant that operated in this space had the same number of signs, at 47.69 square foot per sign.

The signs were installed with L-brackets drilled into the wall of the structure.

The signs are illuminated by pre-existing external lighting, which direct the light downward.

**8. EDR-8 Special Features**

**Exposed storage areas, exposed machinery installations, service areas, truck loading areas, utility buildings and structures, and similar accessory areas and structures shall be subject to such setbacks, screen plantings or other screening methods as shall reasonably be required to prevent their being incongruous with the existing or contemplated environment and the surrounding properties.**

No changes are proposed. The Board can find that this condition is met.

**9. EDR-9 Safety**

**With respect to personal safety, all open and enclosed spaces shall be designed to facilitate building evacuation and maximize accessibility by fire, police and other emergency personnel and equipment. Insofar as practicable, all exterior spaces and**

**interior public and semi-public spaces shall be so designed to minimize the fear and probability of personal harm or injury by increasing the potential surveillance by neighboring residents and passersby of any accident or attempted criminal act.**

No changes are proposed. The Board can find that this condition is met.

**10. EDR-10 Heritage**

**With respect to Arlington's heritage, removal or disruption of historic, traditional or significant uses, structures or architectural elements shall be minimized insofar as practical whether these exist on the site or on adjacent properties.**

The building containing 434 Massachusetts Avenue is listed on the *Inventory of Historically or Architecturally Significant Properties in the Town of Arlington* and is under the jurisdiction of the Arlington Historical Commission. The Historical Commission has not yet scheduled a meeting, but will review the signage.

**11. EDR-11 Microclimate**

**With respect to the localized climatic characteristics of a given area, any development which proposes new structures, new hard surface, ground coverage or the installation of machinery which emits heat, vapor or fumes shall endeavor to minimize insofar as practicable, any adverse impacts on light, air and water resources or on noise and temperature levels of the immediate environment.**

No changes are proposed. The Board can find that this condition is met.

**12. EDR-12 Sustainable Building and Site Design**

**Projects are encouraged to incorporate best practices related to sustainable sites, water efficiency, energy and atmosphere, materials and resources, and indoor environmental quality. Applicants must submit a current Green Building Council Leadership in Energy and Environmental Design (LEED) checklist, appropriate to the type of development, annotated with narrative description that indicates how the LEED performance objectives will be incorporated into the project.**

No changes are proposed. The Board can find that this condition is met.

**IV. Conditions**

1. Any substantial or material deviation during construction from the approved plans and specifications is subject to the written approval of the Arlington Redevelopment Board.
2. The Board maintains continuing jurisdiction over this permit and may, after a duly advertised public hearing, attach other conditions or modify these conditions as it deems appropriate in order to protect the public interest and welfare.



## Town of Arlington, Massachusetts

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### Environmental Design Review Public Hearing

#### Summary:

8:00 p.m. -  
8:30 p.m.

Board will reopen Special Permit Docket #3504 in accordance with the provisions of MGL Chapter 40A § 11, and the Town of Arlington Zoning Bylaw Section 3.4, Environmental Design Review. The applicant, Kevin Flynn for Springboard Schools, Inc. at **93 Broadway**, Arlington, MA, seeks approval of an addition to the school on the adjacent vacant lot, and a request to amend the parking reduction under Section 6.1.5.

- Applicant will be provided 10 minutes for updates from prior public hearing session.
- DPCD staff will be provided 5 minutes for updates.
- Members of the public will be provided time to comment.
- Board members will discuss docket and may vote.

#### ATTACHMENTS:

Type	File Name	Description
▢ Reference Material	200108_ARB_Submission.pdf	200108 ARB Submission
▢ Reference Material	Agenda_Item_1_-_EDR_Public_Hearing_Memo_Docket_#3504_93_Broadway_final.docx	EDR Public Hearing Memo Docket #3504 93 Broadway Final



TOWN OF ARLINGTON  
REDEVELOPMENT BOARD

Application for Special Permit In Accordance with Environmental Design  
Review Procedures (Section 3.4 of the Zoning Bylaw)

1. Property Address 93 Broadway Docket No. \_\_\_\_\_  
Name of Record Owner(s) 93 Broadway LLC Phone 781-646-3855  
Address of Owner 93 Broadway Arlington MA 02476  
Street City, State, Zip
2. Name of Applicant(s) (if different than above) \_\_\_\_\_  
Address \_\_\_\_\_ Phone \_\_\_\_\_  
Status Relative to Property (occupant, purchaser, etc.) \_\_\_\_\_
3. Location of Property \_\_\_\_\_  
Assessor's Block Plan, Block, Lot No. \_\_\_\_\_
4. Deed recorded in the Registry of deeds, Book 18677, Page 49;  
-or- registered in Land Registration Office, Cert. No. \_\_\_\_\_, in Book \_\_\_\_\_, Page \_\_\_\_\_.
5. Present Use of Property (include # of dwelling units, if any) Early Childhood Learning Center
6. Proposed Use of Property (include # of dwelling units, if any) Early Childhood Learning Center
7. Permit applied for in accordance with the following Zoning Bylaw section(s) 3.4 EDR  
6.1.5 parking reduction  
section(s) title(s)
8. Please attach a statement that describes your project and provide any additional information that may aid the ARB in understanding the permits you request. Include any reasons that you feel you should be granted the requested permission.

(In the statement below, strike out the words that do not apply)

The applicant states that Kevin Flynn, Springboard Schools, Inc. is the owner -or- occupant -or- purchaser under agreement of the property in Arlington located at 93 Broadway which is the subject of this application; and that unfavorable action -or- no unfavorable action has been taken by the Zoning Board of Appeals on a similar application regarding this property within the last two years. The applicant expressly agrees to comply with any and all conditions and qualifications imposed upon this permission, either by the Zoning Bylaw or by the Redevelopment Board, should the permit be granted.

Signature of Applicant(s)

Address

Phone

# TOWN OF ARLINGTON

Dimensional and Parking Information  
for Application to  
The Arlington Redevelopment Board

Docket No. \_\_\_\_\_

Property Location 93 BROADWAY

Zoning District B-4

Owner: 93 Broadway LLC

Address: 93 Broadway, Arlington MA 02476

Present Use/Occupancy: No. of Dwelling Units:  
EARLY CHILDHOOD LEARNING CENTER

Uses and their gross square feet:  
7867sf

Proposed Use/Occupancy: No. of Dwelling Units:  
EARLY CHILDHOOD LEARNING CENTER

Uses and their gross square feet:  
16347sf

	<u>Present Conditions</u>	<u>Proposed Conditions</u>	<u>Min. or Max. Required by Zoning for Proposed Use</u>
Lot Size	17,522	17,522	min. -
Frontage	100'	100'	min. 50'
Floor Area Ratio	.45	.93	max. 1
Lot Coverage (%), where applicable	-	-	max. -
Lot Area per Dwelling Unit (square feet)	-	-	min. -
Front Yard Depth (feet)			min. -
Side Yard Width (feet) right side	-	-	min. -
left side	-	-	min. -
Rear Yard Depth (feet)	77'-0"	18'-1"	min. 18'-1"
Height			min.
Stories	2 1/2	2 1/2	stories 3
Feet	35'-0"	35'-0"	feet 35'-0"
Open Space (% of G.F.A.)	55%	29%	min. 10%
Landscaped (square feet)	9803sf	5197sf	(s.f.) 1634sf
Usable (square feet)	-	-	(s.f.) -
Parking Spaces (No.)	3	6	min. 10
Parking Area Setbacks (feet), where applicable	3'-0"	3'-0"	min. 10'-0"
Loading Spaces (No.)	0	0	min. 1
Type of Construction			
Distance to Nearest Building	-	-	min.



Springboard Schools, Inc  
93 Broadway  
Arlington MA 02474

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12/13/19

Environmental Design Review- Impact Statement for 93 Broadway, Arlington.

**The uses requested are listed in the Table of Use Regulations as a Special Permit in the district for which application is made or is so designated elsewhere in this Bylaw.**

The proposed use, an early childhood learning center, is allowed in the B4 zone by Special Permit. We are proposing a new addition with a total of (5) classroom spaces.

**The requested use is essential or desirable to the public convenience or welfare.**

The proposed use, an early childhood learning center, provides a critical resource in the town. Childcare and early childhood learning are fundamental components of a livable community for many families. Being able to accommodate more kids spread thru-out our facility will be an attractive and much needed amenity to the Town.

**The requested use will not create undue traffic congestion, or unduly impair pedestrian safety.**

The parking area on the lot is accessed from Broadway and has 3 spots available for short term pick-up and drop –off. The proposed work requires 10 additional parking spots, however, the applicant is asking to reduce the number of required parking spaces from 10 to 3. The requested use will not add to traffic congestion nor impair pedestrian safety.

**The requested use will not overload any public water, drainage or sewer system or any other municipal system to such an extent that the requested use or any developed use in the immediate area or in any other area of the Town will be unduly subjected to hazards affecting health, safety, or the general welfare.**

The project consists of the development of the adjacent vacant lot which consists almost entirely of impervious paved surfaces. The existing property currently slopes from the rear of the lot out towards the front of the lot along Broadway. Essentially all stormwater that falls on this lot sheet flows across the sidewalk and out into the public way.

All of the existing paved surfaces will be removed, and pervious lawn, landscape and play areas will surround the proposed building. These proposed changes will allow stormwater to remain on site and naturally percolate into the ground instead of running off site into the public way. The proposed construction of the building addition will result in a significant decrease of impervious surfaces on the lot.

The gutters and downspouts from the proposed addition will be directed into a system of subsurface piping that will send these flows into the existing on site stormwater management system in order to further decrease stormwater runoff from the site.

There will be no new utility connections into the municipal systems in Broadway. We are proposing to interconnect the domestic water and sewer services from the proposed addition into the domestic services of the existing building. The existing services that were installed within the past 3 years consist of a 4" sprinkler, 2" domestic water and 6" sanitary and all have more than enough capacity to handle the increase in flows.

**Preservation of Landscape:** The landscape shall be preserved in its natural state insofar as practicable, by minimizing tree and soil removal and any grade changes shall be in keeping with the general appearance of neighboring developed areas.

Proposed changes to the site will include the removal of a large area of relatively impervious material (gravel and asphalt) in preparation for the new addition. In addition a small area of grass located adjacent to the exist'g building will be removed and converted to (3) new parking spaces. The proposed work includes improving the exist'g site with buffer plantings and new lawn area.

**Relation of the Building to the Environment:** Proposed development shall be related harmoniously to the terrain and to the use, scale and architecture of the existing buildings in the vicinity that have functional or visible relationship to the proposed buildings.

The project will look like a natural extension of the exist'g daycare center. It will be similar in size and will be clad in the same materials.

**Open space:** All open space (landscaped and usable) shall be so designed as to add to the visual amenities of the vicinity by maximizing its visibility for persons passing by the site or overlooking it from nearby properties. The location and configuration of usable open space shall be so designed



**as to encourage social interaction, maximize its utility and facilitate maintenance.**

We propose to surround our new addition with a grass covered play space and to ring this with a buffer of evergreen plantings. We intend to install a wood slat fence system for privacy along the back, the side and partially at the front. It will be detailed similarly to the wood slat fencing at the roof-top play area giving some visual continuity between the two spaces.

**Circulation: With respect to vehicular and pedestrian and bicycle circulation, including entrances, ramps, walkways, drives, and parking, special attention shall be given to location and number of access points to the public streets (especially in relation to existing traffic controls and mass transit facilities), width of interior drives and access points, general interior circulation, separation of pedestrian and vehicular traffic, access to community facilities, and arrangement of vehicle parking and bicycle parking areas, including bicycle parking spaces required by Section 8.13 that are safe and convenient and, insofar as practicable, do not detract from the use and enjoyment of proposed buildings and structures and the neighboring properties.**

Vehicle circulation will be largely unchanged. The new site plan provides parking for three (3) additional cars making the total parking capacity of (6) cars. The current arrangement has been adequate for its intended use and this proposal will be adding the same amount of additional spots for one fewer classroom than the original building. The Applicant has applied for a Special Permit under—Parking Reduction in Business, Industrial and Multi-Family Residential Zones. Under this section, ten (10) additional parking spaces are required due to the number of new classrooms. They are requesting the maximum reduction, to three (3) additional parking spaces. The owner has a working TDM in place and will continue to implement its plan. In addition the owner will provide additional covered bike parking for twelve (12) bikes at the secondary entrance canopy. Also, there can be more bike parking provided in the basement storage area.

**Surface Water Drainage: Special attention shall be given to proper site surface drainage so that removal of surface waters will not adversely affect neighboring properties or the public storm drainage system. Available Best Management Practices for the site should be employed, and include site planning to minimize impervious surface and reduce clearing and re-grading. Best Management Practices may include erosion control and stormwater treatment by means of swales, filters, plantings, roof gardens, native vegetation, and leaching catchbasins. Stormwater should be treated at least minimally on the development site; that which cannot be handled on site shall be removed from all roofs, canopies,**

**paved and pooling areas and carried away in an underground drainage system. Surface water in all paved areas shall be collected in intervals so that it will not obstruct the flow of vehicular or pedestrian traffic and will not create puddles in the paved areas.**

The proposal will significantly decrease the impervious surface on the site.

The project consists of the development of the adjacent vacant lot which consists almost entirely of impervious paved surfaces. The existing property currently slopes from the rear of the lot out towards the front of the lot along Broadway. Essentially all stormwater that falls on this lot sheet flows across the sidewalk and out into the public way.

All of the existing paved surfaces will be removed, and pervious lawn, landscape and play areas will surround the proposed building. These proposed changes will allow stormwater to remain on site and naturally percolate into the ground instead of running off site into the public way. The proposed construction of the building addition will result in a significant decrease of impervious surfaces on the lot.

The gutters and downspouts from the proposed addition will be directed into a system of subsurface piping that will send these flows into the existing on site stormwater management system in order to further decrease stormwater runoff from the site.

**Utilities Service: Electric, telephone, cable, TV, and other such lines of equipment shall be underground. The proposed method of sanitary sewage disposal and solid waste disposal from all buildings shall be indicated.**

There will be no new utility connections into the municipal systems in Broadway. We are proposing to interconnect the domestic water and sewer services from the proposed addition into the domestic services of the existing building. The existing services that were installed within the past 3 years consist of a 4" sprinkler, 2" domestic water and 6" sanitary and all have more than enough capacity to handle the increase in flows.

**Advertising Features: The size, location, design, color, texture, lighting and materials of all permanent signs and outdoor advertising structures or features shall not detract from the use and enjoyment of proposed buildings and structures and the surrounding properties.**

There will be no change to the exterior signage with this proposal.

**Special Features: Exposed storage areas, exposed machinery installations, service areas, truck loading areas, utility buildings and structures, and**

similar accessory areas and structures shall be subject to such setbacks, screen plantings or other screening methods as shall reasonably be required to prevent their being incongruous with the existing or contemplated environment and the surrounding properties.

There will be no change to the service areas with this proposal.

**Safety:** With respect to personal safety, all open and enclosed spaces shall be designed to facilitate building evacuation and maximize accessibility by fire, police and other emergency personnel and equipment. Insofar as practicable, all exterior spaces and interior public and semi-public spaces shall be so designed to minimize the fear and probability of personal harm or injury by increasing the potential surveillance by neighboring residents and passersby of any accident or attempted criminal act.

The project includes the construction of exit stairs from the new basement directly to grade as an improvement to egress.

**Heritage:** With respect to Arlington's heritage, removal or disruption of historic, traditional, or significant uses, structures or architectural elements shall be minimized insofar as practical whether these exist on the site or on adjacent properties.

The proposed project will have no impact on historical elements.

**Microclimate:** With respect to the localized climatic characteristics of a given area, any development which proposes new structures, new hard surface, ground coverage or the installation of machinery which emits heat, vapor or fumes shall endeavor to minimize insofar as practicable, any adverse impacts on light, air and water resources or on noise and temperature levels of the immediate environment.

The proposed project will contain play spaces at the perimeter of the new building. The use will not increase in intensity or duration from what is currently occurring. The classrooms all operate on slightly different schedules and efforts will be made to stagger outside time so as not to disrupt the neighborhood.

**Sustainable Building and Site Design:** Projects are encouraged to incorporate best practices related to sustainable sites, water efficiency, energy and atmosphere, materials and resources, and indoor environmental quality. Applicants must submit a current Green Building Council Leadership in Energy and Environmental Design (LEED) checklist,

**appropriate to the type of development, annotated with narrative description that indicates how the LEED performance objectives will be incorporated into the project.**

The project will include Energy Star fixtures and will include high R-value sprayfoam insulation thru-out.

**The requested use will not impair the integrity or character of the district or adjoining districts, nor be detrimental to the health, morals, or welfare.**

The requested use will improve the district by adding to an amenity already in place on a commercial corridor.

**The requested use will not, by its addition to a neighborhood, cause an excess of that particular use that could be detrimental to the character of said neighborhood.**

The property is located on a lot abutting both residential and commercial uses. The proposed use will not be detrimental to the character of the neighborhood; rather, it will improve the character of the neighborhood.

# Learn To Grow

## Transportation Demand Management (TDM)



**December 14, 2019**

**NOTE: Springboard is very pleased to report that the amount of environmentally friendly 'green' transportation use has dramatically risen since the schools inception in 2017. Incentive programs, installation of covered bicycle racks and communication with parents has proved successful. Accompanying this TDM are pictures that speak for themselves. Springboard is developing plans to increase further by using our elevator to store indoors bicycles, tricycles and tandem and sidecars indoors.**

- 1. How many staff members work at the current facility, and how many are projected to work at this site?**
  - a. LTG currently has a total of 19 staff. The staff will increase to 29 with the addition.
  - b. Currently 11 of our staff live in Arlington and we encourage any new staff, particularly those who live in Arlington to walk, bicycle or use public transportation. We anticipate the same ratio with the new staff so we expect 6 of the 10 new staff will also be from Arlington. We have implemented an incentive program with our staff and have are seeing better results over the past year to reduce automobile use.
- 2. How do current employees get to work now?**
  - a. 19 teachers, 7 drive, 5 walk or bicycle and 7 use public transportation.
  - b. We project that with 10 additional teachers, 4 will drive, 3 will walk or bicycle and 3 will use public transportation.
- 3. Are there alternative methods that will work for the existing staff?**
  - a. There are several alternative methods available such as biking, ride share and public transportation.
  - b. We have the great benefit at 93 Broadway of the bus stop next to the building. This provides a significant easier and less expensive means to commute to work.
- 4. How many customers do you expect to drive to the site? Include information on frequency, length of stay, and peak hours.**

The spreadsheet below summarizes the data we have accumulated with respect to this question and reflects what we have observed this past year+ to project the parking requirements for the new addition. We are pleased to report that the steps we have taken to encourage to families to not use cars has resulted in less use of automobiles. One of those steps in 2019, was the installation of multiple bicycle racks under shelter which resulted in much more bicycle use by our families.

Projected (93 Broadway) Family-Child Pick-ups & Drop-offs 2020											
AM	Drop-Off (autos only)	7:15 & 7:30	7:45	8:00	8:15	8:30	8:45	9:00	9:15	9:30	Totals
93 Broadway	Number of Families	12	9	12	11	8	8	7	5	2	74
	Average length of stay	3.5	3.5	3.5	4	3.5	4.5	4	5	4	3.6
	Number of parking required	4	3	6	5	3	3	2	2	1	
PM	Pick-Up (autos only)	12:00	12:30	3:00	4:00	4:30	5:00	5:30	6:00	6:00 +	
93 Broadway	Number of Families	7	6	8	7	10	10	12	9	5	74
	Average length of stay	6	5	5.5	5	4.5	4.5	5	5	5	5.1
	Number of parking required	3	2	2	2	4	5	6	4	2	

- i. The chart above illustrates that pickup and drop-off times are spread out in a manner that it has not been necessary to implement a more disciplined drop off and pick up procedure. LTG will maintain a more disciplined pick-up and drop-off procedure if needed.
- ii. Average length of stay is based on random surveillance of families from 2018 into 2019. The average length of stay is determined by the number of families in each time period in the chart divided into the total length of stays by those families. For example, 3 families staying a total of 15 minutes equals 5 minutes per family. Families do not always come at uniform times so experience shows that random surveillance provides enough data to support our findings. Much of the random surveillance is done during peak pickup and drop-off time periods through use of our parking lot camera.
- iii. LTG has also developed a contingency plan in the event there is a change in family transportation habits that would increase the amount of automobile use. That plan envisions three options;
  - i. We will provide an incentive plan for families (example- lower tuition, rebate or coupon program) to incentivize families to change to bicycling or public transportation means.
  - ii. LTG will institute a more disciplined drop off and pick up procedure. For instance parents will be required to provide or adhere to a specific drop off and pick up schedule and LTG staff will have the children ready to minimize on site stay time.
  - iii. Incentivize families using tuition discounts to pick up children during the non-peak pick up and drop-off time periods.

**5. What is your goal for total number of cars that will be driven to the site? Break out by employee and customer, and time of day, including information on peak hours.**

**b. This plan is still in effect.**

- i. The drop off/pick up spreadsheet provides the current and projected data patterns at LTG.
- ii. Out of the 7 employees who currently drive their arrival is staggered two employees arrive every ½ hour starting at 7:15 am thru 10:00 a.m. At the end of the day the 6 employees depart every ½ hour starting at 3 p.m. thru 6:00 p.m.
- iii. LTG projects an additional 4 employees will drive to the site with the new addition. We will implement the same staggered schedule if needed.

**6. How much parking is provided, and how do you intend to use it?**

- a. 93 Broadway currently has three parking and has not experienced any parking problems with the current capacity of 6 classrooms. The addition will house 5 classrooms, one fewer than the current capacity, but will double parking capacity adding 3 additional spots to total 6. There is also currently 7 parking spaces on Broadway adjacent to the school. We remain confident that the balance we struck between our parking needs and green space for our children and parking availability on Broadway and North Union Street remains correct.

**7. If there is transit in the area, please provide information on bus routes and location of bus stops.**

**Where can I go?**

The Massachusetts Bay Transportation Authority (MBTA, or the “T”) and private bus operators run more than a dozen routes in and near Arlington. They provide convenient connections to Cambridge and downtown Boston, plus service to Burlington, Lexington, Somerville and Waltham. The primary routes serving Arlington, with frequent service including evenings and weekends, are the MBTA Red Line and the #77 bus.

The Red Line provides subway service between Alewife Station (near East Arlington) and Somerville (Davis Square), Cambridge and Boston. Every bus route in Arlington, except for the #80, connects to the Red Line. The Minuteman Bikeway also provides convenient bike and pedestrian access from East Arlington to Alewife Station.

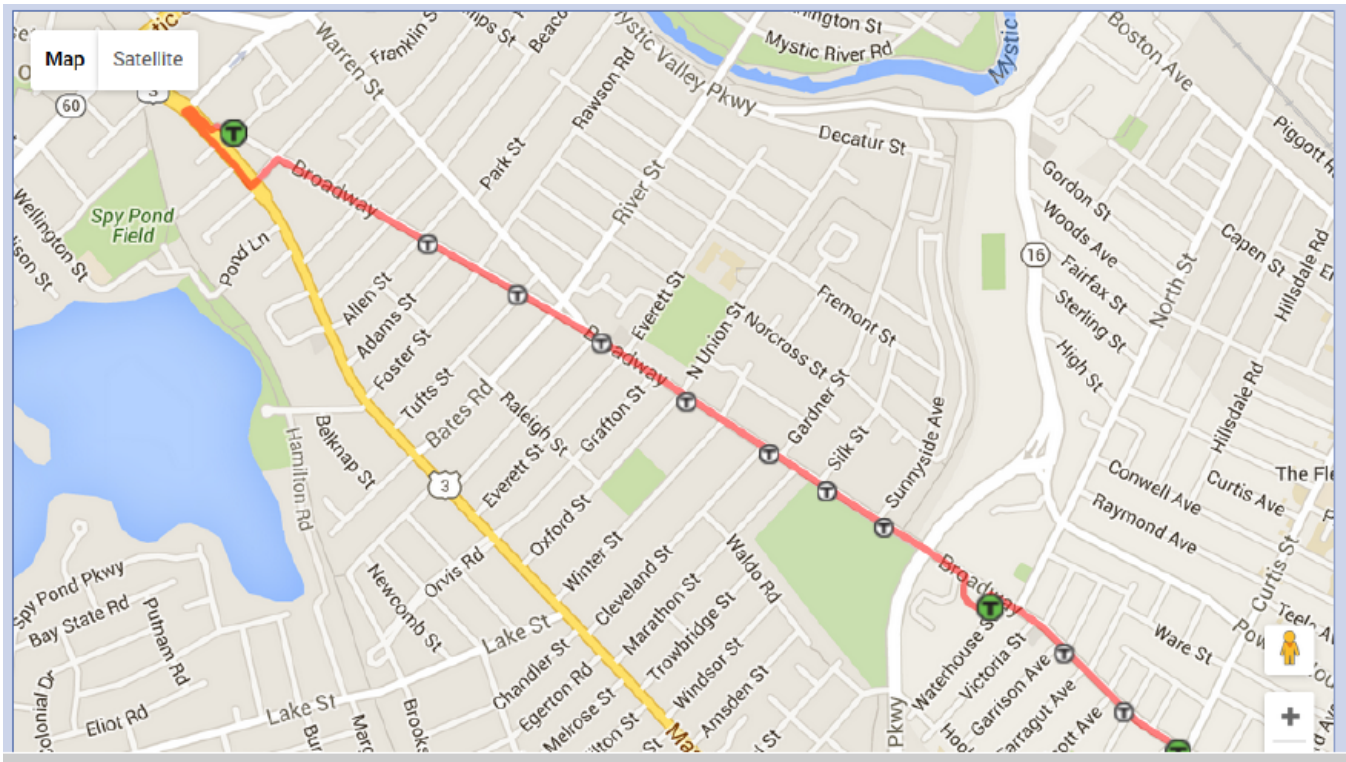
The #77 bus serves Mass Ave between Arlington Heights and Harvard Square, with connections to the Red Line at Porter and Harvard.

Other MBTA bus routes (see [www.mbta.com](http://www.mbta.com) for schedules) include

- #62 Alewife to Arlington Heights (via Park Avenue), then to Lexington and Bedford
- #67 Alewife to Turkey Hill via Pleasant Street and Arlington Center
- #76 Alewife to Hanscom Air Base, via the Route 2 service road and Lexington.
- #78 Harvard to Arlington Heights
- #79 Alewife to Arlington Heights via Mass Ave.
- #80 Lechmere to Arlington Center via Somerville and West Medford (Tufts University). Green Line connection at Lechmere.
- #84 Alewife to Arlington
- #87 Lechmere to Arlington Center, via Broadway in Arlington. Red Line connection in Davis Square, Somerville.
- #350 Alewife to Burlington, via Mass Ave in East Arlington and Mystic Street. Serves Burlington Mall.
- #351 Express bus between Alewife and office parks along Mall Road and Middlesex Turnpike in Burlington.



## Bus #87 stops on Broadway



### 8. How will you communicate with staff and customers about expectations for vehicles and alternative transportation?

For the past 18 months, we have conducted monthly staff meetings and it is a subject we bring up with staff. We discuss the alternative means of transportation as well as point out that our building is directly on the bus stop. During orientation for both new staff and new families we review our parking policies and point out public transportation and bicycling alternatives. This includes procedures for staff parking and drop off and pick up for families.

### 9. Which of the following TDM measures will you use to reduce car traffic to the site? All projects requesting a parking reduction must employ at least three (3) TDM methods described below. Please elaborate with additional information.

- a) Charge for parking on-site; Pay a stipend to workers or residents without cars;
  - i. We provide an incentive plan for families to incentivize families to change to other transportation means. We have had growing success with the use of 'green' transportation and less resistance from family's use of cars that cite time constraints and practicality issues.

**NOTE:** We strongly recommend to the town of Arlington that bicycle lanes be added to Broadway. In our family meetings this is a common theme.

- b) Provide transit pass subsidies (at least 50%);
  - i. We will pay 50% for employee transit passes.
- c) Provide covered bicycle parking and storage;
  - i. LTG created two onsite covered bicycle racks that can house twelve bicycles on Broadway and along North Union Street. The new addition will provide an additional 12 covered bicycle storage racks.
  - ii. To accommodate more bicycle LTG will create additional bicycle storage in the basement. The basement is accessible through the elevator for this use.



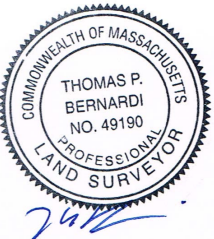
PLAN OF LAND

LOCATED AT  
93 BROADWAY  
ARLINGTON, MA

PREPARED FOR:  
SPRINGBOARD SCHOOL, INC.

SCALE: 1 INCH = 20 FEET

ZONING B4:  
FRONT 0'  
SIDE 0'  
REAR 10+(1/10)



NOTES

THIS PLAN WAS MADE FROM AN INSTRUMENT SURVEY ON THE GROUND BETWEEN THE DATES OF SEPTEMBER 12 AND SEPTEMBER 15, 2016 AND ALL STRUCTURES ARE LOCATED AS SHOWN HEREON.

THIS PLAN WAS PREPARED IN CONFORMITY WITH THE RULES AND REGULATIONS OF THE REGISTERS OF DEEDS OF THE COMMONWEALTH OF MASSACHUSETTS.

THOMAS BERNARDI P.L.S.      DATE: DEC. 8, 2019

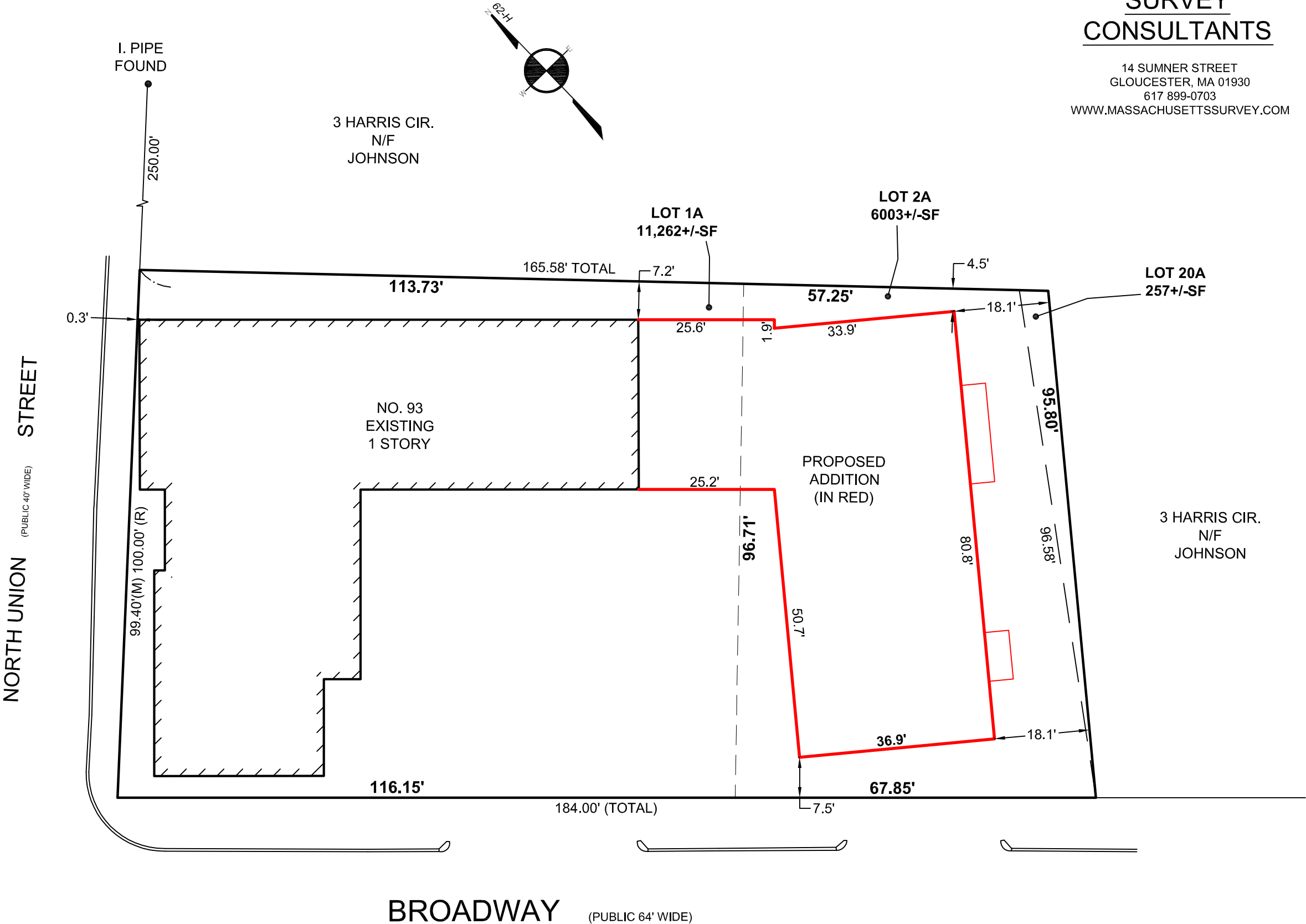
REFERENCES

DEED: BOOK 68144, PAGE 282 (UNREGISTERED LAND - LOTS 1 & 2)  
DEED: BOOK 1037, PAGE 197 (REGISTERED LAND - LOT 20A)  
PLAN: L.C.C. 62C & 62H  
PLAN: BOOK 7614, PAGE 388

ACCORDING TO FEMA MAP 25017C0417E, DATED JUNE 4, 2010, THE EXISTING BUILDING IS LOCATED OUTSIDE THE SPECIAL FLOOD HAZARD AREA (S.F.H.A.)

MASSACHUSETTS  
SURVEY  
CONSULTANTS

14 SUMNER STREET  
GLOUCESTER, MA 01930  
617 899-0703  
WWW.MASSACHUSETTSSURVEY.COM



Springboard Schools, Inc

93 Broadway  
Arlington, MA 02474

**Owner**  
Kevin Flynn  
Springboard Schools, Inc  
508 Groton Rd  
Westford, MA  
978-808-6621

**Architecture**  
BROWN FENOLLOSA Architects, Inc  
197 Broadway Arlington, MA 02474  
zeke@brownfenollosa.com / 781 641 9500

**Surveyor**  
Massachusetts Survey Consultants  
10 1st Ave #24  
Peabody MA 01960  
p: 617-899-0703

DRAWING LIST:	
T1.0	TITLE PAGE, DRAWING INDEX, ZONING NOTES
T1.1	CODE REVIEW, EGRESS
L1.0	SITE PLAN
A2.0	PROPOSED PLANS – BASEMENT
A2.1	PROPOSED PLANS – FIRST FLOOR
A2.2	PROPOSED PLANS – SECOND FLOOR
A2.3	PROPOSED PLANS – ATTIC
A3.0	PROPOSED EXTERIOR ELEVATIONS
A3.1	PROPOSED EXTERIOR ELEVATIONS

ZONING SUMMARY: 93 Broadway Arlington, MA 02474

PARCEL ID:  
ZONING DISTRICT: BUSINESS (B4)  
USE: I-4 CHILD CARE FACILITY

CONSTRUCTION CLASSIFICATION: 5B, UNPROTECTED  
OCCUPANT LOAD:

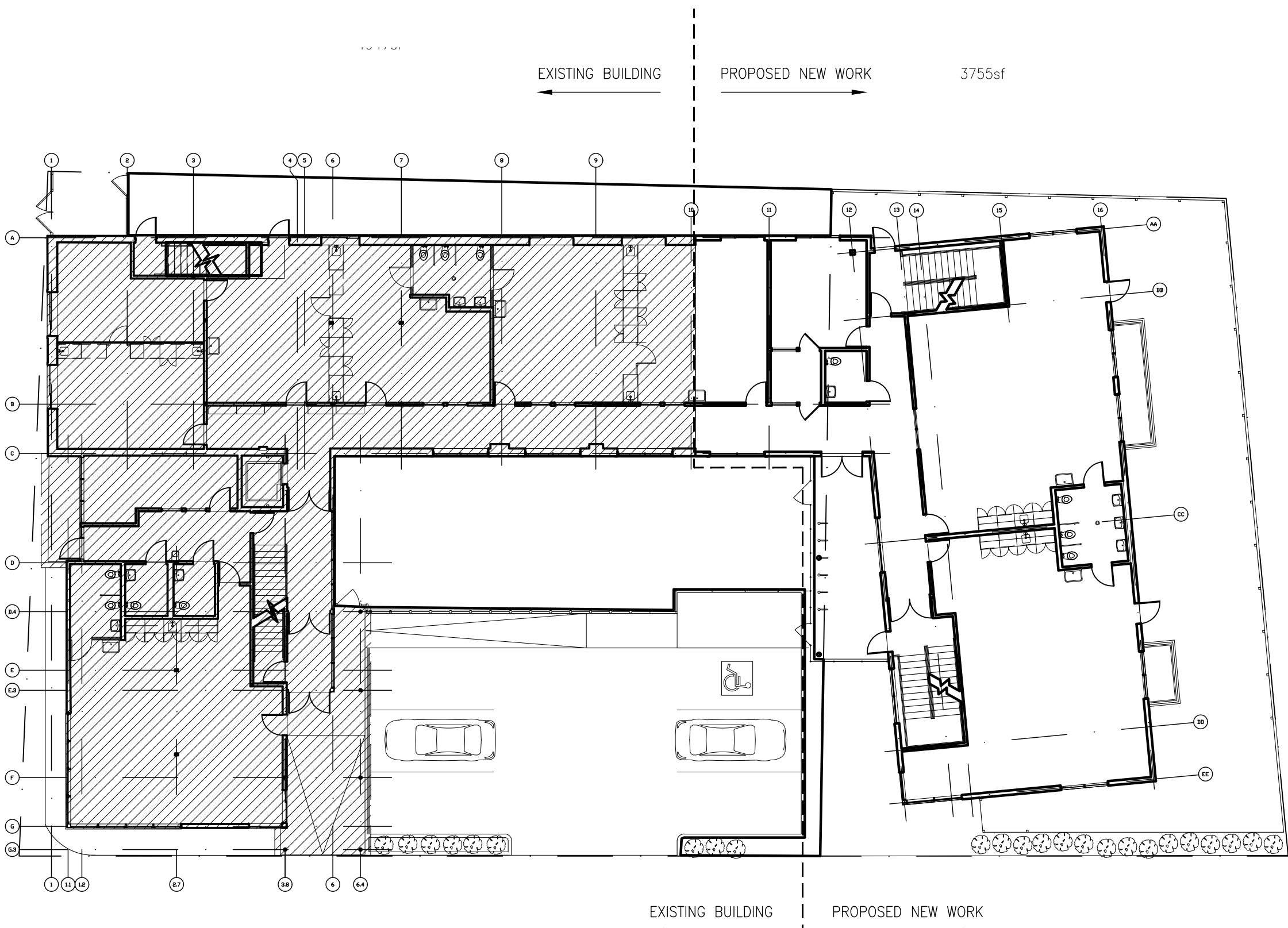
NARRATIVE OF WORK:

THE PROJECT CONSISTS OF A NEW ADDITION TO EXISTING TWO STORY DAY CARE CENTER BUILDING. THE NEW ADDITION WILL BE USED AS AN EXTENSION OF THE DAYCARE CENTER ON FLOORS BASEMENT, ONE AND TWO. THE ATTIC WILL BE USED FOR STORAGE. THE NEW BUILDING WILL BE WOOD FRAME, TYPE 5B CONSTRUCTION, 2 1/2 STORIES, AND FULLY SPRINKLERED. THE PROJECT ALSO CONSISTS OF EXTENSIVE SITE IMPROVEMENTS.

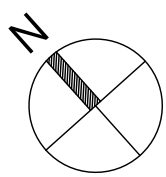
- APPLICABLE CODES  
THE 9TH EDITION OF THE MASSACHUSETTS STATE BUILDING CODE (MSBC). APPLICABLE CODES, STANDARDS, AND GUIDELINES FOR THIS PROJECT INCLUDE, BUT ARE NOT LIMITED TO THE FOLLOWING, CURRENT EDITIONS:
- 2015 IBC: INTERNATIONAL BUILDING CODE
  - 2015 IEBC: INTERNATIONAL EXISTING BUILDING CODE – LEVEL 3 ALTERATION
  - 780 CMR: MASSACHUSETTS AMENDMENTS TO THE INTERNATIONAL BUILDING CODE (MSBC), 9TH EDITION
  - 521 CMR: MA ARCHITECTURAL ACCESS BOARD (MAAB)
  - AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES (ADAAG)
  - 248 CMR: MA STATE PLUMBERS AND GASFITTERS CODE
  - 527 CMR, CH.12: NATIONAL ELECTRICAL CODE WITH MA AMENDMENTS

SUBAREA SUMMARY:	EXISTING BUILDING GROSS AREAS:	PROPOSED BUILDING GROSS AREAS:	EXIST'G AND PROPOSED GFA FOR FAR:
CELLAR:	3300 sf	3379 sf	0 sf
FIRST FLOOR:	4947 sf	3747 sf	8694 sf
SECOND FLOOR:	2920 sf	3564 sf	6484 sf
ATTIC:	0 sf	1169 sf	1169 sf
TOTAL:	11,167 sf	11,859 sf	16,347 sf

DIMENSIONAL CALCULATIONS:	REQUIRED:	EXISTING:	PROPOSED:
MIN. LOT SIZE:.	Not Applicable	17,522sf	Not Applicable
MIN. LOT AREA/DWELLING UNIT:	Not Applicable	Not Applicable	Not Applicable
MIN. FRONTAGE (ft):	50'-0"	100'-0"	100'-0"
MAX. FLOOR AREA RATIO (F.A.R.):	1.0	.45	.93
MIN. OPEN SPACE: LANDSCAPED	10%	55%	29%
MIN. OPEN SPACE: USABLE	Sec. 5.3.21	-	-
MAX. LOT COVERAGE	Not Applicable	Not Applicable	Not Applicable
MAX. BUILDING HEIGHT:			
MAX. STORIES:	3	2 1/2	2 1/2
FEET:	35'-0"	35'-0"	35'-0"
MIN. YARD IN FEET:			
FRONT:	Not Applicable	Not Applicable	Not Applicable
SIDE:	Not Applicable	Not Applicable	Not Applicable
REAR:	10+(L/10)	77'-0"	18'-1"



1 KEY PLAN  
SCALE: 1/16" = 1'-0"



SPRINGBOARD SCHOOLS  
87-89 BROADWAY ARLINGTON MA

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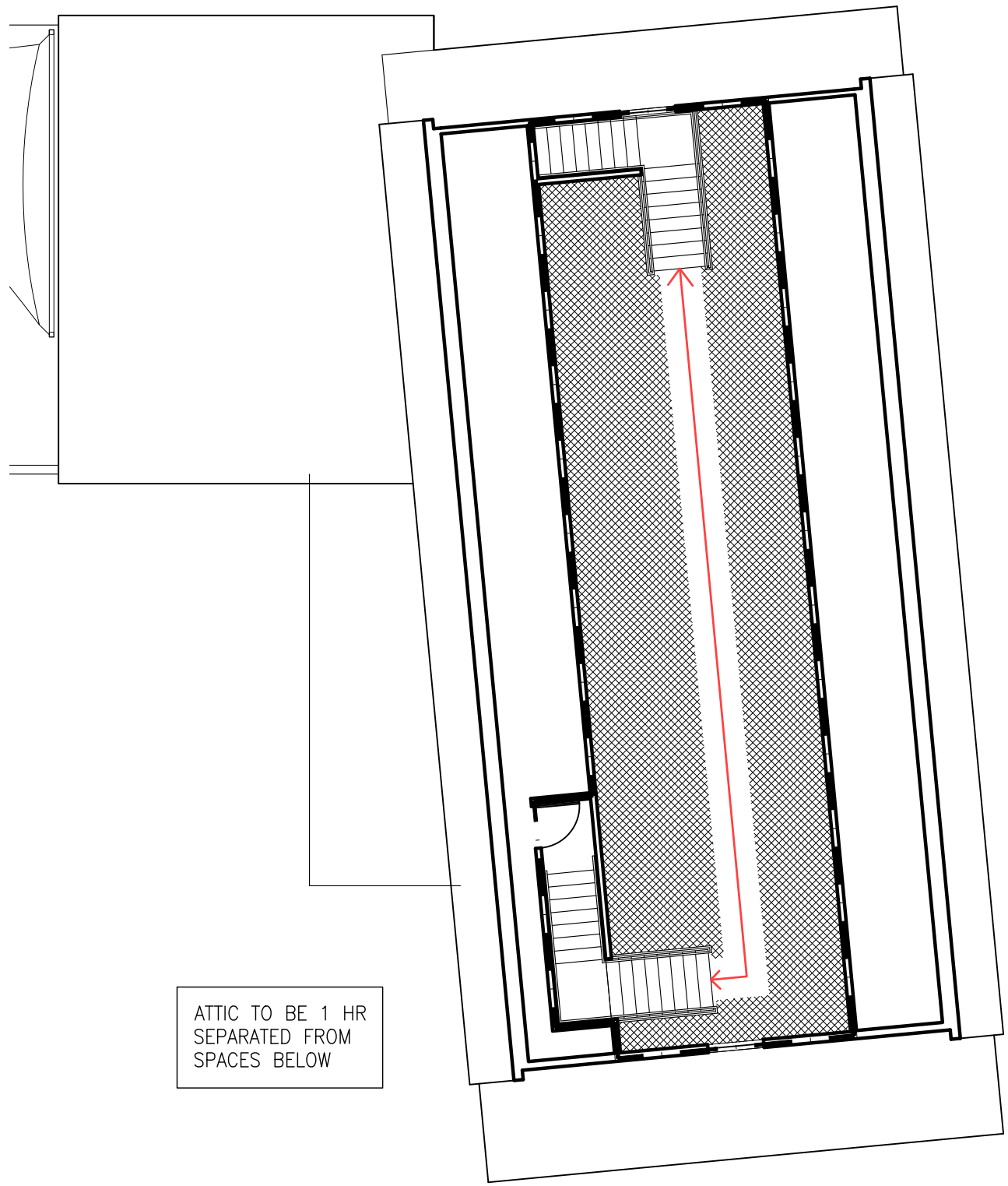
Revision : Date :

TITLE SHEET

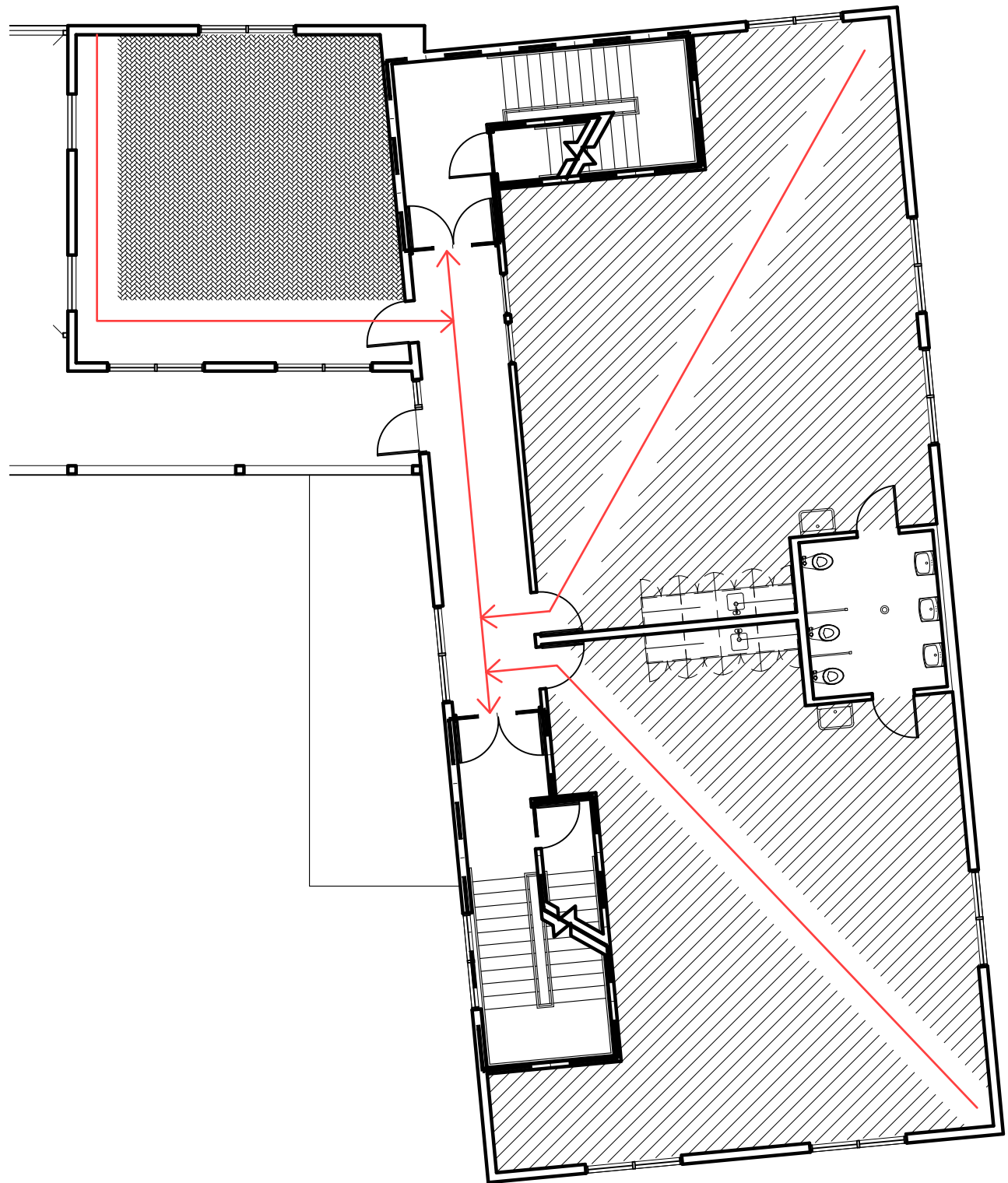
Date : 07-Jan-2020  
Scale : AS NOTED  
Project Number : --  
Drawn by :

T1.0





4 ATTIC EGRESS PLAN  
3/32" = 1'-0"



3 SECOND FLOOR EGRESS PLAN  
3/32" = 1'-0"

### Project Summary

The existing building, 93 Broadway, is located in Arlington, MA. The proposed project involves an addition to the exist'g structure. It will have a 5B classification, it will be fully sprinklered, and will be 2 1/2 story structure with a full cellar.

Code Type	Applicable Codes
Building	780 CMR: Massachusetts State Building Code, 9th Edition (2015 International Building Code) (2015 International Existing Building Code)
Fire Prevention	527 CMR: Massachusetts Fire Prevention Regulations M.G.L. Chapter 148 Section 26G – Sprinkler Protection
Accessibility	521 CMR: Massachusetts Architectural Access Board Regulations
Electrical	527 CMR 12.00: Massachusetts Electrical Code (2014 National Electrical Code)
Elevators	524 CMR: Massachusetts Elevator Code (2004 ASME A17.1)
Mechanical	2009 International Mechanical Code (IMC)
Plumbing	248 CMR: Massachusetts Plumbing Code
Energy Conservation	2009 International Energy Conservation Code

### Occupancy Classification

The proposed building will be used as a Daycare space (Use Group I-4).

### Construction Type

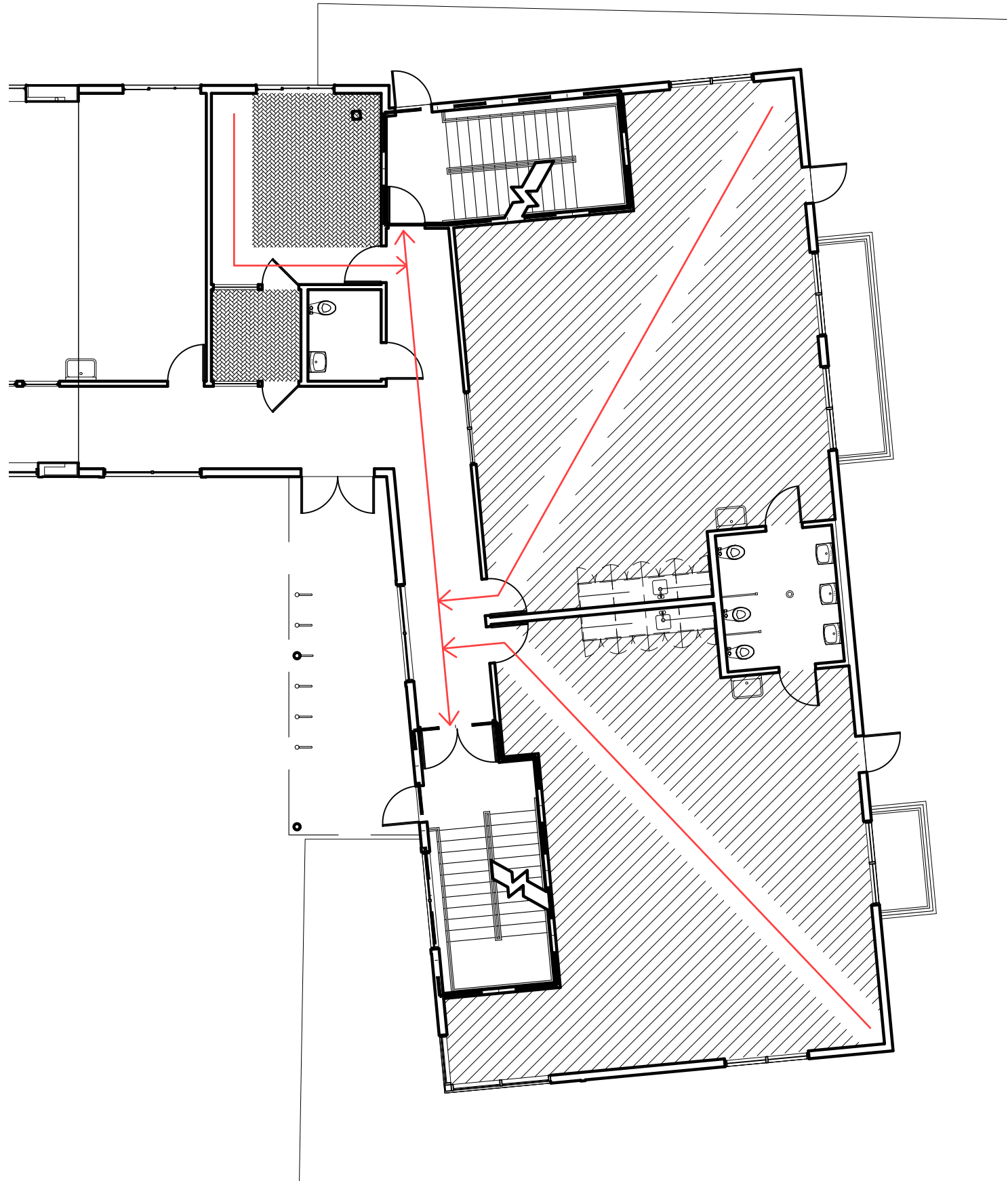
The building is constructed as Type 5B (exterior walls , interior elements are anything permitted).

### Height and Area Limitations

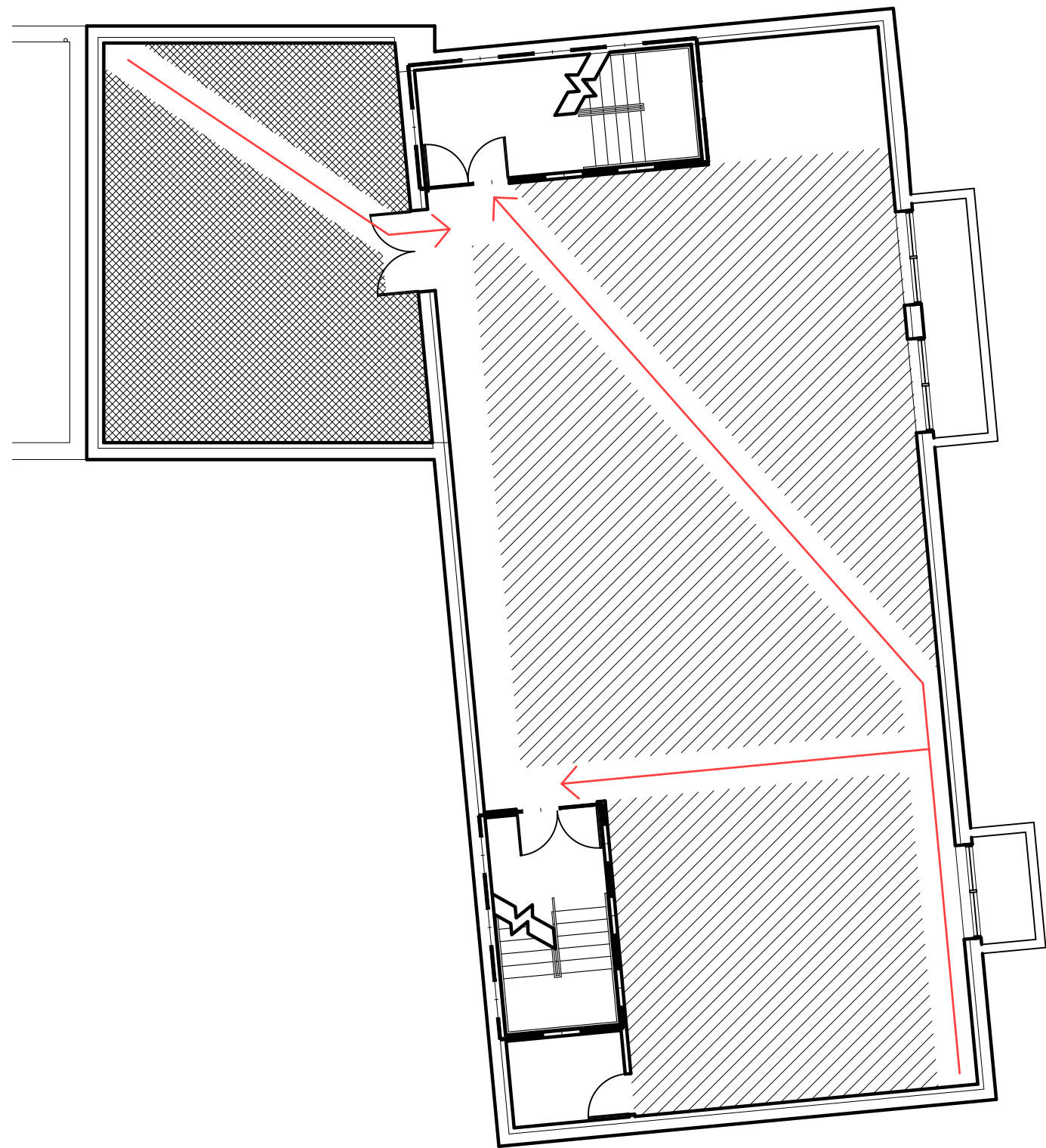
Use Group I-4 (fully sprinklered): 2 stories, 27,000sf  
Overall building area: 23,048sf (including all cellar and attic space).

### Fire Resistance Ratings

The following table summarizes the required fire resistance ratings for various building elements based on 780 CMR Table 601 and other applicable code provisions for Type 5B construction.



2 FIRST FLOOR EGRESS PLAN  
3/32" = 1'-0"



1 CELLAR EGRESS PLAN  
3/32" = 1'-0"

Building Element	Fire Resistance Ratings (Hrs)
Structural Frame	0
Exterior Bearing Walls	0
Interior Bearing Walls	0
Floor Construction	0
Roof Construction	0

Required separation of occupancies- S-2 and I-4- 1hr (IBC 508.4)

### Vertical Opening Requirements

New vertical openings connecting not more than 4 floors in a building protected with an automatic sprinkler system:  
1 hr protection required (IBC 708.4)

### Required Fire Protection Systems

Mass General Law 148 Chapter 26G:

Fire sprinklers are required in all existing and new buildings when the aggregate building area (including all floors) is 7,500sf or greater.

Fire Extinguishers shall be required (IBC 906.1). one per floor.

Fire alarm and detection system required in areas of new occupancy and existing alarm devices shall be automatically activated throughout building. (IBC 907.2.6 )

- Audible alarms (907.5)
- Voice Alarm Communication can be substituted with a distinct signal (MA Amendments 907.5.2.2.6) provided total building evacuation is required.
- Alarm system must have emergency power backup system.
- Visible alarms required at all public and common areas (907.5.2.3)

### Means of Egress

See code floor plans for occupant load and exit capacity calculations

- Maximum exit access travel distance (w/ 2 exits) does not exceed 200 feet for Occupancy I-4 and 400 feet for S-2 (IBC 1017.1) w/ sprinklers.
- Exit enclosures must be 1 hour fire rated when connecting less than 4 stories.
- Maximum dead end corridor length does not exceed 20 feet (IBC 1020.4).
- Remote means of egress are separated by 1/3 of the diagonal dimension of the room or space they serve (IBC 1007.1.1.2).

- All rooms or spaces with an occupant load greater than 10 people, or a travel distance over 75 ft are provided with two egress doors and illuminated exit signs at each exit (IBC 1006.2.1).
- All doors serving an occupant load of 50 or more swing in the direction of egress travel (IBC 1010.1.2.1).
- All means of egress lighting and exit signs throughout the building must be provided with an emergency power supply to assure continued illumination for not less than 1.5 hours in case of primary power loss (IBC 1008.3).

### Interior Finish Requirements

The existing interior finish of walls and ceilings in the work area and in all exits and corridors serving the work area must comply with the code requirements for new construction (IBC 801). All newly installed wall and ceiling finishes, and interior trim materials must also comply with IBC Table 803.11. The requirements are summarized below:

Walls & Ceilings (IBC Table 803.9)		
Use Group	I-4 (Sprinklered)	S-2 (Sprinklered)
Exit Stairs, enclosures and passageways	Class B	Class C
Corridors	Class B	Class C
Rooms & Enclosed Spaces	Class B	Class C

All Admin spaces can be Class C

### Structural

Must comply with IBC 1601

### Electrical

Must comply with IBC 2701

### Energy

Must comply with IBC 1301

### Accessibility

Alterations to the building must comply with the requirements of the Massachusetts Architectural Access Board Regulations (521 CMR).

All portions of the building open to (clients, visitors, delivery, etc) must comply in full with the current requirements of 521 CMR, including the general public

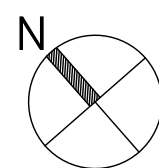
- All public entrances must be accessible (521 CMR 25.1)
- Accessible routes throughout the work area (521 CMR 20.1)
- Accessible toilet rooms (521 CMR 30)
- Accessible drinking fountains, where drinking fountains are provided (521 CMR 36.1)
- Where 2 means of egress are required both must be accessible (521 CMR 20.11.1)

Any employee-only areas such as staff lounges, staff bathrooms, and staff work areas are not required to comply with 521 CMR, as long as public access is not permitted.

OCCUPANT LOAD			
USE	FLOOR AREA	FLOOR AREA/SF	OCCUPANT LOAD
CLASSROOM	5067sf	35	145
ADMIN	556sf	100	6
STORAGE/ MECH	1402sf	300	5
		156	121

EXIT CAPACITY			
EXIT	DOOR WIDTH	ALLOWANCE/ PERSON	CAPACITY (PERSONS)
BACK STAIR	34"	.15	227
FRONT STAIR	34"	.15	227

1 HR RATED ASSEMBLY AT STAIR ENCLOSURES	STORAGE/ MECH
NOTE: CEILING AT BASEMENT AND 2ND FLOOR TO BE 1 HR RATED	CLASSROOM
EGRESS ROUTE	ADMIN



SPRINGBOARD SCHOOLS  
87-89 BROADWAY ARLINGTON MA

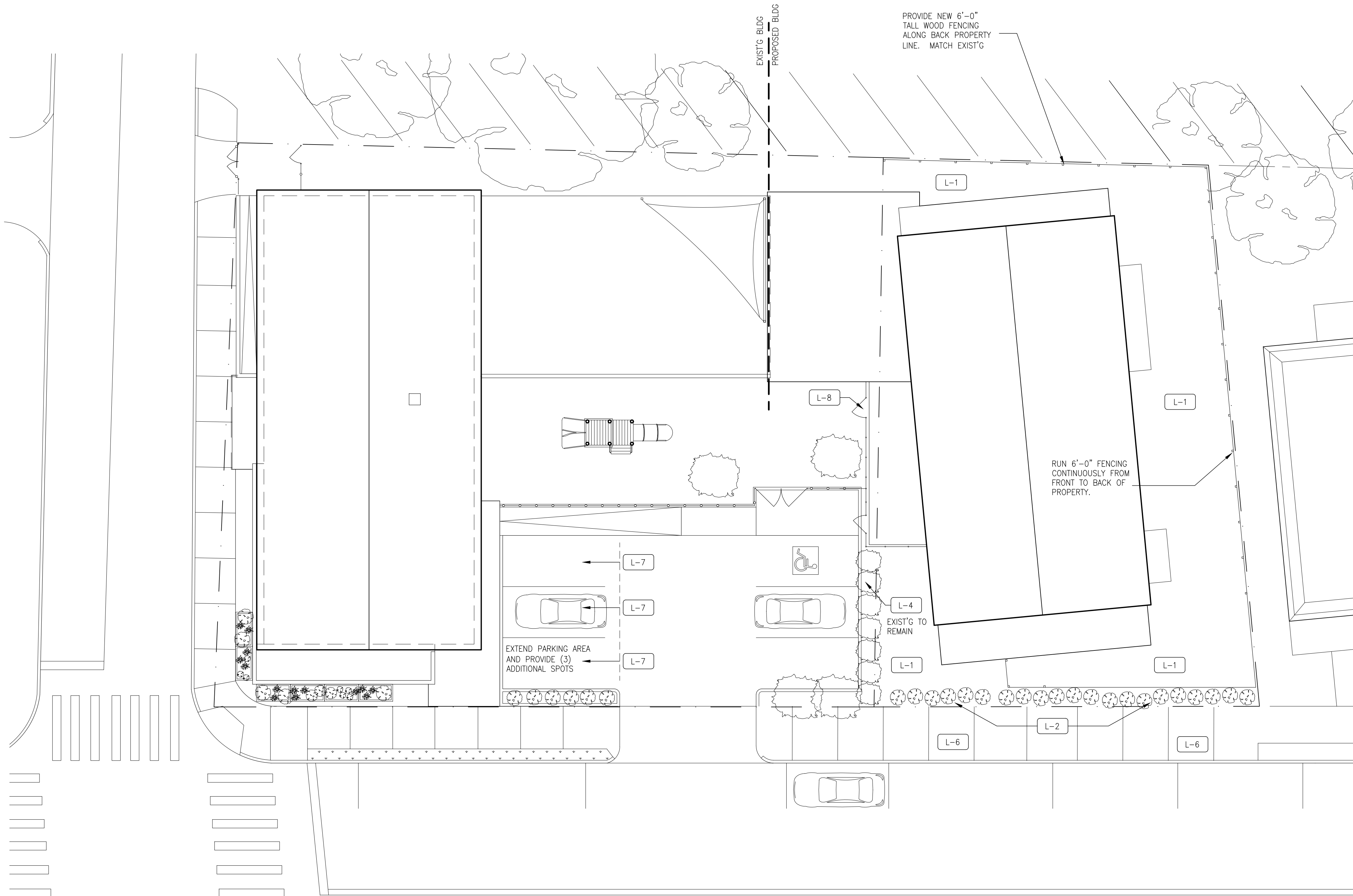
BROWN FENOLLOSA ARCHITECTS INC  
197 Broadway Arlington MA 02474  
P 781 641 9500 info@brownfenollosa.com

### CODE AND EGRESS

Date : 09-Dec-2019  
Scale : AS NOTED  
Project Number : --  
Drawn by :

T1.1





PROVIDE NEW 6'-0"  
TALL WOOD FENCING  
ALONG BACK PROPERTY  
LINE. MATCH EXIST'G

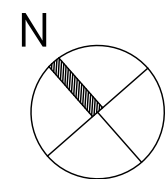
EXIST'G BLDG  
PROPOSED BLDG

RUN 6'-0" FENCING  
CONTINUOUSLY FROM  
FRONT TO BACK OF  
PROPERTY.

EXTEND PARKING AREA  
AND PROVIDE (3)  
ADDITIONAL SPOTS

EXIST'G TO  
REMAIN

1 SITE PLAN  
SCALE: 3/32" = 1'-0"



- L-1 PROVIDE 6"-8" CLEAN FILL FOR ROUGH GRADING,  
4" CLEAN LOAM, RAKED SMOOTH, ROLLED SOD
- L-2 PROVIDE ORNAMENTAL GRASSES. PREP SOIL AS  
NEC'Y, BLACK MULCH BED.
- L-4 PROVIDE CONTINUOUS 'HEDGE' OF 5'-6'H  
EVERGREEN THUJAS. PREP SOIL AS NEC'Y, BLACK  
MULCH BED.
- L-6 SIDEWALK CONCRETE.
- L-7 2 LAYER ASPHALT PAVING, STRIPED AND SIGNED  
FOR PARKING. PROVIDE CLEAN SUB-GRADE  
MATERIALS, COMPACTED. SLOPE TO DRAINS.  
PROVIDE FORMED ASPHALT CURBING
- L-8 4'-0" TALL BLACK CHAIN LINK FENCING

## SPRINGBOARD SCHOOLS

87-89 BROADWAY ARLINGTON MA

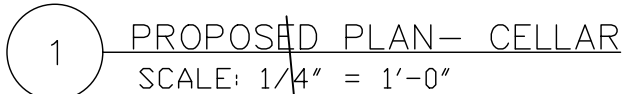
BROWN FENOLLOSA ARCHITECTS INC  
197 Broadway Arlington MA 02474  
P 781 641 9500 info@brownfenollosa.com

Revision : Date :

SITE PLAN

Date : 09-Dec-2019  
Scale : AS NOTED  
Project Number : --  
Drawn by :

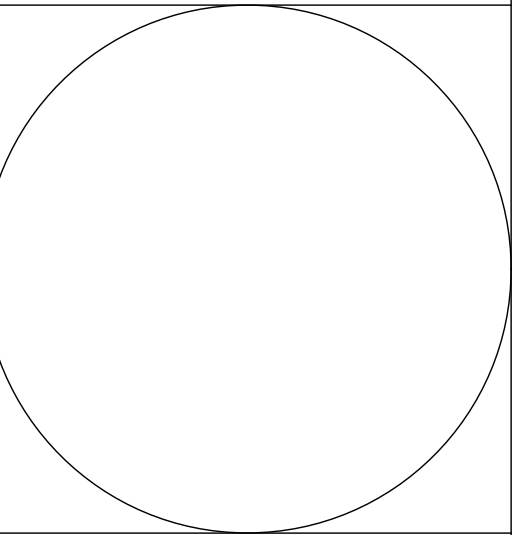
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SPRINGBOARD SCHOOLS

87-89 BROADWAY ARLINGTON MA

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197 Broadway Arlington MA 02474  
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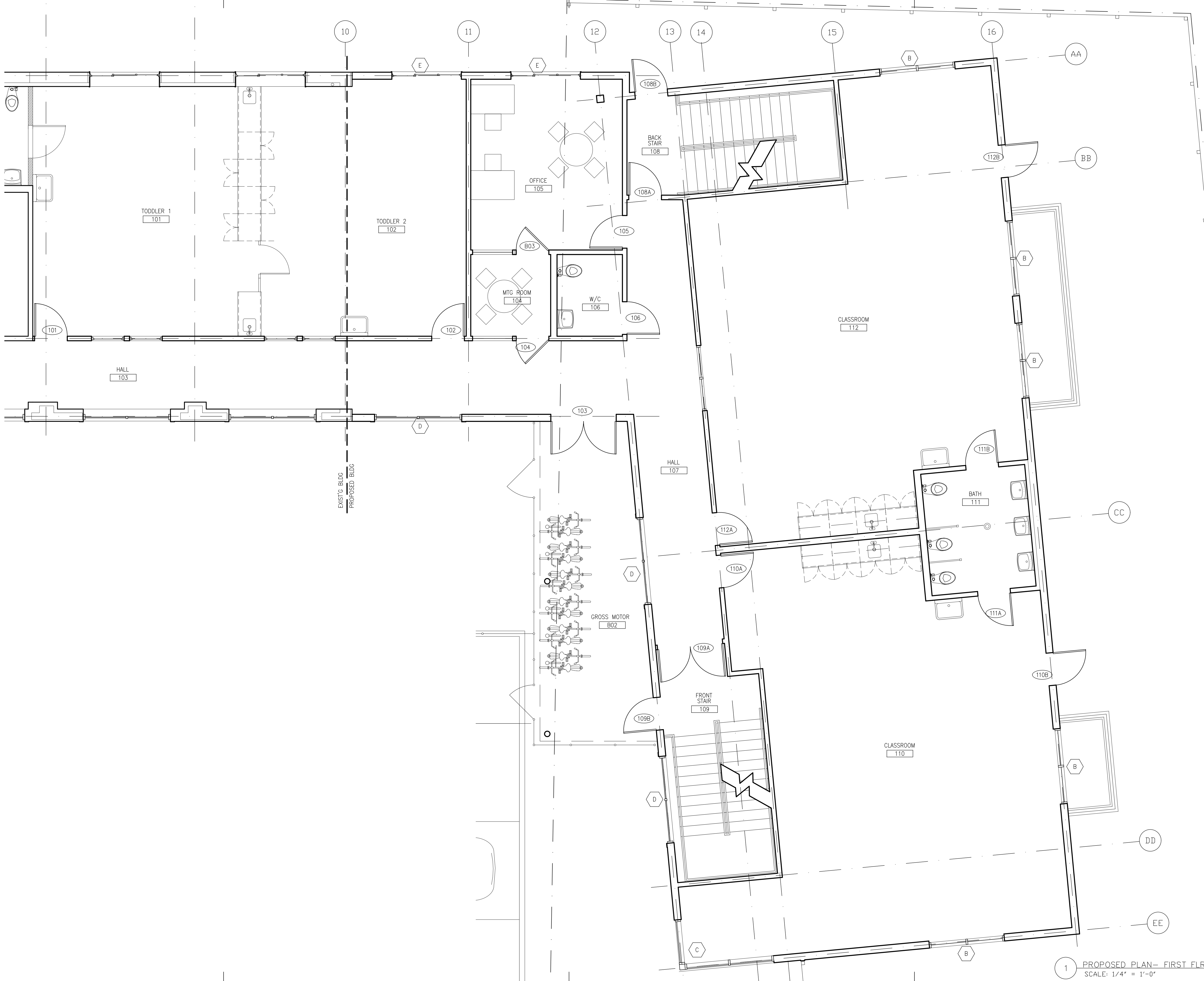


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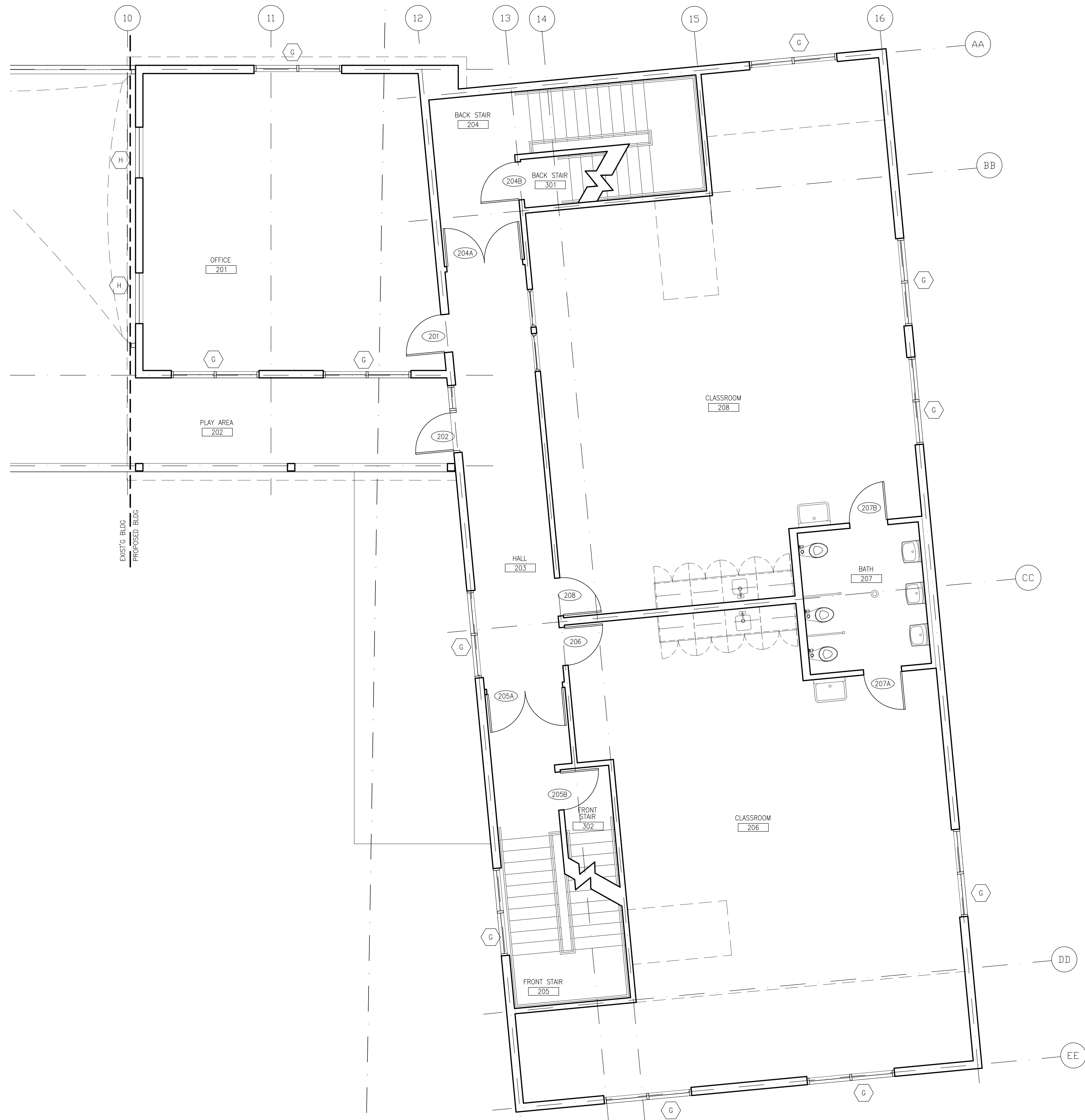
PROPOSED PLANS

Date : 09-Dec-2019  
Scale : AS NOTED  
Project Number : --  
Drawn by :

A2.1



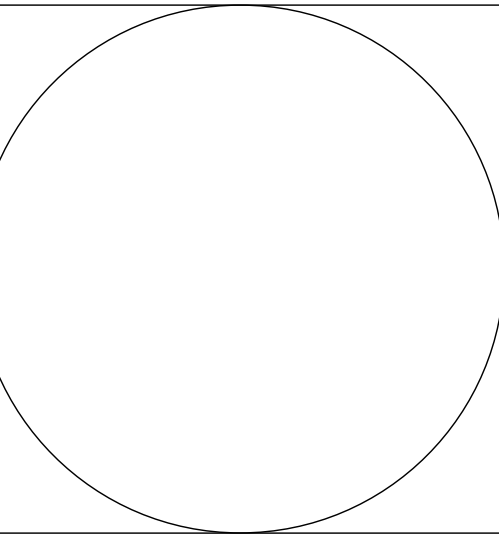
1 PROPOSED PLAN- FIRST FLR  
SCALE: 1/4" = 1'-0"



# SPRINGBOARD SCHOOLS

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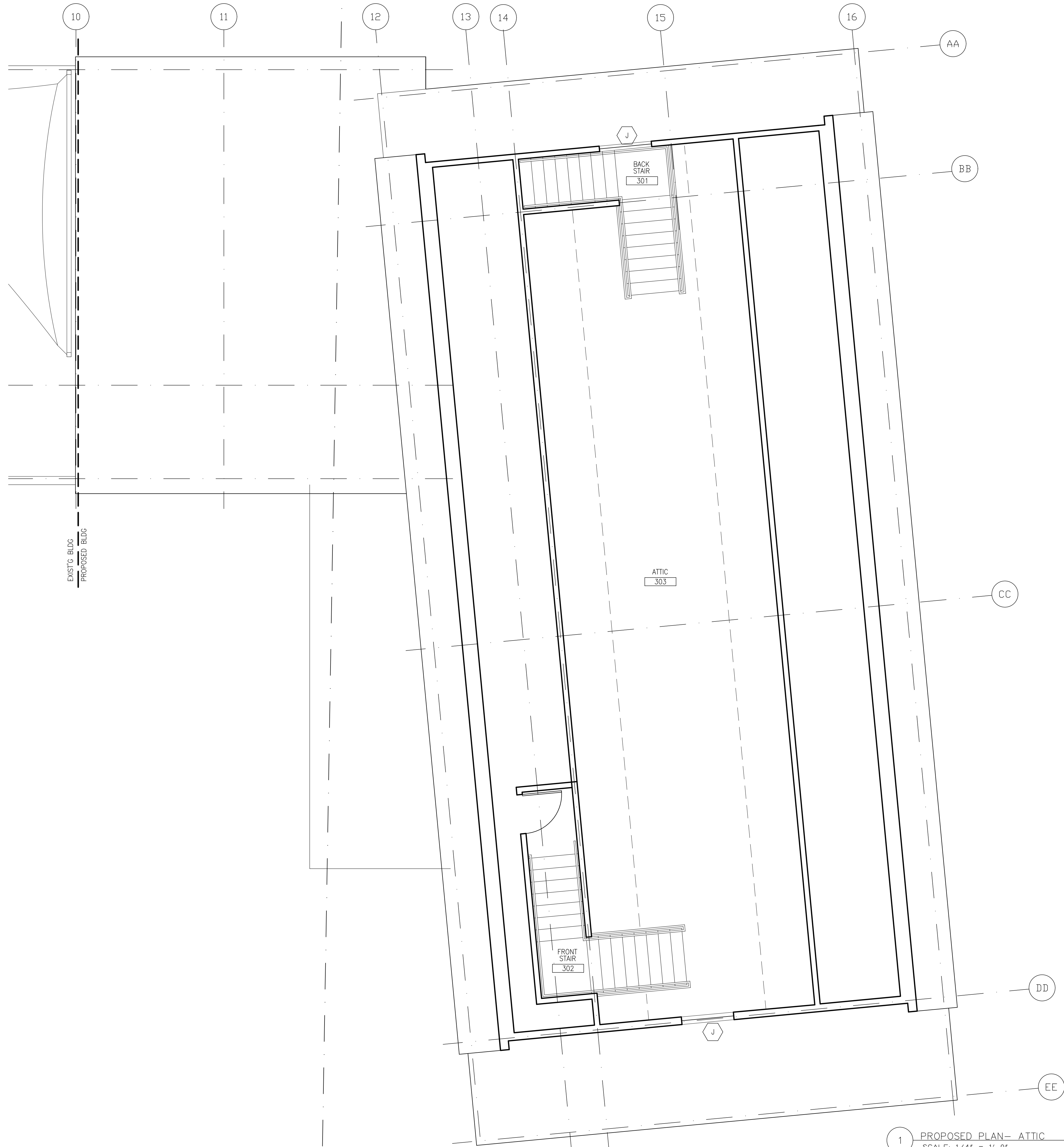
Revision : Date :

## PROPOSED PLANS

Date : 09-Dec-2019  
Scale : AS NOTED  
Project Number : --  
Drawn by :



# A2.2

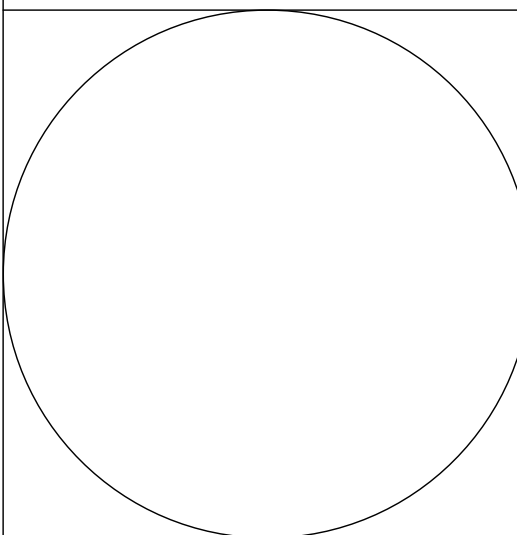


1 PROPOSED PLAN- ATTIC  
SCALE: 1/4" = 1'-0"

SPRINGBOARD SCHOOLS

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Revision : Date :

ATTIC PLAN

Date : 09-Dec-2019  
Scale : AS NOTED  
Project Number : --  
Drawn by :

A2.3



EXTERIOR FINISHES:

- SID-1

HORIZONTAL HARDI-PANEL SIDING, PAINTED. PANEL HEIGHT: APPROX 1'-8". PANEL LENGTH 10'-0". STAGGER VERTICAL SEAMS. RUNNING BOND PATTERN. FRY REGLET SEAMS, TRANSITIONS. UNPAINTED ANODIZED ALUM.
- SID-2

PAINTED HORIZONTAL HARDI LAP SIDING. SMOOTH SIDE OUT, REVEAL 10 3/4". STAGGER JOINTS. PAINTABLE PVC 1X TRIM AT WINDOWS, CORNERS, FRIEZE AND BASE.
- SID-3

VERTICAL HARDI PANEL SIDING, PAINTED. PANEL WIDTH 1'-8". PROVIDE 1X3 PAINTED PVC BATTEN AT EACH VERTICAL SEAM AND ALONG RAKE AND BASE. PANELING TO BE PROUD OF SID-2 BY 1 1/2".
- SID-4

PAINTED HORIZONTAL HARDI LAP SIDING. SMOOTH SIDE OUT, REVEAL 7 1/4". STAGGER JOINTS. PAINTABLE PVC 1X TRIM AT WINDOWS, CORNERS, FRIEZE AND BASE.
- LAT-1

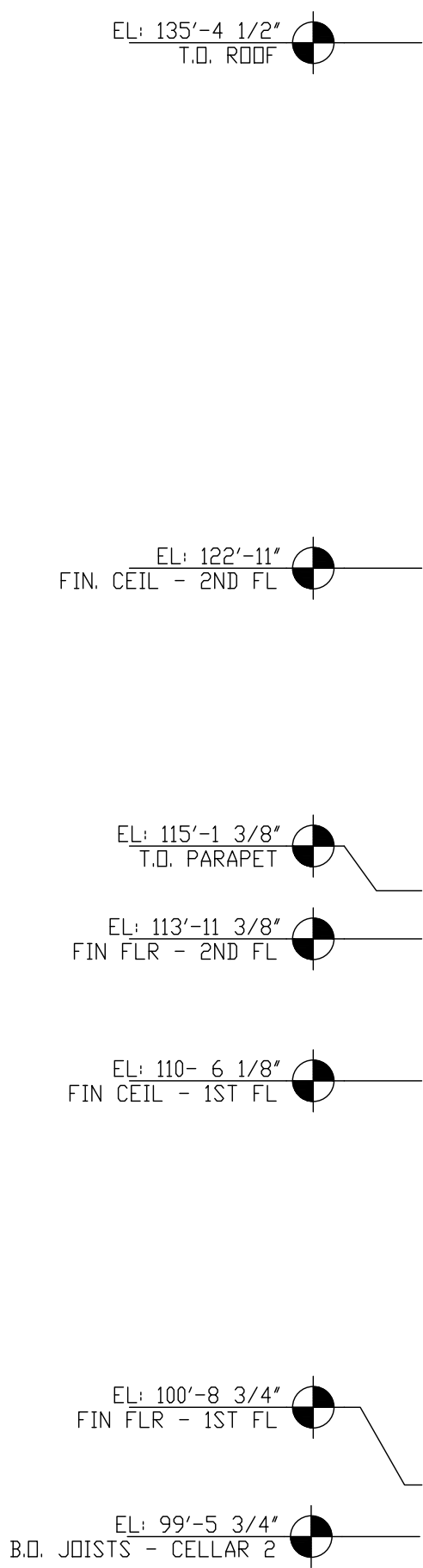
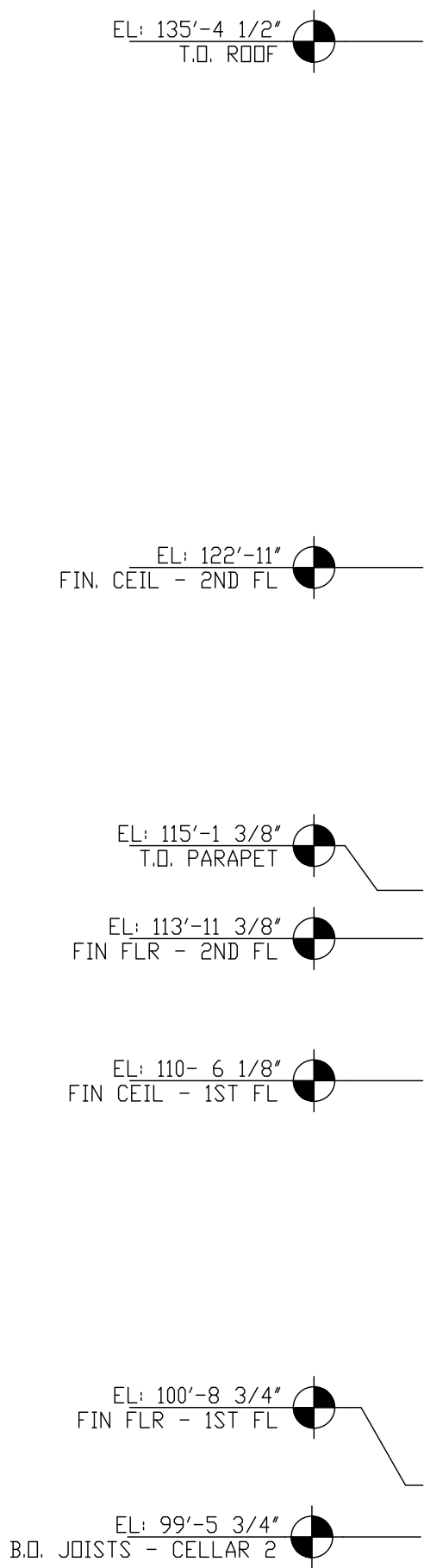
VERTICAL 1X4 WESTERN RED CEDAR BOARDS, 1/2" GAP BETWEEN BOARDS. WATER SEALED
- RF-1

30 YEAR ARCHITECTURAL GRADE ASPHALT SHINGLES
- RF-2

EPDM MEMBRANE, FULLY ADHERED TO RECOVERY BOARD PANELS. SLOPED AT 1/2":12 WITH TAPERED INSULATION PANELS
- CAN-1

-PAINTED CHANNEL STEEL FASCIA  
-LGMF ROOF STRUCTURE  
-3/4" FIRE RETARDANT PLYWOOD DECKING  
-RF-2 MEMBRANE  
-HARDIPANEL WITH BATTENS AT SOFFIT, PAINTED  
-4" PAINTED STEEL POSTS  
-GUT-1
- GUT-1

4"x5" ALUMINUM GUTTER AND DOWNSPOUTS



SPRINGBOARD SCHOOLS

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Revision : Date :

EXTERIOR ELEVATIONS

Date : 09-Dec-2019  
Scale : AS NOTED  
Project Number : --  
Drawn by :

A3.0

EXTERIOR FINISHES:

- SID-1

HORIZONTAL HARDI-PANEL SIDING, PAINTED. PANEL HEIGHT, APPROX. 1'-8". PANEL LENGTH 10'-0". STAGGER VERTICAL SEAMS. RUNNING BOND PATTERN. FRY REGLET SEAMS, TRANSITIONS. UNPAINTED ANODIZED ALUM.
- SID-2

PAINTED HORIZONTAL HARDI LAP SIDING, SMOOTH SIDE OUT, REVEAL 10 ¼", STAGGER JOINTS. PAINTABLE PVC 1X TRIM AT WINDOWS, CORNERS, FRIEZE AND BASE.
- SID-3

VERTICAL HARDI PANEL SIDING, PAINTED. PANEL WIDTH 1'-8". PROVIDE 1X3 PAINTED PVC BATTEN AT EACH VERTICAL SEAM AND ALONG RAKE AND BASE. PANELING TO BE PROUD OF SID-2 BY 1 ½".
- SID-4

PAINTED HORIZONTAL HARDI LAP SIDING, SMOOTH SIDE OUT, REVEAL 7 ½", STAGGER JOINTS. PAINTABLE PVC 1X TRIM AT WINDOWS, CORNERS, FRIEZE AND BASE.
- SID-5

PAINTED HORIZONTAL WOOD BOARDS TO MATCH INTERIOR WALLS OF PLAY AREA.
- LAT-1

VERTICAL 1X4 WESTERN RED CEDAR BOARDS, 1/2" GAP BETWEEN BOARDS. WATER SEALED
- RF-1

30 YEAR ARCHITECTURAL GRADE ASPHALT SHINGLES
- RF-2

EPDM MEMBRANE, FULLY ADHERED TO RECOVERY BOARD PANELS. SLOPED AT ¼":12 WITH TAPERED INSULATION PANELS
- CAN-1

-PAINTED CHANNEL STEEL FASCIA

-LOWE ROOF STRUCTURE

-¾" FIRE RETARDANT PLYWOOD DECKING

-RF-2 MEMBRANE

-HARDIPANEL WITH BATTENS AT SOFFIT, PAINTED

-4" PAINTED STEEL POSTS

-GUT-1
- GUT-1

4"x5" ALUMINUM GUTTER AND DOWNSPOUTS



- EL: 135'-4 1/2"

T.O. ROOF
- EL: 122'-11"

FIN. CEIL - 2ND FL
- EL: 115'-1 3/8"

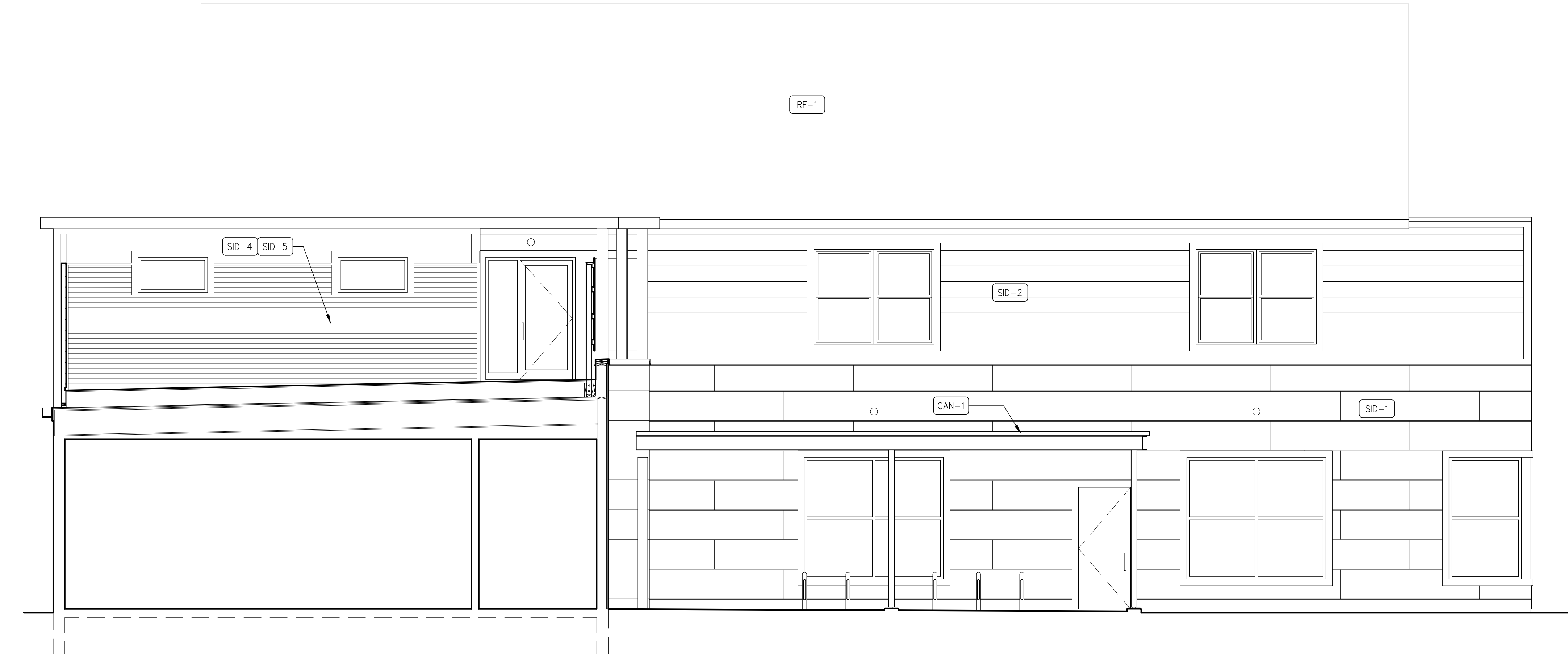
T.O. PARAPET
- EL: 113'-11 3/8"

FIN FLR - 2ND FL
- EL: 110'-6 1/8"

FIN CEIL - 1ST FL
- EL: 100'-8 3/4"

FIN FLR - 1ST FL
- EL: 99'-5 3/4"

B.O. JOISTS - CELLAR 2



- EL: 135'-4 1/2"

T.O. ROOF
- EL: 122'-11"

FIN. CEIL - 2ND FL
- EL: 115'-1 3/8"

T.O. PARAPET
- EL: 113'-11 3/8"

FIN FLR - 2ND FL
- EL: 110'-6 1/8"

FIN CEIL - 1ST FL
- EL: 100'-8 3/4"

FIN FLR - 1ST FL
- EL: 99'-5 3/4"

B.O. JOISTS - CELLAR 2

SPRINGBOARD SCHOOLS

87-89 BROADWAY ARLINGTON MA

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Revision : Date :

EXTERIOR ELEVATIONS

Date : 09-Dec-2019  
Scale : AS NOTED  
Project Number : --  
Drawn by :

A3.1









Y ? N

0	0	0	Location and Transportation		15
		X	Credit	LEED for Neighborhood Development Location	15
		X	Credit	Sensitive Land Protection	1
		X	Credit	High Priority Site	2
X			Credit	Surrounding Density and Diverse Uses	5
X			Credit	Access to Quality Transit	4
X			Credit	Bicycle Facilities	1
X			Credit	Reduced Parking Footprint	1
		X	Credit	Green Vehicles	1

0	0	0	Sustainable Sites	12
Y			Prereq Construction Activity Pollution Prevention	Required
Y			Prereq Environmental Site Assessment	Required
		X	Credit Site Assessment	1
		X	Credit Site Development - Protect or Restore Habitat	2
X			Credit Open Space	1
X			Credit Rainwater Management	3
		X	Credit Heat Island Reduction	2
		X	Credit Light Pollution Reduction	1
		X	Credit Site Master Plan	1
		X	Credit Joint Use of Facilities	1

0	0	0	Water Efficiency	12
Y			Prereq Outdoor Water Use Reduction	Required
Y			Prereq Indoor Water Use Reduction	Required
Y			Prereq Building-Level Water Metering	Required
	X		Credit Outdoor Water Use Reduction	2
X			Credit Indoor Water Use Reduction	7
		X	Credit Cooling Tower Water Use	2
		X	Credit Water Metering	1

0	0	0	Energy and Atmosphere	31
Y			Prereq Fundamental Commissioning and Verification	Required
Y			Prereq Minimum Energy Performance	Required
Y			Prereq Building-Level Energy Metering	Required
Y			Prereq Fundamental Refrigerant Management	Required
		X	Credit Enhanced Commissioning	6
		X	Credit Optimize Energy Performance	16
		X	Credit Advanced Energy Metering	1
		X	Credit Demand Response	2
		X	Credit Renewable Energy Production	3
		X	Credit Enhanced Refrigerant Management	1
		X	Credit Green Power and Carbon Offsets	2

Date: 1/21/2020

0	0	0	Materials and Resources		13
Y			Prereq	Storage and Collection of Recyclables	Required
Y			Prereq	Construction and Demolition Waste Management Planning	Required
		X	Credit	Building Life-Cycle Impact Reduction	5
		X	Credit	Building Product Disclosure and Optimization - Environmental Product Declarations	2
		X	Credit	Building Product Disclosure and Optimization - Sourcing of Raw Materials	2
		X	Credit	Building Product Disclosure and Optimization - Material Ingredients	2
		X	Credit	Construction and Demolition Waste Management	2

0	0	0	Indoor Environmental Quality	16
Y			Prereq Minimum Indoor Air Quality Performance	Required
Y			Prereq Environmental Tobacco Smoke Control	Required
Y			Prereq Minimum Acoustic Performance	Required
		X	Credit Enhanced Indoor Air Quality Strategies	2
X			Credit Low-Emitting Materials	3
		X	Credit Construction Indoor Air Quality Management Plan	1
		X	Credit Indoor Air Quality Assessment	2
X			Credit Thermal Comfort	1
X			Credit Interior Lighting	2
X			Credit Daylight	3
X			Credit Quality Views	1
X			Credit Acoustic Performance	1

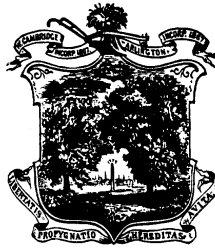
0	0	0	Innovation	6
		x	Credit Innovation	5
		x	Credit LEED Accredited Professional	1

0	0	0	Regional Priority		4
		X	Credit	Regional Priority: Specific Credit	1
		X	Credit	Regional Priority: Specific Credit	1
		X	Credit	Regional Priority: Specific Credit	1
		X	Credit	Regional Priority: Specific Credit	1

0	0	0	TOTALS	Possible Points:	33
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**Certified:** 40 to 49 points. **Silver:** 50 to 59 points. **Gold:** 60 to 79 points. **Platinum:** 80 to 110





**Town of Arlington, Massachusetts**  
Department of Planning & Community Development  
730 Massachusetts Avenue, Arlington, Massachusetts 02476

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## **Public Hearing Memorandum**

*The purpose of this memorandum is to provide the Arlington Redevelopment Board and public with technical information and a planning analysis to assist with the regulatory decision-making process.*

**To:** Arlington Redevelopment Board

**From:** Jennifer Raitt, Secretary Ex-Officio

**Subject:** Environmental Design Review, 93 Broadway, Arlington, MA  
Docket #3504

**Date:** January 28, 2020

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### **I. Docket Summary**

This is an application by Kevin Flynn for Springboard Schools, Inc., at 93 Broadway, Arlington, MA, 02476, to reopen Special Permit Docket #3504 in accordance with the provisions of MGL Chapter 40A § 11, and the Town of Arlington Zoning Bylaw Section 3.4, Environmental Design Review. The reopening of the Special Permit is for the Board to review an addition to the school on the adjacent vacant lot, and a request to amend the parking reduction under Section 6.1.5.

Springboard Schools has operated the Learn To Grow early childhood learning center at 93 Broadway since 2017. The project was the subject of Environmental Design Review (EDR) Special Permit Docket #3504 for which the ARB issued a Decision dated June 20, 2016. Springboard Schools applied for and the Board approved an EDR Special Permit for a mixed-use building at 87-89 Broadway in 2017. The mixed-use building was never constructed, and at this time the Applicant proposes to expand the school into the vacant lot at 87-89 Broadway.

Although Learn To Grow received an EDR Special Permit in 2016 for the school, early childhood learning centers are protected under MGL c. 40A Section 3. As such, the use is allowed by-right and is only subject to reasonable regulation including the bulk and height of structures and in determining yard sizes, lot area, setbacks, open space,

parking, and building coverage requirements. While the bylaw has since been amended by Town Meeting to correctly address how protected uses must be treated by law and the Board subsequently adopted a rule regarding reviews of “Dover uses”, since this property has an existing Special Permit authorizing the use, the early childhood learning center is returning to the Redevelopment Board for review.

The existing Learn To Grow early childhood learning center has 6 existing classrooms. During the original review, the applicant requested and received a reduction of the number of required parking spaces to allow 3 parking spaces. A Transportation Demand Management Plan was approved and put into place for the school in exchange for the reduction.

The proposed addition is nearly a mirror image of the existing Learn To Grow structure. Five new classrooms will be housed in the expanded school. Three new parking spaces will be added. Once the expansion is completed, the school will have 11 classrooms and 6 parking spaces, one of which is accessible.

The following is a zoning analysis for the property:

	<b>Requirement</b>	<b>Proposed</b>	<b>Notes</b>
Minimum Lot Area	None	17,552 sf	Includes 93 Broadway and 87-89 Broadway
Minimum Lot Frontage	50 ft	284 ft	Entire frontage on Broadway and North Union Street
Front Yard	None	0.3 ft / 7.5 ft	
Side Yard	None	4.5 ft	Side Yard is designated opposite of Broadway
Rear Yard	10 + (L/10) or 18.08 ft	18.1 ft	Rear Yard is designated opposite of North Union Street
Landscaped Open Space	1,636.5 sf	5,197 sf	
Usable Open Space	None	None	Usable open space is calculated off of the residential floor area. There is no in this proposal.
Maximum Height in Feet	35 ft	34.5 ft	
Maximum Height in Stories	2.5 stories	2.5 stories	
Floor Area Ratio	1.00	0.93	

Materials submitted for consideration of this application:

- Application for EDR Special Permit;

- Narrative dated December 13, 2019;
- Transportation Demand Management Plan dated December 14, 2019;
- LEED Checklist;
- Plan of Land, dated December 8, 2019; and
- Plan Set dated January 7, 2020, including
  - Title Page, Drawing Index, and Zoning Notes,
  - Code Review, Egress,
  - Site Plan,
  - Proposed Plans - Basement,
  - Proposed Plans – First Floor,
  - Proposed Plans – Second Floor,
  - Proposed Plans – Attic,
  - Proposed Exterior Elevations.

## II. Application of Special Permit Criteria (Arlington Zoning Bylaw, Section 3.3)

### 1. Section 3.3.3.A.

**The use requested is listed as a Special Permit in the use regulations for the applicable district or is so designated elsewhere in this Bylaw.**

Educational facilities are allowed by-right in any zoning district per MGL c. 40A Section 3. Since this property has an existing Special Permit authorizing the use, the early childhood learning center returns to the Redevelopment Board for review. The Board can find that this condition is met.

### 2. Section 3.3.3.B.

**The requested use is essential or desirable to the public convenience or welfare.**

The existing Learn To Grow facility provides a critical resource in the town. Childcare and early childhood learning are fundamental components of a livable community for many families. As noted in the application materials, being able to accommodate more students at the facility will be an attractive and much needed amenity to the town. While community planning and economic development initiatives often focus on housing, transportation, and employment; childcare is often the missing link. Learn to Grow is located in East Arlington where the center continues to make childcare and early childhood learning available to the community.

### 3. Section 3.3.3.C.

**The requested use will not create undue traffic congestion or unduly impair pedestrian safety.**

Learn To Grow has a single parking lot accessed from Broadway. The existing parking lot will be expanded to 6 parking spaces, including one accessible parking space. On a



highly trafficked portion of Broadway, the single curb cut reduces the number of potential conflicts between vehicles and pedestrians on the sidewalk.

The school also has an existing Transportation Demand Management Plan in place and its implementation has been successful. In the application materials, the applicant provides projected demand during drop-off and pick-up at the school. In most cases, the parking lot will be able to accommodate parents or guardians, and the available on-street parking on Broadway will be able to accommodate short-term parking during these periods. Outside of these periods, the parking lot can accommodate longer-term visits by families or other visitors to the school. As such, the expansion of the school will not create undue or exacerbate existing traffic congestion.

The Board can find that this condition is met.

**4. Section 3.3.3.D.**

**The requested use will not overload any public water, drainage or sewer system or any other municipal system to such an extent that the requested use or any developed use in the immediate area or in any other area of the Town will be unduly subjected to hazards affecting health, safety, or the general welfare.**

With the proposed addition, the school will expand onto the adjacent vacant lot at 87-89 Broadway, which is currently entirely impervious. Stormwater on this site generally flows toward the street and is not contained.

There will be an increase to pervious surfaces around the proposed addition due to the installation of lawn, landscaping, and play areas that will surround the proposed addition. Rather than sheet flow off of the site, stormwater will be reabsorbed on site. Additionally, the gutters and downspouts from the proposed addition will direct runoff to the existing on subsurface stormwater management system in order to further decrease stormwater runoff from the site.

No new utility connections are necessary, as the utilities will be connected through the existing building.

The Board can find that this condition is met.

**5. Section 3.3.3.E.**

**Any special regulations for the use as may be provided in the Bylaw are fulfilled.**

No special regulations are applicable to the proposal. The Board can find that this condition is met.

**6. Section 3.3.3.F.**

**The requested use will not impair the integrity or character of the district or adjoining districts, nor be detrimental to the health or welfare.**

The use does not impair the integrity or character of the neighborhood. The property at 87-89 Broadway has been vacant for more than 2 years since the Redevelopment Board issued its prior Decision. The expansion of the school onto this property enhances the services Learn To Grow provides to the community and will improve the streetscape.

The proposed addition creates an attractive and welcoming building. The façade design improvements are consistent with the Design Standards for the Town of Arlington. The proposed design of the structure is consistent with the standards for building height, the public realm interface, façade and materials, and signage and wayfinding.

The proposed project will contain play spaces at the perimeter of the new building. The use will not increase in intensity or duration from what is currently occurring. The classrooms all operate on slightly different schedules and efforts will be made to stagger outside time so as not to disrupt the neighborhood.

The Board can find that this condition is met.

**7. Section 3.3.3.G.**

**The requested use will not, by its addition to a neighborhood, cause an excess of the use that could be detrimental to the character of said neighborhood.**

The use will not be in excess or detrimental to the character of the neighborhood. The Board can find that this condition is met.

**III. Environmental Design Review Standards (Arlington Zoning Bylaw, Section 3.4)**

**1. EDR-1 Preservation of Landscape**

**The landscape shall be preserved in its natural state, insofar as practicable, by minimizing tree and soil removal, and any grade changes shall be in keeping with the general appearance of neighboring developed areas.**

The existing site condition is primarily impervious and there is no vegetation to preserve. Landscaped areas and buffers will be added around the addition, and be maintained by the Applicant regularly. The Board can find that this condition is met.

**2. EDR-2 Relation of the Building to the Environment**

**Proposed development shall be related harmoniously to the terrain and to the use, scale, and architecture of the existing buildings in the vicinity that have functional or visible relationship to the proposed buildings. The Arlington Redevelopment Board may require a modification in massing so as to reduce the effect of shadows on the abutting property in an R0, R1 or R2 district or on public open space.**

This proposal does not detract from the district's business character or the adjacent residential uses. The proposed addition will mirror the existing building at 93 Broadway, and will provide a welcoming appearance to the school. The proposed addition utilizes a vacant lot.

The scale and architecture of the building is consistent with the surrounding area, where the first floor of the existing structure and the proposed addition have a commercial feel, while the upper story relates to the residential buildings. The long connector hallway between the two main structures will not be much longer than it is today. There may be an opportunity to continue the red accent of the canopies across the structure to enliven the appearance.

The Board can find that this condition is met.

**3. EDR-3 Open Space**

**All open space (landscaped and usable) shall be so designed as to add to the visual amenities of the vicinity by maximizing its visibility for persons passing by the site or overlooking it from nearby properties. The location and configuration of usable open space shall be so designed as to encourage social interaction, maximize its utility and facilitate maintenance.**

Exterior space around the addition will be used as an outdoor play area on the property. It will be surrounded by a buffer of evergreen plantings. A wood fence will also be installed around the rear, side, and partially along Broadway to provide some privacy for the play space. The Board can find that this condition is met.

**4. EDR-4 Circulation**

**With respect to vehicular and pedestrian and bicycle circulation, including entrances, ramps, walkways, drives, and parking, special attention shall be given to location and number of access points to the public streets (especially in relation to existing traffic controls and mass transit facilities), width of interior drives and access points, general interior circulation, separation of pedestrian and vehicular traffic, access to community facilities, and arrangement of vehicle parking and bicycle parking areas, including bicycle parking spaces required by Section 6.1.12 that are safe and convenient and, insofar as practicable, do not detract from the use and enjoyment of proposed buildings and structures and the neighboring properties.**

The application materials indicate that 3 additional parking spaces will be provided on the site, for a total of 6 parking spaces. The applicant has requested a parking reduction under Section 6.1.5:

Parking Requirement			
		Zoning Requirement	Total Parking Required
Classrooms	11 rooms	2 spaces per	21 spaces

	classroom	
<b>Section 6.1.5 Reduction</b>		Up to 25% of the requirement, or 6 spaces
<b>Total Parking Provided</b>		6 spaces

Three parking spaces currently exist on the site, and 3 more will be added leading from the existing curb cut. This will result in the loss of some landscaped area, but overall, landscaped area will increase due to the new landscaped and lawn areas at 87-89 Broadway.

The proposed project is also in compliance with the recently adopted bicycle parking amendments. For the school in its entirety, 1.70 short-term spaces are required per classroom, or 19 spaces are required. There are currently 12 bicycle parking spaces under the canopy at the existing school and the applicant proposes to install another 12 spaces for a total of 24 short-term bicycle parking spaces. Per the regulations, 0.30 long-term bicycle parking spaces are required per classroom for a total of 4 long-term bicycle parking spaces. The school will create bicycle storage in the basement which is accessible by elevator. Additionally, per Section 6.1.12.H(1), the ARB may allow 4 short-term parking spaces to satisfy the long-term bicycle parking requirements. There are more than adequate short-term bicycle parking spaces to satisfy this allowance.

Learn To Grow also updated the Transportation Demand Management Plan per Section 6.1.5. of the Zoning Bylaw to account for the increase in the number of classrooms. Overall, Learn To Grow reports success in encouraging families and employees to choose alternative modes to arrive at the school, including the covered bicycle parking and general communication with the parents and guardians of the students. There are also incentives provided to families who choose not to drive. For those families that choose to or need to drive to the school, Learn To Grow has projected the number of vehicles that would be parked onsite, and determines that the 6 parking spaces will be able to accommodate the timed drop-offs and pick-ups. If there is an instance where the parking lot is full, on-street parking on Broadway is available. Learn To Grow also reports that since the school has been open, the 3 parking spaces have been adequate.

Learn to Grow will have 29 teachers at the expanded school. The school projects that 11 will drive, 8 will walk or use a bicycle, and 10 will use public transportation. Learn To Grow provides a 50% subsidy of transit passes for employees and covered bicycle parking as well as the ability to bring bicycles into the school.

Overall, it appears that the school is committed to encouraging the use of alternative modes of transportation beyond a personal vehicle for staff and families. Learn to Grow has a specific plan in place to ensure that the 6 parking spaces will meet the needs of the school community and the surrounding neighborhood. The Board can find that this condition is met.



**5. EDR-5 Surface Water Drainage**

Special attention shall be given to proper site surface drainage so that removal of surface waters will not adversely affect neighboring properties or the public storm drainage system. Available Best Management Practices for the site should be employed, and include site planning to minimize impervious surface and reduce clearing and re-grading. Best Management Practices may include erosion control and stormwater treatment by means of swales, filters, plantings, roof gardens, native vegetation, and leaching catch basins. Stormwater should be treated at least minimally on the development site; that which cannot be handled on site shall be removed from all roofs, canopies, paved and pooling areas and carried away in an underground drainage system. Surface water in all paved areas shall be collected in intervals so that it will not obstruct the flow of vehicular or pedestrian traffic and will not create puddles in the paved areas.

In accordance with Section 3.3.4., the Board may require from any applicant, after consultation with the Director of Public Works, security satisfactory to the Board to insure the maintenance of all stormwater facilities such as catch basins, leaching catch basins, detention basins, swales, etc. within the site. The Board may use funds provided by such security to conduct maintenance that the applicant fails to do.

The Board may adjust in its sole discretion the amount and type of financial security such that it is satisfied that the amount is sufficient to provide for any future maintenance needs.

Learn To Grow will expand into the vacant and impervious lot at 87-89 Broadway. There are no stormwater controls on the vacant site, and stormwater sheet flows from the site into the road. With the proposed addition, some impervious areas will be removed from the property and replaced with lawn and landscaping where stormwater can be reabsorbed on site. Additionally, the gutters and downspouts on the addition will direct stormwater from the roof into the existing subsurface stormwater management system located under the parking area. The Board can find that this condition is met.

**6. EDR-6 Utilities Service**

Electric, telephone, cable TV, and other such lines of equipment shall be underground. The proposed method of sanitary sewage disposal and solid waste disposal from all buildings shall be indicated.

No new utility connections will be made, as the existing utility connections will be extended into the addition to the school. The Board can find that this condition is met.

**7. EDR-7 Advertising Features**

**The size, location, design, color, texture, lighting and materials of all permanent signs and outdoor advertising structures or features shall not detract from the use and enjoyment of proposed buildings and structures and the surrounding properties.**

There are no changes to the existing signage as a result of the proposed addition. The Board can find that this condition is met.

**8. EDR-8 Special Features**

**Exposed storage areas, exposed machinery installations, service areas, truck loading areas, utility buildings and structures, and similar accessory areas and structures shall be subject to such setbacks, screen plantings or other screening methods as shall reasonably be required to prevent their being incongruous with the existing or contemplated environment and the surrounding properties.**

The existing service area is located off of North Union Street behind the existing Learn To Grow structure. This area will continue to serve the expanded school and will be screened from public view or access. The Board can find that this condition is met.

**9. EDR-9 Safety**

**With respect to personal safety, all open and enclosed spaces shall be designed to facilitate building evacuation and maximize accessibility by fire, police and other emergency personnel and equipment. Insofar as practicable, all exterior spaces and interior public and semi-public spaces shall be so designed to minimize the fear and probability of personal harm or injury by increasing the potential surveillance by neighboring residents and passersby of any accident or attempted criminal act.**

Safety will be improved within the building and around the project site through the addition of an egress stair from the basement. While there is no information in the application materials regarding exterior lighting, it is understood that the entrances and walkways will be well-lit. The Board can find that this condition is met.

**10. EDR-10 Heritage**

**With respect to Arlington's heritage, removal or disruption of historic, traditional or significant uses, structures or architectural elements shall be minimized insofar as practical whether these exist on the site or on adjacent properties.**

The existing structure is not listed on the *Inventory of Historically or Architecturally Significant Properties in the Town of Arlington* nor is it under the jurisdiction of the Arlington Historical Commission. The Board can find that this condition is met.

**11. EDR-11 Microclimate**

**With respect to the localized climatic characteristics of a given area, any development which proposes new structures, new hard surface, ground coverage or the installation of machinery which emits heat, vapor or fumes shall endeavor to**

**minimize insofar as practicable, any adverse impacts on light, air and water resources or on noise and temperature levels of the immediate environment.**

There are no proposed changes (new structures, hard surface, ground coverage, or machinery) that will impact the microclimate. The Board can find that this condition is met.

**12. EDR-12 Sustainable Building and Site Design**

**Projects are encouraged to incorporate best practices related to sustainable sites, water efficiency, energy and atmosphere, materials and resources, and indoor environmental quality. Applicants must submit a current Green Building Council Leadership in Energy and Environmental Design (LEED) checklist, appropriate to the type of development, annotated with narrative description that indicates how the LEED performance objectives will be incorporated into the project.**

A LEED Checklist is provided. The project will include Energy Star fixtures and will include high R-value insulation. The Board can find that this condition is met.

**IV. Conditions**

1. The final design, façade improvements, landscaping, fencing, and lighting plans shall be subject to the approval of the Arlington Redevelopment Board. Any substantial or material deviation during construction from the approved plans and specifications is subject to the written approval of the Arlington Redevelopment Board.
2. Any substantial or material deviation during construction from the approved plans and specifications is subject to the written approval of the Arlington Redevelopment Board.
3. The Board maintains continuing jurisdiction over this permit and may, after a duly advertised public hearing, attach other conditions or modify these conditions as it deems appropriate in order to protect the public interest and welfare.
4. Snow removal from all parts of the site, as well as from any abutting public sidewalks, shall be the responsibility of the owner and shall be accomplished in accordance with Town Bylaws.
5. Trash shall be picked up only on Monday through Friday between the hours of 7:00 am and 6:00 pm. All exterior trash and storage areas on the property, if any, shall be properly screened and maintained in accordance with the Town Bylaws.
6. The Applicant shall provide a statement from the Town Engineer that all proposed utility services have adequate capacity to serve the school. The

Applicant shall provide evidence that a final plan for drainage and surface water removal has been reviewed and approved by the Town Engineer.

7. Upon installation of landscaping materials and other site improvements, the Applicant shall remain responsible for such materials and improvement and shall replace and repair as necessary to remain in compliance with the approved site plan.
8. Upon the issuance of the building permit the Applicant shall file with the Inspectional Services Department and the Police Department the names and telephone numbers of contact personnel who may be reached 24 hours each day during the construction period.





## Town of Arlington, Massachusetts

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### Debrief and follow-up from joint meeting with Select Board on January 13, 2020

#### Summary:

8:35 p.m. -  
8:45 p.m. • Board will debrief meeting and discuss next steps.

#### ATTACHMENTS:

Type	File Name	Description
Reference Material	Agenda_Item_4_a- _Updated_Memo_regarding_proposed_warrant_article_review_process_for_SB_ARB_DRAFT.pdf	Updated Memo regarding proposed warrant article review process for SB ARB DRAFT
Reference Material	Agenda_Item_4_b_-_STM_2021_Engagement_Schedule_Revised_01-22-20.pdf	STM 2021 Engagement Schedule Revised 01- 22-20



**TOWN OF ARLINGTON**  
DEPARTMENT OF PLANNING and  
COMMUNITY DEVELOPMENT

TOWN HALL, 730 MASSACHUSETTS AVENUE  
ARLINGTON, MASSACHUSETTS 02476  
TELEPHONE 781-316-3090

**MEMORANDUM**

Date: January 21, 2020

To: Adam Chapdelaine, Town Manager

From: Jennifer Raitt, Director, Planning and Community Development

RE: Proposed Warrant Article – Redevelopment and Select Board Town Meeting review process updated per joint Board discussion on January 13, 2020

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As part of our discussion on Monday, January 13, 2020, the Boards agreed to jointly review and discuss Town Meeting warrant articles of mutual interest to establish a framework for those reviews. The following process was discussed:

<b>Timeframe</b>	<b>Actions</b>
<b>January</b>	Warrant Articles filed.
<b>February</b>	Chair of the Redevelopment Board, Chair of the Select Board, Town Manager, Director of Planning and Community Development and Town Counsel discuss all Warrant Articles as filed and recommend which articles their respective Boards will review.
<b>February through April</b>	Board review of articles.
<b>April</b>	Board issues written comments on any Articles reviewed and votes on recommended actions.
<b>April</b>	Joint meeting to further discuss articles prior to filing their respective reports to Town Meeting, as needed.
<b>April – May</b>	Annual Town Meeting
<b>July</b>	Joint Board goal setting meeting and timetable for moving forward with goals.
<b>Fall</b>	Warrant Article consideration.
<b>Fall</b>	Joint Board meeting to provide update on goals, status, and plan for Town Meeting.

STM 2021 Engagment Schedule

Intent/notes	2020												2021	
	JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC	JAN	FEB
<b>1 Engagement Awareness (Inform)</b>														
1.1 Joint Select Board/ARB meeting in order to define the need and consider options as solutions	X													
1.2 Publicize summary document outlining all opportunities for input	X													
1.3 Design materials for phase 1	X													
<b>2 Broad open engagement</b>														
2.1 Open ended campaign to solicit residents opinions and ideas about housing in Arlington		X	X	X	X									
2.2 Community workshops / open houses			X	X	X									
2.3 Interdepartmental/Board and Committee engagement		X	X	X	X									
2.4 Town Survey	X	X	X	X										
2.5 Prep materials for phase 2				X										
Needs: facilitation guides, data, FAQ, engagement plan with meeting dates, presentations for community workshops														
<b>3 Feedback / Comment Period</b>														
3.1 Present what was heard in Phase 1 and the information we gathered back to the community / public meetings					X									
3.2 Survey / online comment box					X	X								
3.3 Postcard Campaign for soliciting input / feedback					X	X								
3.4 Finalize and promote engagement summary / what we heard in phase 2						X								
3.5 Interdepartmental/Board and Committee engagement					X	X								
3.6 Select Board and Redevelopment Board joint meeting to discuss goal setting							X							
Needs: presentation for public meetings, survey, comment cards, FAQ														
<b>4 Policy Development</b>														
4.1 Develop recommendations for policy changes based on engagement and data							X	X	X	X	X			
4.2 Focused ARB / Select Board meetings depending on policy proposals							X	X	X	X	X			
4.3 Community workshops / open houses, including a form with developers									X	X	X			
4.4 Interdepartmental/Board and Committee engagement							X	X	X	X	X			
4.5 Select Board and Redevelopment Board joint meeting to provide update on goals, status, and plan for Town Meeting										X	X			
<b>5 Town Meeting Preparation</b>														
5.1 Warrant Submission												X		
5.2 Select Board / ARB Hearings depending on the warrant article submissions												X	X	
5.3 Proposed Town Meeting (February 2021)														X

\*Please note that all meetings listed above will be open to the public.



## Town of Arlington, Massachusetts

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### Proposed Town Committee on Residential Development

#### Summary:

8:45 p.m. - • Board will discuss and may wish to submit correspondence to Select Board regarding  
9:00 p.m. proposal

#### ATTACHMENTS:

Type	File Name	Description
Reference Material	Agenda_Item_5a_-_Memo_regarding_review_of_Warrant_Article.pdf	Memo regarding review of Warrant Article
Reference Material	Agenda_Item_5b_-_Warrant_Article_to_Establish_Town_Committee_on_Residential_Development.pdf	Warrant Article to Establish Town Committee on Residential Development



**TOWN OF ARLINGTON**  
DEPARTMENT OF PLANNING and  
COMMUNITY DEVELOPMENT

TOWN HALL, 730 MASSACHUSETTS AVENUE  
ARLINGTON, MASSACHUSETTS 02476  
TELEPHONE 781-316-3090

**MEMORANDUM**

Date: January 31, 2020

To: Redevelopment Board members

From: Jennifer Raitt, Director of Planning and Community Development

RE: Proposed Town Committee on Residential Development

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We received notification of the Warrant Article proposing the establishment of a Town Committee on Residential Development, a 10 registered voter article. Please see attached document received from the Select Board's office. We have been asked to provide any comments by February 5. The Select Board begins Warrant Article hearings on February 10.



## **Warrant Article Submission Form**

### **PETITION OF TEN REGISTERED VOTERS FOR INSERTION OF ARTICLE INTO THE WARRANT FOR THE ANNUAL (SPECIAL) TOWN MEETING.**

**File Completed Form with the Board of Selectmen's Office no later than Friday, January 31, 2020, 12:00/Noon.**

We, the undersigned registered voters (10 for Annual, 100 for Special) of the Town of Arlington, hereby petition the Board of Selectmen pursuant to MGL c.. 39, § 10 to insert the following article(s) into the warrant for the Annual (Special) Town Meeting:

#### **Establishment of Town Committee on Residential Development**

To see if the Town will vote to establish a Committee of the Town Meeting to examine the current issues related to residential development in the R0, R1, R2, R3, and R4 zoning districts. These issues shall include, but not be limited to, effects on the neighborhood during teardown and construction activities; new construction impacts on neighborhood character/environment; permanent negative impacts on abutters; loss of healthy tree canopy; and exacerbation of particular negative impacts on abutters with existing non-conforming lots.

Members of the Committee shall be appointed by the Town Moderator, and consist of not less than 5 nor more than 9 members. A quorum shall consist of a majority of the appointed members. Members of the Committee shall be residents of the Town, a majority of whom shall be persons who are not Town employees and/or have no financial interest (either directly or indirectly) in the development, building, or real estate communities.

The Committee's objectives will include development of recommendations for proposing new zoning and general bylaws and/or amendments, as applicable, to provide long-term mitigation of the significant issues identified; or take any action related thereto.

Requested by:

Paul Parise

(Name one person who will be the contact individual for this article and will serve as the person for upcoming hearings regarding this article – Paul Parise)

Address: 106 Hemlock St. Arlington, MA 02474

Telephone: 617-835-5616

Email: paul456x@gmail.com



## Town of Arlington, Massachusetts

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### Lease Extension for Retirement Board

#### Summary:

9:05 p.m. -

- Board members will review and may approve lease extension

9:15 p.m.

#### ATTACHMENTS:

Type	File Name	Description
<input type="checkbox"/> Reference Material	Agenda_Item_7_-_Retirement_Board_Lease_Extension_2020_01-29-20.pdf	Retirement Board Lease Extension 2020 01-29-20

**AGREEMENT FOR  
EXTENSION OF LEASE TERM**

This Agreement for Extension of Lease Term ("Agreement") is made as of the \_\_\_\_\_ day of February, 2020 by and between the TOWN OF ARLINGTON ("Landlord") by the Arlington Redevelopment Board, and ARLINGTON RETIREMENT BOARD ("Tenant").

WHEREAS, the Landlord has leased to Tenant the Ground Floor Right Office, of the building known as the Central School located at 20 Academy Street, Arlington, Middlesex County, Massachusetts ("Demised Premises") under Lease dated as of July 1, 2016, ("Lease") and amended on July 1, 2019;

WHEREAS, the Landlord and Tenant are desirous of extending the Term of the Lease until 11:59 P.M. on December 31, 2019;

WHEREAS, the Landlord has leased to Tenant, 400 square feet in Suite 202B, of the building known as the Central School located at 20 Academy Street, Arlington, Middlesex County, Massachusetts ("Demised Premises") under Lease dated as of July 1, 2016, ("Lease"), beginning on March 1, 2020;

WHEREAS, the Landlord and Tenant are desirous of extending the Term of the Lease until 11:59 P.M. on June 30, 2020;

NOW THEREFORE, for the Ground Floor Right suite shall be rented for ONE-THOUSAND THREE-HUNDRED SIXTY-THREE DOLLARS AND NINETEEN CENTS (\$1,363.19) per month and Suite 202B shall be

rented for SEVEN-HUNDRED FIFTY-FIVE DOLLARS (\$755.00) per month and for other good and valuable consideration paid, the receipt and sufficiency of which is hereby acknowledged, the Landlord and Tenant hereby agree as follows:

1. Any capital or cosmetic improvements made by the tenant shall be reviewed and approved prior to carrying out work in Suite 202B.
2. The full Term of the Lease is hereby extended and shall expire at 11:59 P.M. on June 30, 2020, unless the Term shall sooner terminate pursuant to the provisions of the Lease.
3. All other terms and provisions of the Lease are to remain in full force and effect and are unmodified by the provisions hereof.

IN WITNESS WHEREOF, Landlord and Tenant have executed, sealed and delivered this Agreement as of the day and year first above written.

WITNESS TO SIGNATURES: \_\_\_\_\_

\_\_\_\_\_  
By: Town of Arlington  
Arlington Redevelopment Board

\_\_\_\_\_  
By: Arlington Retirement Board



## Town of Arlington, Massachusetts

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### Final Broadway Corridor Report

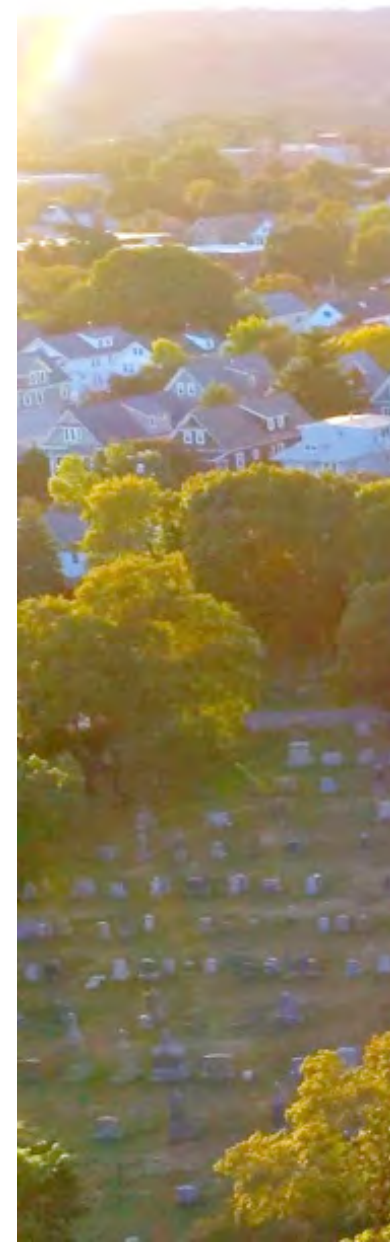
#### Summary:

9:15 p.m. - • Staff will provide final report  
9:20 p.m.

#### ATTACHMENTS:

Type	File Name	Description
Reference Agenda_Item_8_- Material	_Building_a_Better_Broadway_Planning_Study_for_the_Broadway_Corridor_2019_MIT_DUSP.pdf	Building a Better Broadway Planning Study for the Broadway Corridor 2019 MIT DUSP





# Building a Better Broadway

Planning Study for the Broadway Corridor, Arlington, MA







Prepared for the Town of Arlington Department of Planning & Community Development as part of MIT's practicum course 11.360 - Community Growth and Land Use Planning, Fall 2019.

Presented to:

The Arlington Redevelopment Board

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Massachusetts Institute of Technology  
School of Architecture + Planning  
Department of Urban Studies and Planning  
Cambridge, MA

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Kendrick Manymules  
Ian Ollis  
Paulo César Alvarado Pérez  
Mary Hannah Smith



## Acknowledgments

We would like to share our heartfelt gratitude to everyone who lent their time and effort to the creation of this study. In particular, we would like to extend a special thanks to the following:

### **Department of Planning & Community Development, Town of Arlington**

Jennifer Raitt, Director

Erin Zwirko, Assistant Director

The Arlington Redevelopment Board

### **Community members who participated in our public engagement sessions**

### **Local business owners and employees**

---

Unless otherwise noted, all figures are credited to the U.S. Census American Community Survey (2017).





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# OVERVIEW

In September 2019, a team of ten students from MIT's Department of Urban Studies and Planning began a semester-long study of the Broadway Corridor, an area of East Arlington that had yet to benefit from an in-depth planning study. This report summarizes our analysis, the views we heard from community members, and our recommendations for improving mobility, housing and neighborhood character along the corridor.

The report is structured in several sections. We begin with an introduction that outlines our understanding of the town's history, and mentions demographic and environmental characteristics of the corridor that have influenced our analysis and recommendations. The introduction also explains our approach to community engagement for this project and cites some of the opinions that we heard about what town residents would like to see along Broadway in the future. We close the introduction by presenting the three study goals that shape the report's recommendations: Safety & Walkability; Housing Affordability & Variety; Vibrance & Quality of Life.

The body of the report focuses on our analysis and recommendations for mobility, housing and neighborhood character along Broadway. Each concludes with a vision for a specific focus area. The mobility section provides a vision for a new Broadway streetscape that includes bike lines on both sides of the street, and leaves room for more greenery and pedestrians. The housing section reimagines the Lahey Building and parts of Sunnyside Avenue as the site of a new mixed-use development that incorporates housing, retail and commercial space. Finally, the neighborhood character section proposes an urban design concept that pulls the public from Broadway into Lussiano Field, and highlights the potential for that site to become a community gathering space for the entire neighborhood.



Figure 1. Three sections of our report conclude with a vision for the future of Broadway at specific focus areas: Lussiano Field, the Broadway Streetscape, and the Lahey Building site.



# I. PLANNING CONTEXT



# INTRODUCTION

The Town of Arlington, through their Department of Planning and Community Development (DPCD), invited graduate students from MIT's Department of Urban Studies and Planning (DUSP) to conduct a neighborhood study of the Broadway corridor. As the culmination of a semester-long course titled Community Growth and Land Use Planning, this document presents student findings developed through conversations with residents, site visits, and additional quantitative and qualitative research.

The Broadway corridor features in a number of Town planning documents, including the 2015 Master Plan, the 2017 Arts & Culture Action Plan, and the Arlington Complete Streets Policy. However, the Broadway corridor has not yet been the focus of its own planning study. As part of the Town's efforts to focus more specifically on the needs of residents and businesses along Broadway, this report aims to provide a foundational understanding of current conditions on the corridor as well as to identify high-level actions the Town could take to enhance Broadway while furthering the Town's overarching planning goals.

In developing this document, we have sought to align the historic legacy of the neighborhood with potential future changes, while respecting the distinct residential character that the town holds dear. This report uses a land use perspective to analyze demographic and development trends, mobility issues, as well as less-tangible aspects of planning, while recognizing the needs and desires declared by residents and users of the neighborhood. We believe that incorporating the findings

of this study into the vision of Arlington can provide equitable benefits to all residents, present and future.

## **ABOUT ARLINGTON**

Known by many previous names, the pre-European settlement of the lands now encompassing Arlington were inhabited by the Massachusetts tribe, a member of the larger Algonquin community. Widowed and facing disease ravaging her community, the 'Squaw Sachem of Mistick' deeded much of the Massachusetts' tribal lands to English colonists in 1635. Taking form as a farming village of Cambridge, which borrowed the native place-name 'Menotomy,' the northwest precinct eventually split off and became West Cambridge in 1807, and was renamed Arlington in 1867 in honor of the Arlington National Cemetery.

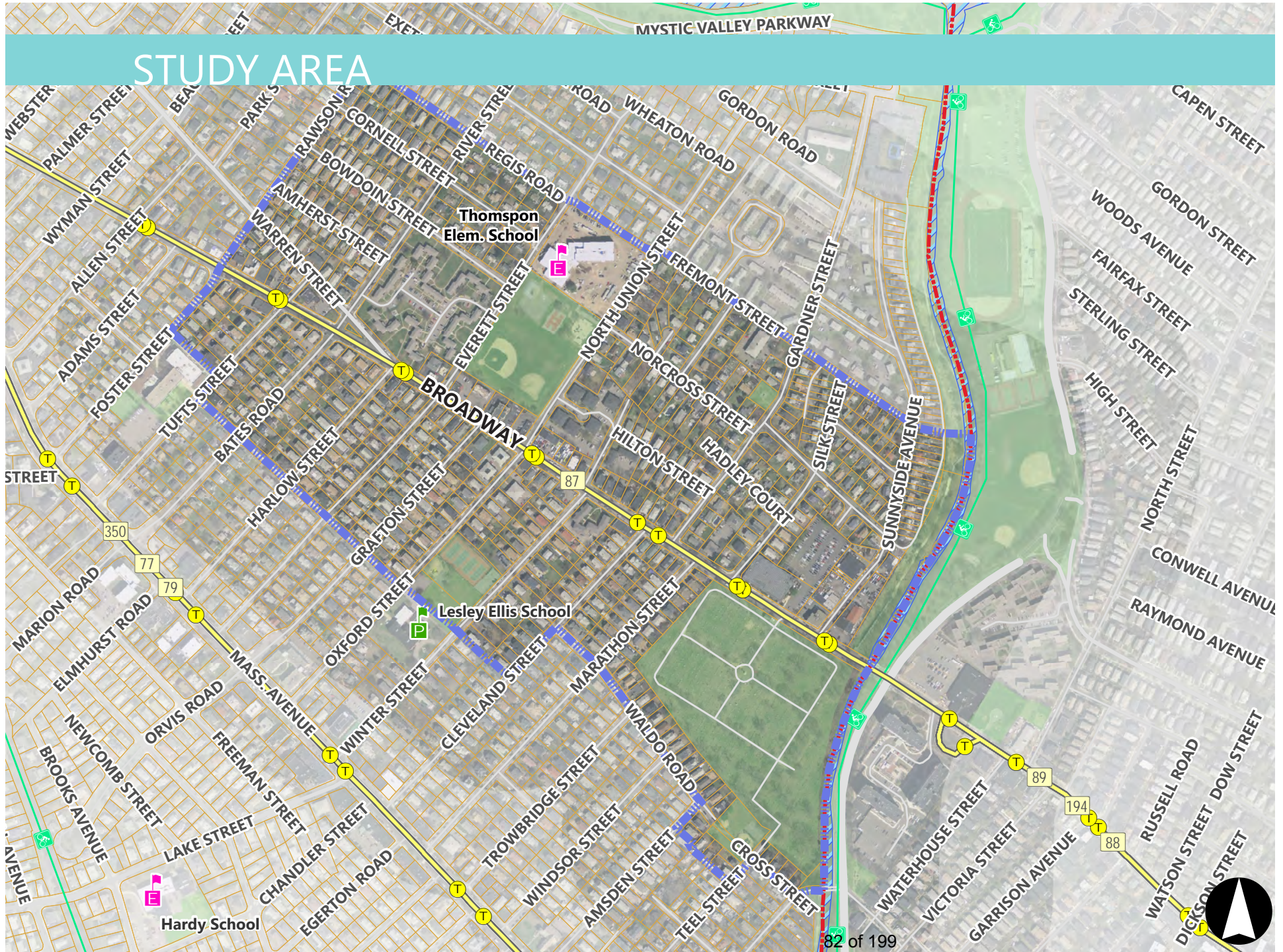
Arlington is now a predominantly residential 'streetcar suburb' of nearby Boston comprised of approximately 45,000 residents living within 5.5 square miles, making it among the most densely populated towns in Massachusetts.<sup>1</sup> Lying six miles northwest of the state capital, the town is bordered by Cambridge, Somerville, Medford, Winchester, Lexington and Belmont. Defined by the civic spirit that helped spark the American Revolution, Arlington takes a particularly New England approach to local governance, issuing an annual Warrant for Town Meeting where 252 elected representatives vote on the year's proposed Articles.

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<sup>1</sup> Metropolitan Area Planning Council, 2008.



# STUDY AREA



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Despite its proximity to the technological hubs of Boston and Cambridge, Arlington remains distinctly town oriented in many approaches to its daily workings. Rejecting a proposed terminus of the MBTA's Red Line into Arlington Center in the 1980's with slogans such as "128 or nothing" helped insulate the town from denser development, preserving the organic nature of some pre-zoning development. However, as Arlington and Greater Boston continue to grow, residential growth and mixed-use development have become more pressing issues, and have been the subject of heavily-debated Articles in recent years' Town Meetings.

The Broadway corridor ("the study area") extends generally southeast from Arlington Center, ending at the border with Somerville along Alewife Brook Parkway. As with most commercial corridors, the density of homes and businesses along Broadway is higher than the single- and two-family residential uses in the surrounding neighborhood. Public lands and open spaces are interspersed throughout the study area, including the Alewife Brook Greenway, Lussiano Field, and Crosby Park. In addition, the neighborhood is home to multiple schools, including the Thompson Elementary School, the Gibbs School, and the Lesley Ellis School.

Much of the zoning along Broadway reflects a patchwork of different historical land uses, not all of which have kept pace with the current needs of residents. For example, vehicular-oriented zoning dominates much of the study area, which is partially responsible for the large number of parking lots and auto repair shops along the corridor.

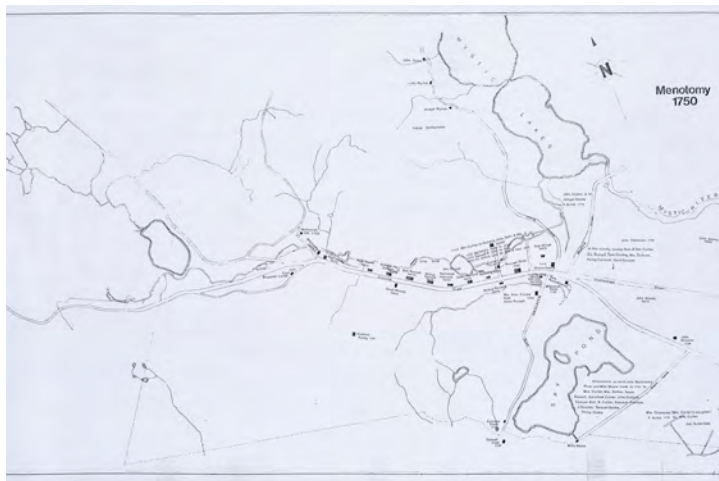


Figure 2. A 1750 map of Menotomy.  
Source: Digital Commonwealth

The Broadway corridor, like much of Arlington, is experiencing demographic changes. For example, the share of non-white residents in the study area has increased from 18% in 2010 to 25% in 2017. Similarly, the share of foreign-born residents has climbed from 19% of the corridor's population in 2010 to 25% in 2017. Larger families are the exception as opposed to the rule around the corridor, as roughly 71% of residential units are occupied by 1- or 2-person households, and the majority of those are renters. As a result of this trend, age cohorts in the corridor have been bifurcating, with increasing shares of children and aging Arlington residents. Residents aging in place, as is the tendency regionally, can help explain some of the trends seen in this demographic data.



Figure 3. Broadway is near amenities such as the Minuteman Bikeway.  
Source: Flickr.

## East Arlington Environmental Challenges

East Arlington, including the study area, suffers from localized heat islands, meaning that uncovered surfaces may be much hotter than the neighborhood's air temperature.<sup>1</sup> The area's thin tree canopy combined with high amounts of impermeable surfaces exacerbates the public health impacts of warm weather by making it more difficult for residents to stay cool.<sup>2</sup> The relatively sparse tree canopy of East Arlington was further cleared by recent severe weather events and has yet to fully recover.

Connected to the local permeability issues, and compounded by the low-lying topography of the corridor, freshwater flooding after rainstorms has been reported by some residents along Alewife Brook. The current floodplain along the Mystic River and Alewife Brook may shift due to the impacts of climate change, making more structures vulnerable to flooding.<sup>3</sup> The brook itself is susceptible to contamination from pollutants in storm water, which damages watershed environments. In July 2019, the town received a Coastal Zone Management grant to construct bioretention basins and infiltration trenches along Alewife Brook (south of Mass Ave) to mitigate aquatic pollution. It is also targeting a decrease in town-wide impervious surface coverage to improve pollutant filtering.<sup>4</sup>

1 US EPA, "Learn About Heat Islands."

2 MA Climate Change Clearinghouse, "Rising Temperatures."

3 Town of Arlington, "Community Resilience Building Workshop: Summary of Findings."

4 "Notice of Intent for Coverage for Small MS4 General Permit."

# BUILDING A VISION

Our process to learn about the Broadway corridor drew on a combination of outreach to residents and businesses in the neighborhood, site visits, conversations with town committees and staff members, and additional quantitative and qualitative research. To meet different members of the community, we designed different forms of both general public outreach and targeted outreach to specific groups. Our aim was to develop a better understanding of what people thought were the strengths of the neighborhood, as well as what changes they might like to see. We then used the ideas we heard from community members to generate a set of goals for the study which helped shape the recommendations we have included in this report.

In addition to community outreach, our group made several site visits to Broadway to observe and discuss the current conditions in the neighborhood. We also conducted additional research using information from the U.S. Census and other sources to understand issues such as environmental and flooding risks, demographic change, and the distribution of services and amenities in and around the study area.

## ***COMMUNITY ENGAGEMENT PROCESS***

We engaged in different forms of community outreach, with the aim of gathering diverse perspectives on the neighborhood's strengths and areas for improvement. Members of our team attended Town Day on September 14th and held two tabling sessions on different days where we set up a table along Broadway to talk with residents who walking in the neighborhood. During these sessions, we asked questions about Broadway that were intentionally open-ended so that people could provide their own perspectives on the neighborhood. In addition to attending Town Day and tabling on Broadway, members of the team also stopped in at businesses along the corridor to speak with owners and employees.

We followed up these general outreach activities with more targeted outreach to speak with local groups. We met with members from the following organizations:

- Equitable Arlington
- Arlington Residents for Responsible Redevelopment
- Housing Corporation of Arlington
- Mystic River Watershed
- The Thompson Elementary School Parent-Teacher Organization

# WHAT WE HEARD

In addition, we spoke with the following Town committees and department heads to hear about the work they have been doing along Broadway:

- Transportation Advisory Committee
- Tree Committee
- Police Department
- The Department of Planning and Community Development
- Recreation Department

After an initial round of community engagement, our team convened a community workshop on October 28th. The workshop began with a half-hour presentation from our team that included an overview of the perspectives we had heard so far on the neighborhood, as well as a draft set of goals for the report. Following the presentation, the workshop participants gathered around small tables, each focused on a different focus site along Broadway, to discuss different ideas that community members had for the neighborhood.

Finally, while the bulk of our community engagement occurred through in-person activities, we also created a project email address and posted a flyer describing the project in different stores along Broadway. We received several pieces of feedback through this email address, and also followed up with our own email-based questionnaire, which focused on getting feedback on mobility issues along Broadway. The Transportation Advisory Committee in particular was helpful in distributing the written mobility questionnaire.



Figure 4. Students and community members discuss potential mobility improvements to the corridor at the public workshop on October 28th.



Figure 5. Students presenting initial findings at the public workshop.







# WHAT WE HEARD

## COMFORT, SAFETY, AND STREETScape

Many of the comments we heard focused on changes the Town could make to improve the comfort and feel of Broadway. While the sidewalks along Broadway are generally in good shape, and include intersections with accessible tip-downs for strollers and wheelchairs, many residents felt that Broadway could be made safer for pedestrians and cyclists, and that the Town could consider bigger changes at some of the difficult intersections, such as the triangle where Broadway, Warren Street, and River Street converge.



Figure 10. Crossing the intersection where Broadway, Warren Street, and River Street converge can be difficult.

*“I’d like to see a safer bike path along Broadway.”*

*“I worry about kids going to school walking along Broadway. I don’t think the cars slow down there.”*

*“The traffic along River St. & Alewife can really back up, particularly in the morning.”*

## HOUSING

Housing repeatedly came up in conversations with residents. Many residents noted the increasing cost of rental housing in the neighborhood, as well as the difficulty of finding a home to buy, particularly for families with kids.

*“There aren’t enough good housing options for people with middle incomes.”*

*“The housing market is really tight. Broadway could be a great place to help create more supply.”*



Figure 11. Triple-deckers are an example of existing housing density along Broadway.

## QUALITY OF LIFE

Many residents appreciated the sense of community in the neighborhood, as well as the unique public assets, such as Lussiano Park and both new and longstanding businesses. At the same time, people felt like there could be more vibrancy along the corridor and a greater number of amenities and destination points along Broadway.

*“It’d be great to bring more of the vibe of Arlington Center down here to the Broadway corridor.”*

*“The empty lots on Broadway are an issue.”*



Figure 12. While Lussiano Park (above) is a key asset in the neighborhood, there is potential to reinvigorate vacant and underutilized lots.

# STUDY GOALS

Based on our research, qualitative analysis and the conversations we had with community members, we identified the following three goals for the study area that we have used to shape the recommendations in this report.

## 1. *Safety & Walkability*

Ensure that the street design for Broadway is safe and comfortable for all users, while facilitating connections between the neighborhood, the town and the wider region.

## 2. *Housing Affordability & Variety*

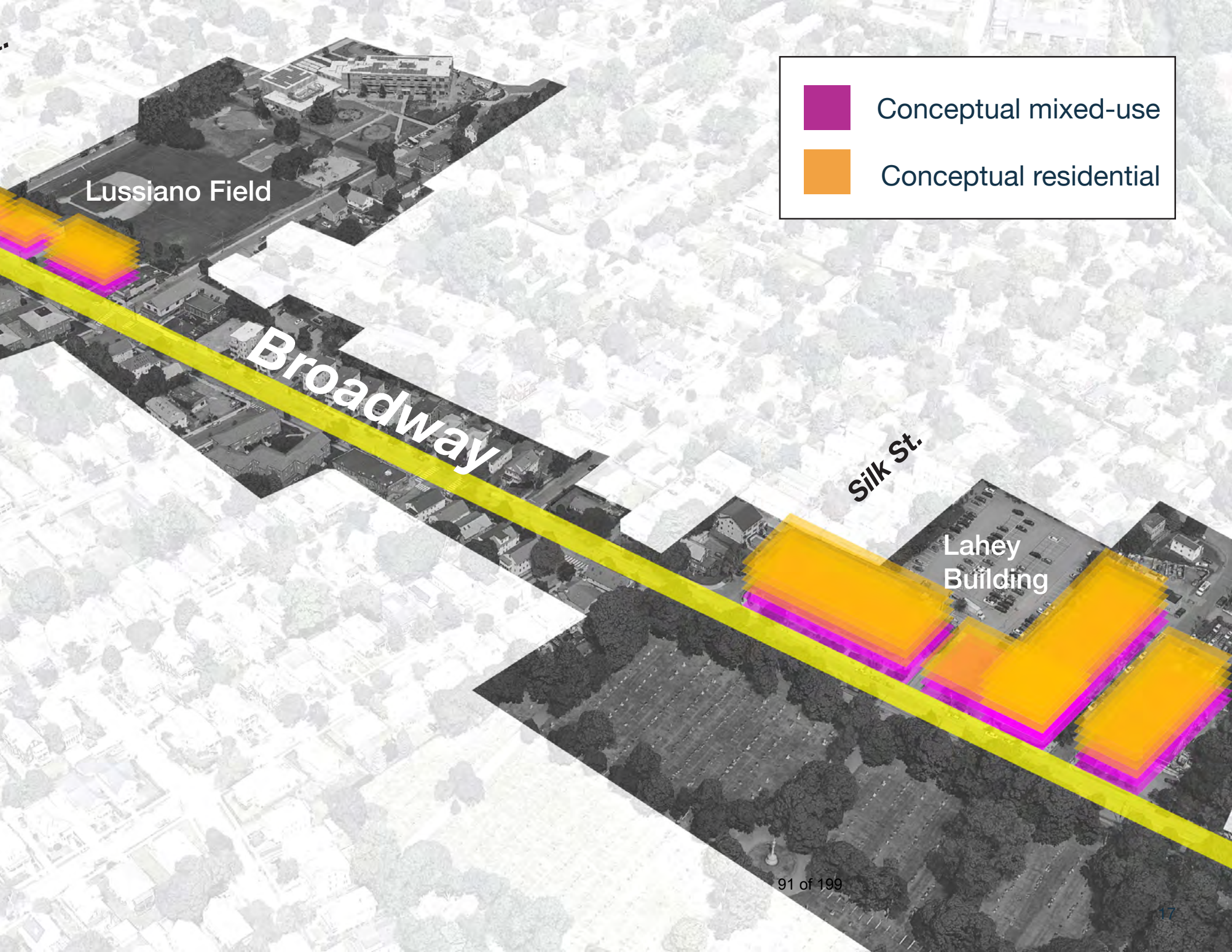
Maintain a healthy housing supply that provides options for a range of income levels.

## 3. *Vibrance & Quality of Life*

Build on the neighborhood feel of the corridor while enhancing the social, economic and cultural opportunities that are available locally in the community.







Lussiano Field

Broadway

Silk St.

Lahey Building



Conceptual mixed-use



Conceptual residential



# II. MOBILITY



# MOBILITY

## ***Getting To, From, & Around Arlington***

Today Arlington boasts some 95 miles of public streets and 24 miles of private roadways and is traversed by 6 miles of state highways and parkways.<sup>1</sup> While the town is less dense than other areas in the metropolitan area, goods and people need to move freely across town. The mobility infrastructure and systems should make an ease of movement possible, integrating Arlington, and Broadway specifically, into regional activities and the economy.

Bus services supplied primarily by the MBTA have been the transit mode serving Arlington, including the Broadway corridor, since passenger rail on the Lexington Branch through Town Center closed. The MBTA number 87 bus connects Broadway to transit elsewhere.

The Broadway corridor today is a key mobility corridor connecting the Arlington Town Center with Somerville, Davis Square, the Alewife Brook and Parkway and areas further afield, while also providing connecting pathways for the residents and businesses in our study area. In this section, we outline the current state of mobility and offer recommendations informed by the views of residents and business people seeking to build on existing strengths.



Figure 13. Broadway currently has room for cars, but minimizes space for trees and pedestrians, and forces bicyclists to ride in traffic.

<sup>1</sup> Town of Arlington, <https://www.arlingtonma.gov/>.

# THEMES & GOALS

In this report we focus on four key modes of mobility: walking, biking, transit use, and private automobile travel. In developing this plan for the Broadway corridor, goals for the transportation study arose from consultation with the local community. Our overarching goal for mobility is to ensure that the street design for Broadway is safe and comfortable for all users, while facilitating connections between the neighborhood, the town, and the wider region.

Our research indicates that most residents of the Broadway study area get to work by car. However, the area has a relatively high percentage of bicyclists and transit riders, for both the state and the Town. Current census statistics indicate that 5.4% of residents in the Broadway study area bike as compared to 3.3% in Arlington generally and 0.8% in Massachusetts as a whole. Twenty-three percent of residents in the study area use public transit compared to 20.4% in Arlington and 10.2% across the state. A full 60.4% of residents drive but this is lower than 66.5% in Arlington and 78.1% in Massachusetts.

Most residents of the study area do not work in Arlington and must commute each weekday to and from the cities of Boston, Cambridge, and Somerville. Those who do work along Broadway mostly commute from other cities or towns. Reliable regional transportation connectivity is a priority for the constituents of the Broadway study area.



Figure 14. Overlooking the Broadway / Warren St. intersection, facing north.



# WALKING THE CORRIDOR

The Broadway corridor has sidewalks along every street and frequent crosswalks. However there are some spots which are problematic.

There are cases, such as the Broadway/Warren St./River St. intersection (discussed further on page 34), where the in-street crossing distance for pedestrians is unsuitably long. There are instances where curbs are cut for non-existent entrances and a few streets where marked crossings for pedestrians have not been implemented.

*“The corridor has sidewalks on both sides, in varying states of repair. Corners are often impassible during the winter, due to piled up snow. It [Broadway] is a fairly wide street with long crossing distances, and no curb extensions or refuge islands at the crosswalks.”*

*“The bridge over the Alewife Brook is an issue when it snows, as it’s often not shoveled and becomes packed with ice.”*

*“I live on Sunnyside Avenue. Walking here is a problem. The city was supposed to repave sidewalks but got sidetracked by the tree root issue. The sidewalk is narrow, the street has no curbing...so people end up walking in the street.”*



# SIDEWALK SAFETY RECOMMENDATIONS

Residents of the area made numerous suggestions for pedestrian safety, accessibility, and comfortability improvements along the corridor. The following recommendations synthesize what we heard from both residents and visitors, what we heard from various Town departments, and what we know about regional transportation needs and initiatives.

1. Broadway needs to be made safe for children crossing streets on their way to school. We recommend strategic placement of high-visibility marked crosswalks and signs at intersections along common paths to schools. The following are our suggestions:

*Near Gibbs School:* Broadway/Foster St./Rawson St., Broadway/Tufts St., Tufts St./Raleigh St., Bates Rd./Raleigh St.

*Near Thompson Elementary School:* Everett St./University Rd., Everett St./Purcell Rd., N. Union St./Purcell Rd./Fremont St., N. Union St./Norcross St., Broadway/N. Union St./Oxford St.

*Near Lesley Ellis School:* Oxford St./Raleigh St., Broadway/N. Union St./Oxford St. (also mentioned above).

2. Physical safety and visibility conditions at crosswalks on Broadway itself should be improved. Curb extensions (“bulb-outs”) and removal of 1 or 2 parking spaces on either side of a crosswalk (“daylighting”) are excellent for this purpose.

3. A redesign of the Broadway/Warren St./River St. intersection altogether, with an emphasis on safety conditions, is necessary. This is also discussed later in this same chapter.

We believe the Town can make a compelling case for any of these example pedestrian safety improvements when pursuing capital funding grants from other levels of government.



Figure 15. Curb extensions could help shorten crossing distances and slow traffic. Source: NACTO Urban Street Design Guide.

# BIKING BROADWAY

The Strava Map and Lime Bike map shown on this page represent the best data we have on current bike travel along Broadway. They illustrate the routes most heavily traveled by cyclists. The Strava Map indicates riders who have opted to provide geolocation data on the Strava mobile app. Lime Bikes, a brand of dockless bikeshare presently operating in Arlington, also have built-in geolocation tracking.

In each case we see large flows of cyclists using Broadway—seemingly almost as much as travel along Massachusetts Avenue or the Minuteman Bikeway. Line color (blue to red in the Strava Map, faint to dark red in the Lime Map) indicates the number of riders using a particular road. It is clear that many bicyclists are riding on Broadway, despite a complete lack of bicycle infrastructure on the street.

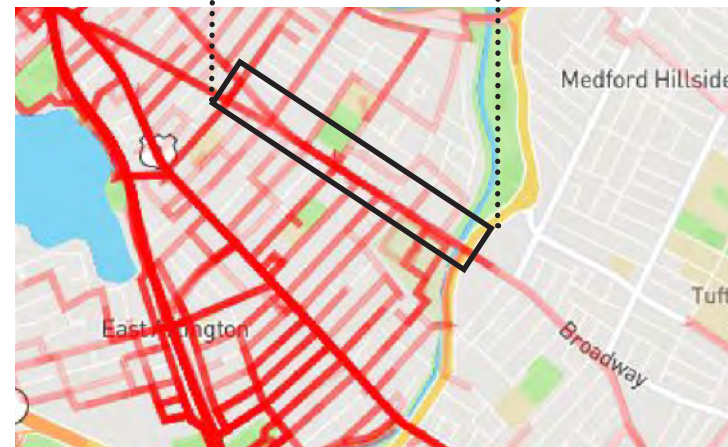


Figure 16. (Top) Map of bicycle traffic from Strava, an app for recreational and athletic users. Source: Strava.

Figure 17. (Bottom) Map of bicycle traffic from Lime, a local dockless bike-share service. Source: Daniel Amstutz, Town of Arlington Dept. of Planning & Community Development.

# BIKE SAFETY AND INFRASTRUCTURE

Currently, there is no cycling infrastructure along Broadway. We received many comments in favor of making improvements to bike safety along Broadway and connecting to further destinations.

Somerville has implemented bike lanes on much of Broadway, and bike lanes exist along much of Mass Ave. Implementing bike lanes on Broadway in Arlington may spur Somerville to add the missing link and create a seamless route into Davis Square. The ever popular Minuteman Bikeway could be reached if bike lanes were extended down Broadway to Massachusetts Avenue creating a safe connection to the Bikeway.

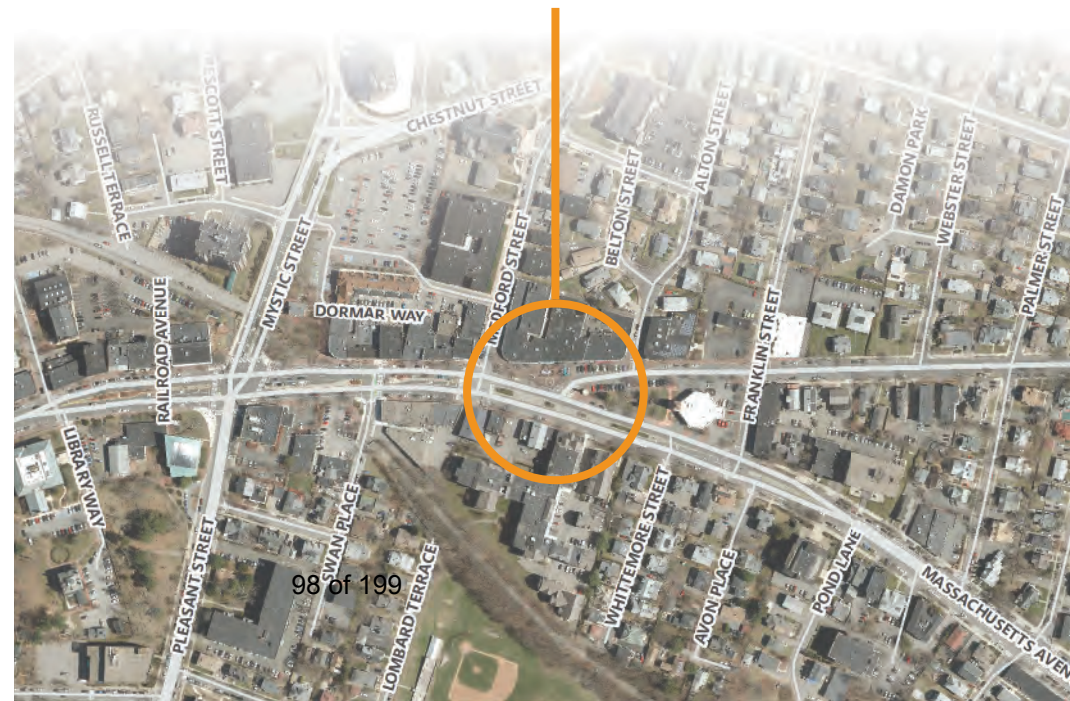
Currently, cyclists are in mixed traffic and a number that we spoke with have raised serious concerns about safety on the roads. With the increasing numbers of cyclists in Arlington, and the Broadway corridor, these concerns should be taken seriously.

Three particular intersections are problematic for cyclists:

## **MASSACHUSETTS AVENUE & BROADWAY\***

\*Near our study area, though outside of its boundary

*“Cycling westbound on Broadway between Franklin and Mass Ave is a drag, though I’m not sure how to fix it off the top of my head.”*





### WARREN STREET & BROADWAY

*“The intersection of Warren and Broadway is not great as a westbound cyclist. I feel at risk both of westbound vehicle traffic turning right onto Warren ahead of me and of eastbound traffic on Warren turning across me as soon as they identify a gap in vehicle traffic.”*

### ALEWIFE GREENWAY BIKE PATH & ROUTE 16

*“At the intersection with Route 16... bikes get severely pinched and often have to ride on the sidewalk.”*

*“For the Alewife Greenway, there is no provision for crossing Broadway other than using sidewalks and the signal at Route 16.”*





# INCREMENTAL IMPLEMENTATION OF BIKE LANES

This report recommends the installation of bike lanes along all of the Broadway corridor. We recommend 5'-6' lanes with a 1'-2' inside-edge striped buffer where possible, and physical barriers (e.g. plastic bollards) if snow-plowing equipment permits. Furthermore, we recommend placing the bike lanes on the outside edge of the parking lane(s), next to the curb. Recent research has shown that bike lanes without any physical protection are not as safe.<sup>1</sup>

We have generally found there is community buy-in to removing on-street parking on one side of Broadway to create space for these bike lanes—however, this is welcome in some areas and not others. For this sensitive decision, we recommend a needs-based approach such as:

- Preserve on-street parking next to commercial or mixed-use land uses without their own off-street customer parking.
- Remove on-street parking near commercial land uses with their own off-street customer parking.
- Preserve some on-street parking near residences so that visitors can park.
- Remove on-street parking next to the cemetery.
- Anywhere on-street parking is removed on either side of the road, ensure crosswalks are nearby and safe to use.

<sup>1</sup> Marshall and Ferencsak, "Why Cities with High Bicycling Rates Are Safer for All Road Users."



Figure 18. This report recommends the installation of bike lanes along all of the Broadway corridor. 100 of 199

# THE STATE OF TRANSIT

Currently Arlington is served by a number of buses, with the Massachusetts Avenue and Broadway buses having some of the highest boarding numbers out of all MBTA east-west bus lines.<sup>1</sup> Broadway is served on weekdays and Saturdays by the #87 bus which connects with the Red Line at Davis Square and the Green Line at Lechmere.

However, the 87 bus has a number of shortcomings according to local residents. It does not operate along the corridor on Sundays, instead terminating at Clarendon Hill just outside the Town border. Furthermore, commuters and local residents complain that evening runs of the bus are rarely reliable and service is often delayed due to traffic congestion and operational ineffectiveness. The MBTA #88 and #89 buses connecting with central Somerville stop at Clarendon Hill and do not even enter Arlington.

It is clear that the residents in the study area as well as those working in the area could benefit by an improved bus service.

*“[On Sunday] people who live further in town have a much longer walk to Clarendon Hill.”*

*“It would be great to have the 87 bus continue to Arlington Center on every trip rather than stopping at 8PM. The 87 has a very long route, which causes lots of variability in its arrival time in the evenings.”*

*“There should be a consolidation of the inbound 87 bus stops at Silk and Sunnyside. They are only one block apart and typically there are only one or two people at each, in the AM at least.”*

---

<sup>1</sup> MBTA, “Better Bus Project.”

# TRANSIT INFRASTRUCTURE AND FURNITURE

Most bus stops on Broadway are quite minimal, consisting solely of a small pole-mounted sign indicating the bus line. There is no street furniture or shelters for passengers, and of course there are no dedicated bus lanes on Broadway. In Somerville, however, there are a number of metal benches, and larger stops have glass bus shelters to protect commuters from inclement weather.

Similar bus shelters, benches and lighting as those witnessed on Massachusetts Avenue and on Broadway in Somerville could be accommodated along the corridor to provide a more comfortable experience for transit riders.



Figure 19. Most bus stops on Broadway are quite minimal, consisting solely of a small pole-mounted sign indicating the bus line.

*“The 87 is second only to the Mass Ave buses in density of boarding at its stops along Broadway. The stops have only the most rudimentary accommodation [just a sign]. There are no shelters, usually no hard surfaced landing pads [needed for wheelchair access], and the stops are poorly cleared in winter. The stops at the eastern end of Broadway are not near any convenient pedestrian crossings. At the outbound stop near Rawson Road, illegally parked cars sometimes prevent the bus from pulling to the curb.”*

# ENVIRONMENTAL ISSUES

Improving the Broadway streetscape could be an opportunity for the town to address existing environmental concerns in the neighborhood. Any modification to the streetscape of the corridor should try to mitigate the additional weather-related challenges faced by pedestrians and cyclists. Arlington should consider:

- Planting additional trees along stretches where gaps exist to provide shade and mitigate neighborhood heat islands. The town should also analyze existing minor gas leaks along the street, and work with the local gas utility to remedy them, as these leaks threaten tree health.
- Devoting more space to tree pits parallel to the sidewalk in order to improve tree health.
- Installing water fountains at strategic locations, such as near bus stops, to aid pedestrians in the hot summer months.
- Making streetscape modifications that are fully plowable in the winter, and do not lead to ice build-up in the bike lane or sidewalk.
- Implementing low-maintenance rain gardens or bioswales on the corridor to capture and process storm water.

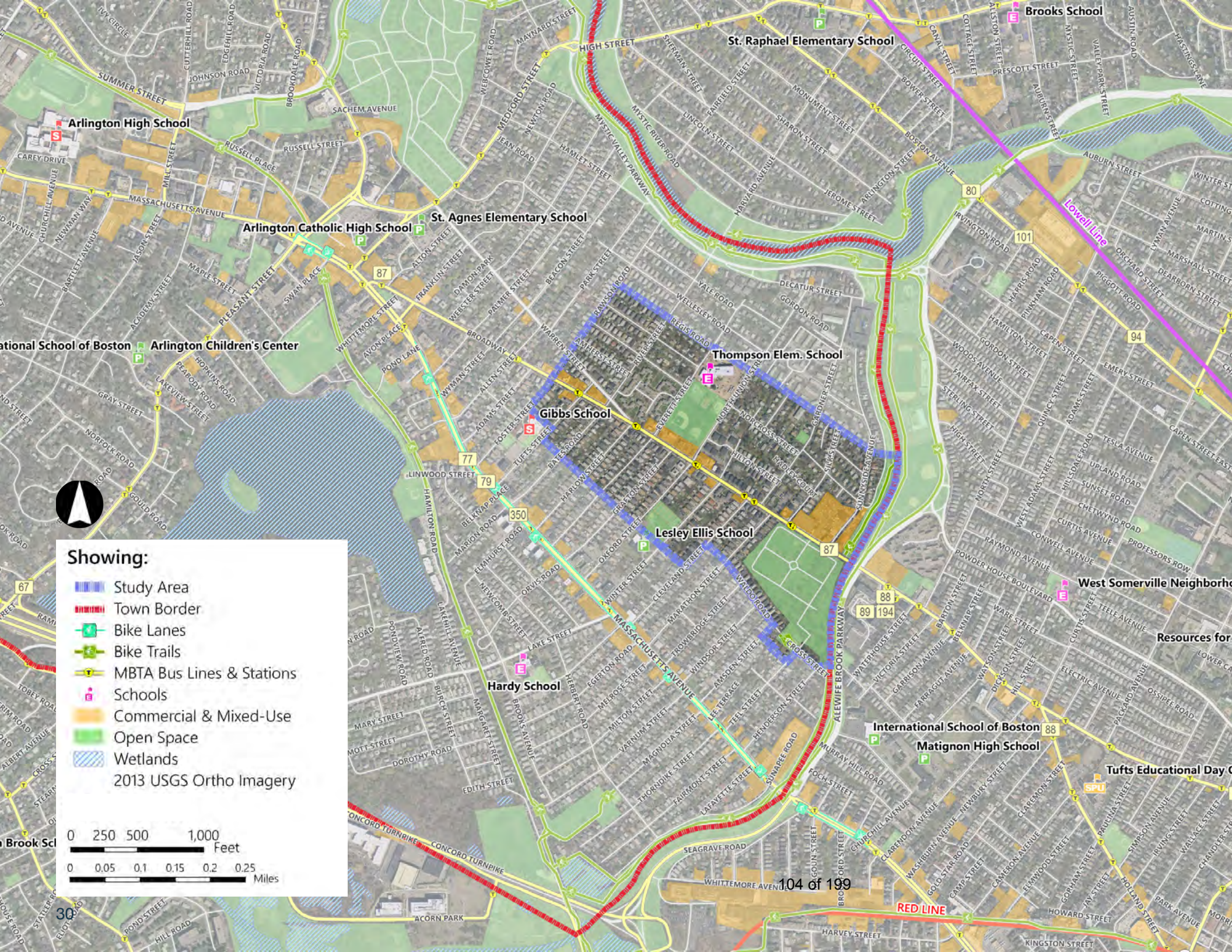


Figure 20. Existing street trees along Broadway, with gaps highlighted in red. Source: Arlington Tree Committee.



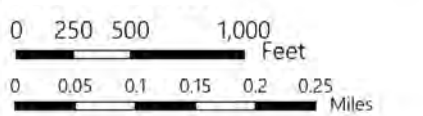
Figure 21. Green infrastructure, like this rain garden in East Arlington, could improve the street aesthetically and provide environmental services.





Showing:

- Study Area
  - Town Border
  - Bike Lanes
  - Bike Trails
  - MBTA Bus Lines & Stations
  - Schools
  - Commercial & Mixed-Use
  - Open Space
  - Wetlands
- 2013 USGS Ortho Imagery







## REGIONAL TRANSIT

Without any stops on the Green Line, Red Line or Commuter Rail in Arlington, it is important to maintain frequent bus service, connecting the Town to the region. Rail transit connections (Davis Square, Alewife, and to a lesser extent Lechmere) are well utilized by Arlington residents and workers traveling to and from the Town by bus, yet the only direct connection from Broadway without a transfer is Davis Square. All other connections to the regional rail system require changing buses.

The future addition of rail stations at Tufts/Medford, Ball Square and a potential future terminus at Route 16 on the under-construction Green Line Extension will increase this interconnectivity, however the MBTA is not yet certain how bus service will change once it opens.<sup>1</sup>

<sup>1</sup> MBTA, "Better Bus Project."

Figure 22. Map of regional transportation. Note that the Red Line Alewife station, connecting to many bus routes through Arlington, is just off-map to the south. Additionally, note the alignment of the Green Line Extension to the east of the corridor.

# TRANSIT RECOMMENDATIONS

At Broadway Plaza terminus of the 87 bus, where there are the most daily on-boardings and also a transfer from other lines arriving from Arlington Heights, we recommend a bus shelter with real-time arrival predictions. We also recommend benches at the higher-frequented stops, such as Broadway/Oxford St./North Union St., and Broadway/Cleveland St. at a minimum.

To improve the reliability of the 87 bus, we recommend:

- Piloting a red-painted bus and zbike-only queue-jump lane on the eastbound approach to Alewife Brook Parkway. This will entail the removal of a short distance of on-street parking, but no existing traffic lanes.
- Considering the elimination of the Broadway/Silk St. stop, given that it has the lowest daily on-boardings for the corridor and other stops are approximately 400 feet away in either direction.

To improve the utility of the #87 for transit-dependent users, we strongly recommend extending Sunday service on that line from Clarendon Hill up to the regular daily terminus at Broadway Plaza. Aspirationally, if new developments envisioned in this study (or other similar initiatives) manifest on Broadway, we suggest that the MBTA could extend the #88 and/or #89 along Broadway to Broadway Plaza.

The often delayed service should also be discussed with the MBTA, particularly in the early mornings and the evening commute.

Note that the westbound 87 bus is poised to have its layover stop moved to Franklin St. (two blocks before Broadway Plaza) and travel from there to Broadway Plaza upon beginning the eastbound trip. We do not expect this to meaningfully impact service quality.



Figure 23. To improve the reliability of the 87 line, we recommend piloting a queue-jump lane and considering eliminating the Broadway/Silk St. stop.

# TRAVELING BY CAR

Generally cars move at high speed down the Broadway corridor. However, there is heavy throughput during rush hour periods in the morning and evening, leading to complaints about congestion at certain signalized intersections.

The public impression is that congestion is increasing and a number of key intersections have become difficult in rush hour while driving. Places of growing peak hour congestion are around the schools, at the Broadway/Sunnyside Ave. intersection, the Broadway/Alewife Brook Parkway intersection and the Broadway/Warren St./River St. intersection.

Apart from the congestion-related issues, numerous residents complain about safety issues primarily at those three intersections. The area of Broadway near Sunnyside Ave. and Alewife Brook Parkway is a bottleneck, and the Broadway/Warren St./River St. intersection has been described by some as very dangerous due to poor visibility and unsafe merging.

We discuss these sites in some detail over the following pages and suggest solutions for the Town to consider. Intervening in these two groups of intersections will improve safety for all modes of transportation.

*“Traffic has gotten progressively worse over the years. Rush hour is especially difficult for residents trying to enter Broadway towards route 16.”*

*“The number of students in the Arlington school system is growing every year and Thompson now has over 500 students...A low estimate would be that half of them are driven to school by their parents. That would mean 250 cars between 7:30 and 8:00 o'clock every morning.”*



# WARREN STREET INTERSECTION

The Warren Street, Broadway, River Street intersection is the confluence of a number of roads in a small geographical area. A number of issues make this intersection dangerous:

- The distance that pedestrians have to cross is particularly long, due to corners that have been cut for firetrucks' turning radii and the long distance to cross both Broadway and Warren Street.
- River Street, Bates Street and Tufts Street also come together with Warren Street and Broadway in the same area.
- Many of the approach angles for cars moving between the various streets are very acute or obtuse angles, impeding visibility of cars on the other streets and making it more difficult to anticipate the movements of other vehicles. This is particularly the case when merging from Warren Street eastbound onto Broadway.

## ***Our recommendation:***

Improve safety for all transportation modes at the Broadway/Warren St. intersection through a redesign of the intersection looking at the following potential interventions:

### *Basic improvements:*

- Crosswalk safety improvements, as discussed earlier on page 22.
- Signal and/or crosswalk retiming.
- Painted bike lanes in both directions.

### *More ambitious option, for the Town's consideration:*

- Closing eastbound lanes on Warren St. between River St. and Broadway, and redirecting that traffic onto southbound River St. to intersect Broadway at a right angle.



Figure 24. We recommend improving safety at the Broadway/Warren St. intersection through crosswalk safety improvements, signal and/or crosswalk retiming, and painted bike lanes in both directions.

# SUNNYSIDE AVE INTERSECTION

This intersection is complicated because it includes a number of transport modes and two adjacent intersections each with impacts on the other. We have heard numerous complaints and comments from local residents about this confluence of intersections.

There is a clear need to improve safety and traffic flow for all modes of transportation at the Sunnyside Ave. & Alewife Brook Parkway intersections with Broadway.

## **Our Recommendation:**

We recommend:

- *Basic improvements:* Commission an engineering study on weekday AM congestion and the difficulty of turning motions, examining potential solutions in signal retiming. Consider our earlier suggestion for piloting a bus and bike-only queue-jump lane on the eastbound approach to Alewife Brook Parkway.
- *More ambitious option, for the Town's consideration:* Consider shifting southbound traffic exiting Sunnyside Ave. onto Silk St. where a signalized intersection may be placed. Sunnyside Ave. is too close to Alewife Brook Parkway for a signal. Our vision for the Lahey Building site, discussed on pages 55-62, supports this change.

*“Driving south towards the Route 16 intersection, it’s unclear when the road goes from one lane to two — a clear delineation there is needed. The no-turn-on-red from Broadway onto North Union seems to be unnecessary.”*

*“The intersection with Route 16 is complicated. Cars are always skipping the red lights because the intersection gets clogged due to poor left turn design. This affects the safety of both pedestrians and cyclists, with many close calls.”*

*“Turning into and out of Sunnyside Ave. is challenging.”*



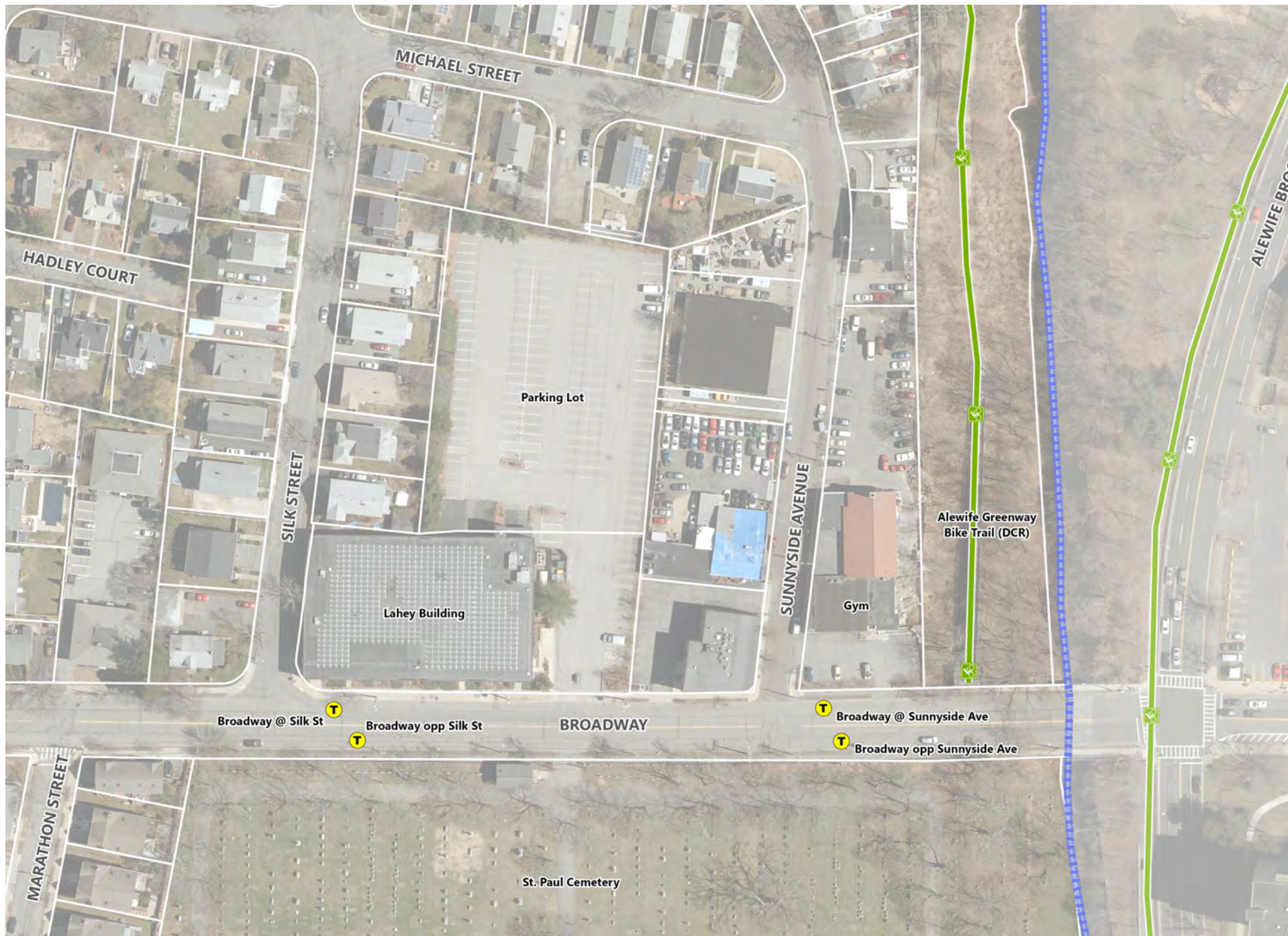


Figure 25. There is a clear need to improve safety and traffic flow for all modes of transportation at the Sunnyside Ave. and Alewife Brook Parkway Intersections with Broadway.



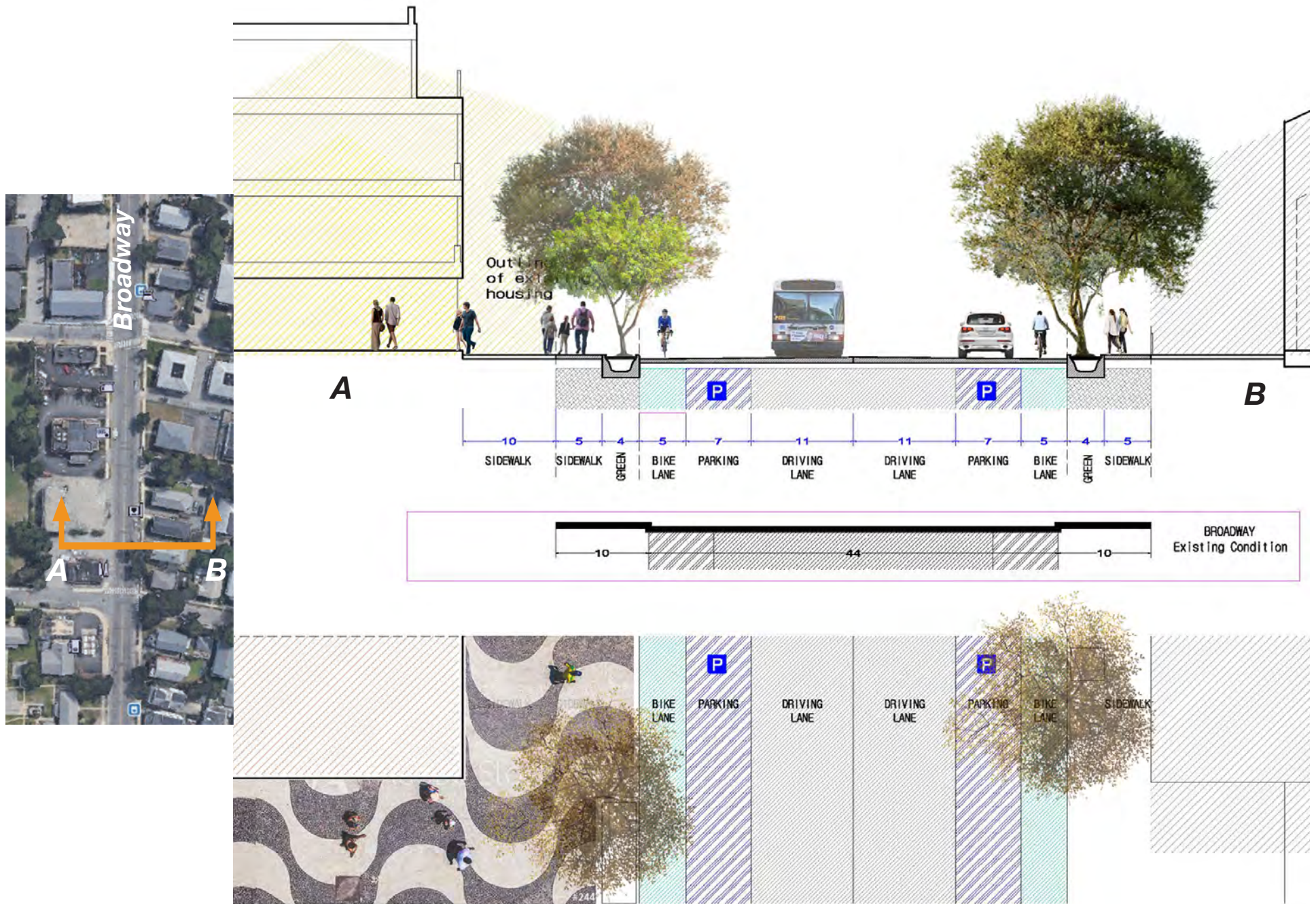


Figure 26. Streetscape example with bicycle lanes and parking on both sides of the street.



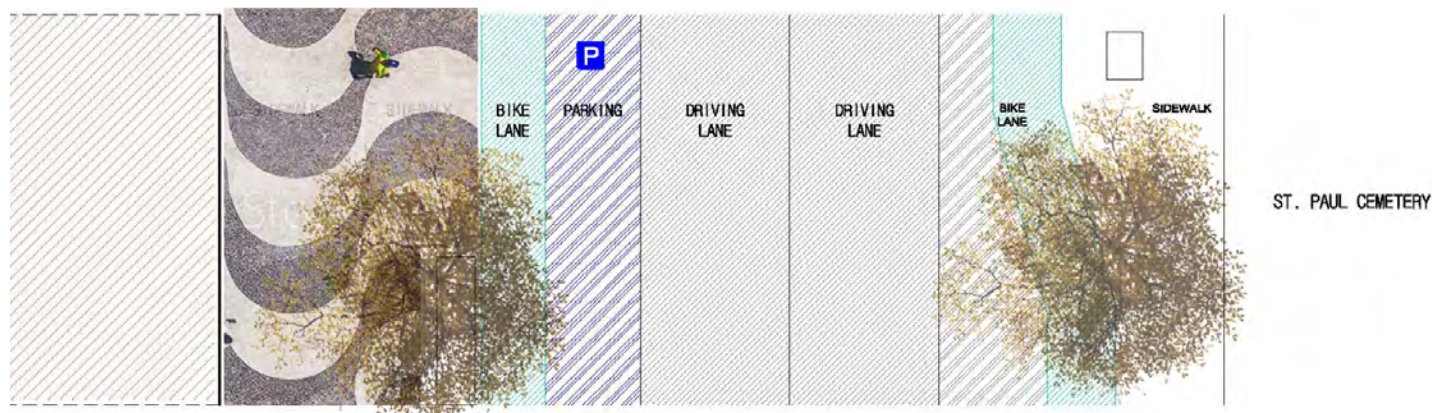
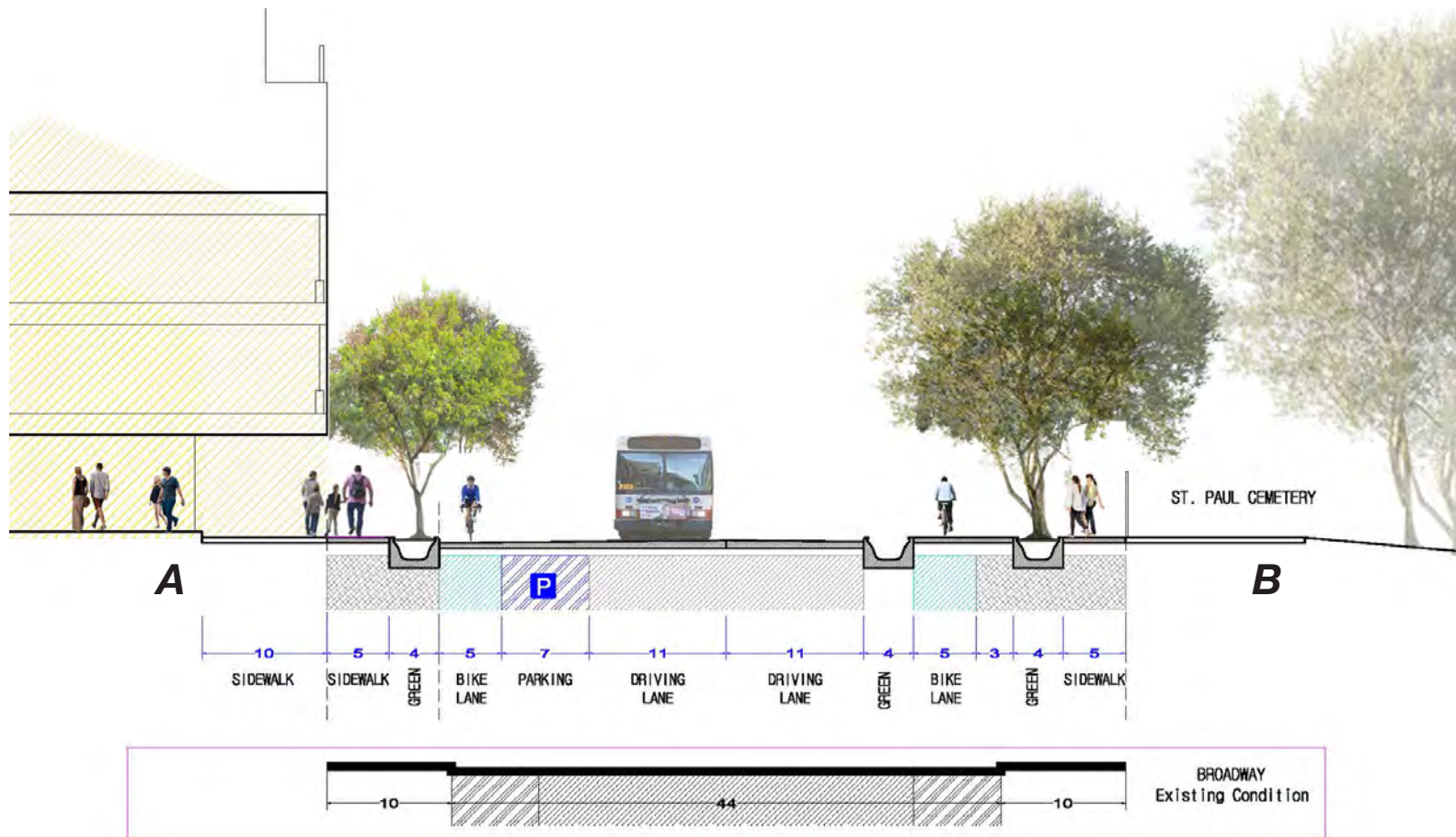


Figure 27. Streetscape example with bicycle lanes along both sides of the street and parking only on the north side of Broadway, adjacent to the Laney Building site.

# Mobility Recommendations

The following items represent a summary of our thinking and recommendations for improving mobility along the Broadway corridor:

- Redesign the Broadway/Warren St. intersection to improve safety for all transportation modes. Shorten crosswalks with curb extensions and consider realigning traffic.
- Complete a study on weekday congestion and the difficulty of turning motions at the Sunnyside Ave./Broadway Intersection and at the Route 16/Broadway Intersection. Examine potential solutions in signal retiming and alternatives to improve safety.
- Pilot a 10-11' painted bus queue-jump lane on Broadway approaching Alewife Brook Parkway from the west.
- Eliminate the Broadway and Silk St. stop, given that it has the lowest daily on-boardings for the corridor and other stops are close by.

- Extend Sunday service of the 87 bus to Broadway Plaza.
- Implement high-visibility, yellow crosswalks and retroreflective school zone or crosswalk signs at intersections on paths to schools within the corridor. Safe Routes to School grant funding should be utilized.
- Implement curb extensions and 'daylighting' (removing 1-2 parking spaces in the opposite direction of vehicle traffic in the adjacent lane) for signalized crosswalks on Broadway, mentioned above in the case of the Broadway/Warren St./River St. intersection.
- Implement bike lanes with safety buffers in both directions, with alternating on-street parking as the road width and land uses allow.
- Remove curb parking adjacent to the cemetery on Broadway, particularly in the section closest to the Alewife Brook Parkway, where the Bus Priority Lane will be implemented.
- Improve bus transit furniture and infrastructure by installing bus shelters, benches, water fountains and improved signage.
- Enhance environmental services along the roadway by planting additional trees where gaps exist, enlarging planter spaces, and installing rain gardens or filtration ditches.



# III. HOUSING





# GROWING TOGETHER, EQUITABLY

The Town of Arlington has experienced noticeable growth over the past few years. From 2010 to 2018, the town experienced a 6.6% increase in population, the major racial groups being White, Asian, and Black / African American. Such growth has emerged partly from increasing costs of living in the greater Boston metropolitan area as a whole. This has caused Arlington to search for a balance between taking advantage of regional economic growth, and a strong desire to maintain its distinct identity as a small New England town. For the town, it remains important that its diverse and lively neighborhoods remain places where residents can rely on each other and provide a sense of belonging.<sup>1</sup> This aspiration is directly affected by the ability of residents to acquire affordable housing and remain in the neighborhoods in which they may have long-standing connections.

In this section, we outline the current state of housing within our study area. We then offer proposals informed by the input of residents, seeking to build upon existing community strengths and assets. While past developments have clustered along the Massachusetts Avenue corridor, Broadway has the potential to enhance the neighborhood by providing safe and walkable streets with community-oriented commercial uses and much needed housing. We hope our suggestions can aid in prompting more equitable growth as the town continues to expand.

<sup>1</sup> Arlington Redevelopment Board, "Arlington Master Plan."



Figure 28. Existing multi-family apartment housing on Broadway.

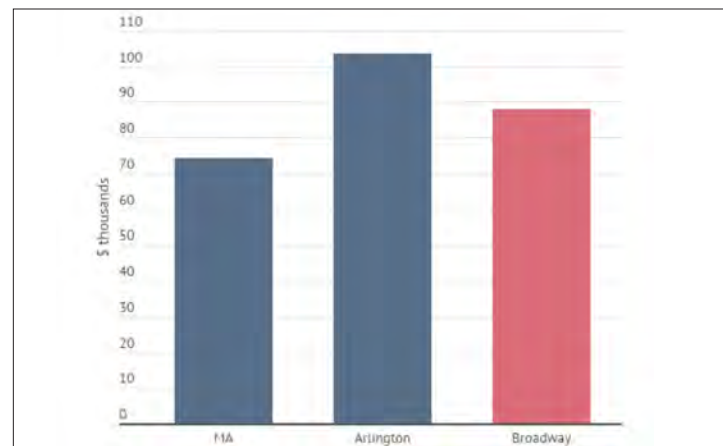


Figure 29. Comparison of median household incomes in MA, Arlington, and the study area. We believe the sense of belonging is directly related to the ability of residents to acquire affordable housing and remain in the neighborhood.

# CURRENT CONDITIONS

## *Improving Access to Housing*

The Broadway corridor is home to a broad representation of people. Among residents, 26% speak a primary language other than English, encompassing more than 12 languages. The share of foreign-born residents (predominantly of Asian origin) comprised about one quarter of Arlington's recent growth. This demographic diversity can enable promising avenues for equitable development, and pave the way for a new chapter in Arlington's history.

The fact remains, nonetheless, that the town's population growth is at odds with the supply of housing available at an affordable rate. In community outreach with the Thompson Elementary Parent-Teacher Organization (PTO), parents voiced concerns about their perceived ability to remain in the Town. Such an issue is reflective of the larger state of housing in our study area, where 37% of households are cost-burdened and spend over 30% of their monthly income on housing costs and 11% spending more than 50% of their monthly income. Our planning study seeks to make recommendations in light of this. As of 2018, Arlington's subsidized housing inventory (SHI) is 5.6% of the town's total housing stock – a ratio that has only increased by 0.1% from 2001 to 2018.<sup>1</sup>

<sup>1</sup> Metropolitan Area Planning Council and JM Goldson, "Arlington Housing Production Plan."



Figure 30. The Broadway corridor is home to a broad representation of people. Among residents, 26% speak a primary language other than English, encompassing more than 12 languages.

### Cost and Access

As Arlington continues to grow, the current supply of housing needs more examination. A 2016 housing report by the Metropolitan Area Planning Council found that Arlington at large would need to add 834 additional housing units to meet the Massachusetts 40B 10% target for affordable housing stock.<sup>1</sup> The corridor can accommodate a share of this need.

The median age of the housing stock along the corridor is above 60 years, with one block in the study area extending up to 80 years. Aging housing stock can present safety concerns and cost more to maintain before becoming uninhabitable. For renters, the median monthly cost of housing along the corridor is \$2,504, which is 19% higher than the state-wide median monthly cost. Additionally, the median home value within the study area ranges from \$480,000 to \$590,000, a distribution 60% lower than the town-wide median. Further, residents along the corridor are proximate to only two restaurants and one corner store. Housing costs and lack of amenities can be addressed by allowing, for example, more mixed-use development to occur.

Later in this report, we detail some changes in zoning that can help new development to proceed.

<sup>1</sup> Metropolitan Area Planning Council and JM Goldson, "Arlington Housing Production Plan."



Figure 31. 37% of households in the Broadway corridor spend over 30% of their monthly income on housing.

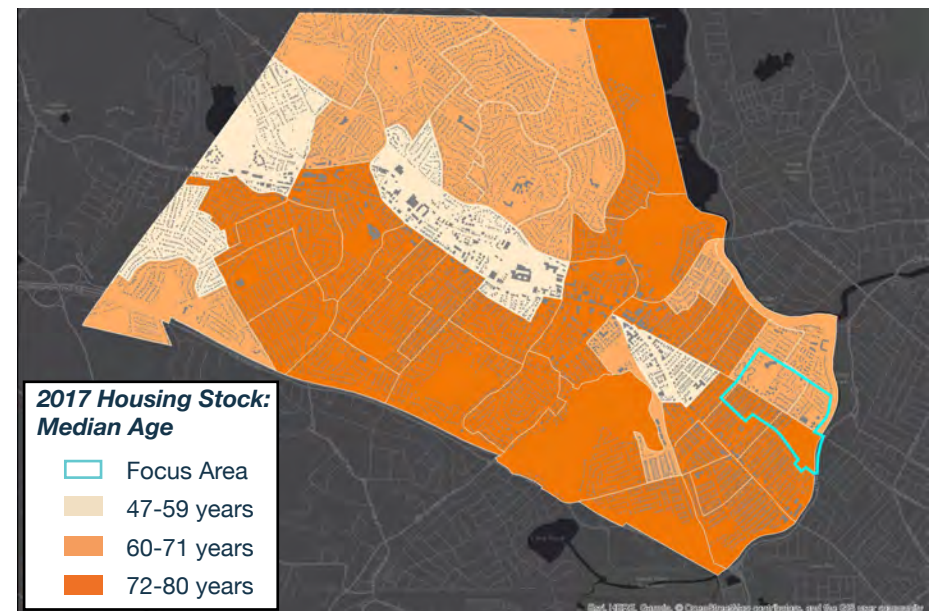


Figure 32. The median age for much of the housing stock in Arlington and the study area is above 60 years.



# CURRENT CONDITIONS

## ***Density and Stock***

In relation to Arlington at large, the study area is relatively population dense. Moreover, population density here corresponds with housing density, such that the areas proximate to Massachusetts Avenue and Broadway have more elevated housing densities compared to the rest of Arlington.

Currently, housing consumption is split fairly evenly among renters and owners, with 49.6% being owners and the remaining 50.4% being renters. Household sizes are distributed between 37% young couples with no children, 34% singles, and 29% young families with one or more children. In contrast to Massachusetts Avenue, development along Broadway is sparse, lower in density, and is oriented toward residential uses, with a few commercial and mixed-use parcels interspersed among them. The residential units along the corridor are predominantly two-and-a-half story buildings with a few triple-deckers. Any proposed new development is governed by the Town of Arlington Design Standards, which includes building materials, height, setbacks, and interface with the streetscape.<sup>1</sup>

<sup>1</sup> Town of Arlington and Gamble Associates, "Design Standards for Town of Arlington."

Recent redevelopment efforts on Broadway include the 117 Broadway project, which will add 14 affordable housing units with ground-level commercial space for the Arlington Food Pantry and an additional tenant.<sup>2</sup> Based on feedback from community members, such additions to the affordable housing stock are well warranted. This study makes recommendations to support the addition of similar proposals along the Broadway corridor.

<sup>2</sup> YourArlington.com, "Affordable Housing at Downing Square, Broadway Gets Funding."



Figure 33. Recent redevelopment efforts on Broadway include the 117 Broadway project, which will add 14 affordable housing units with ground-level commercial space for the Arlington Food Pantry and an additional tenant.

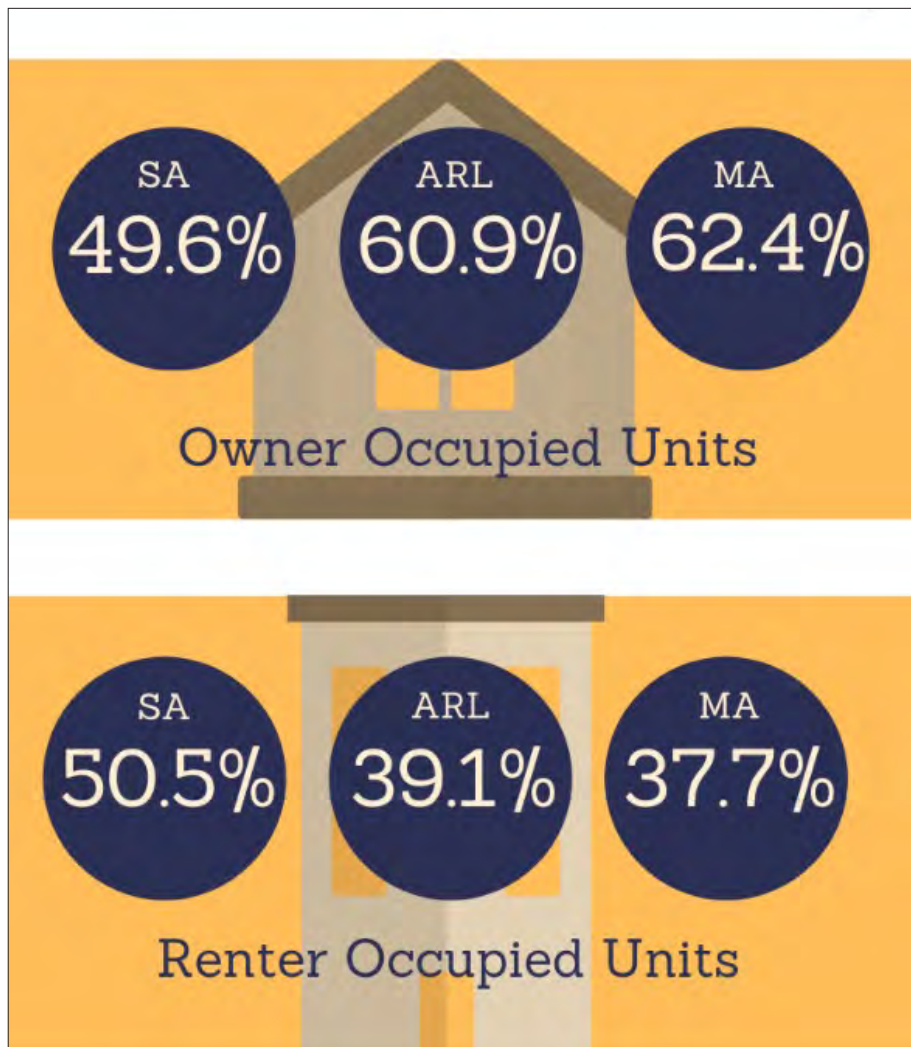


Figure 34. Currently, housing consumption in the study area (SA) is split evenly among renters and owners.

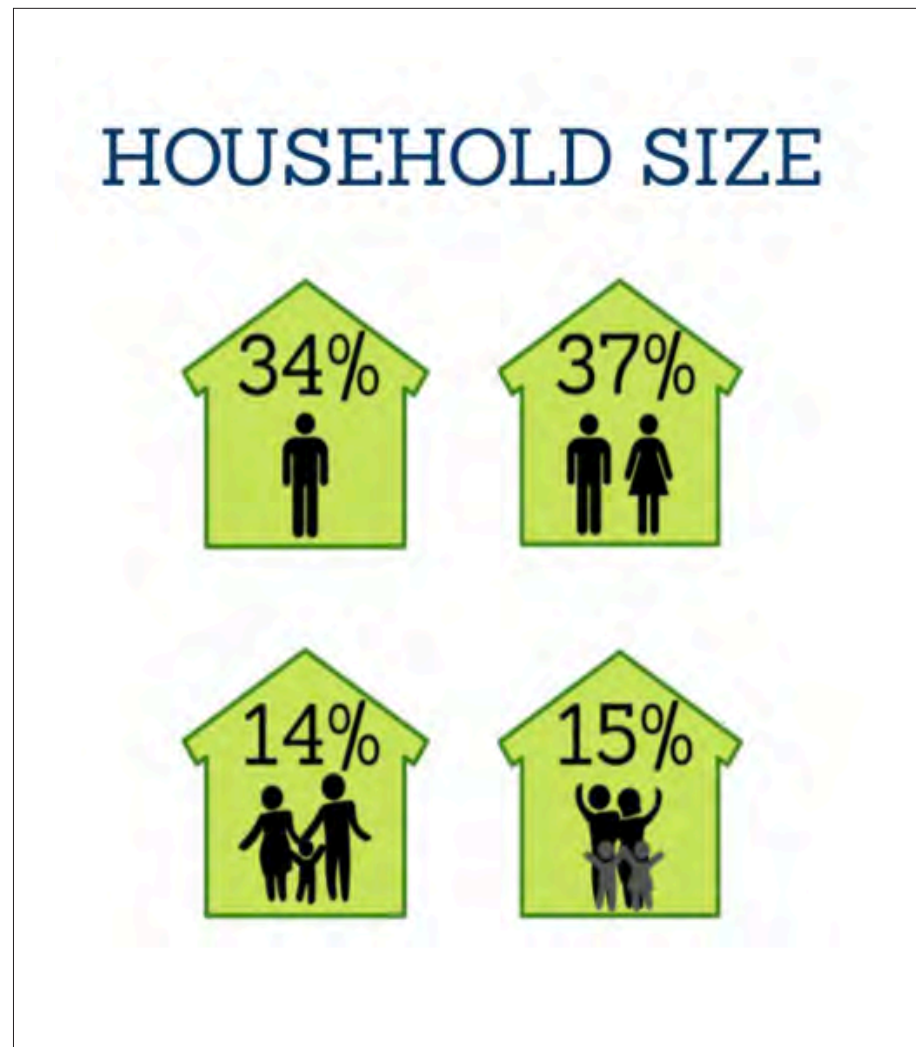


Figure 35. Most households in the study area (71%) are single or two-person families.

# WHAT WE HEARD

Interactions with local residents have largely guided this study's understanding of the neighborhood and the following proposals. From September to November 2019, team members met and consulted with local community organizations and stakeholders to get a better sense of town needs and desires regarding the themes explored here. Additionally, an email account was set up to facilitate dialogue with interested parties.

Participation in Arlington Town Day in September confirmed the growing concern over housing affordability by residents and also provided team members an opportunity to informally chat with community members about their perception of the town. Following this, meetings were scheduled with members of entities such as Equitable Arlington, the Housing Corporation of Arlington, and Arlington Residents for Responsible Redevelopment.<sup>1</sup> Moreover, team members conducted a community-wide workshop in October at the Hardy School Elementary School, where residents received updates on our study and participated in a workshop sharing concerns and ideas for three sites presented at the meeting: the Lahey site, Lussiano Field, and the streetscape along the corridor.

Our team identified the ability of the Broadway corridor to harmonize with existing development on Massachusetts Avenue in a way that moderates existing issues of housing affordability and lack of amenities in the study area. Generally, community members in the study area would like to see more amenities made available to them, and see promise in mixed-use development along the corridor. The workshop provided the most direct forum for community members to share their opinions about the study's site selection and proposal for mixed-use development at the Lahey Building near the boundary of Arlington and Somerville, elaborated upon further in the following sections.



Figure 36. Community feedback about housing on Town Day.

<sup>1</sup>Other stakeholders included a town environmental planner, the Arlington Recreation Department, and the Thompson School PTO, as noted earlier.



*“The housing market is really tight. Broadway could be a great place to help create more supply.”*

*“There aren’t enough good housing options for people with middle incomes.”*

*“The main thing is housing. It’s hard for people to just buy a piece of land and build on it. Everything needs a special permit.”*



Figure 37. Triple-decker houses on Broadway.



Figure 38. Existing apartment housing on Broadway.

# CURRENT ZONING AND BARRIERS TO DEVELOPMENT

In community engagement sessions, residents expressed that any new development should seek to retain the existing character of the town. In the study area, the current zoning scheme (in the following figure) is dominated by two- and three-family housing, as well as low-density apartments (R2, R3, and R5 zoning codes respectively), as shown in the following figure. In addition to these residential parcels, there are a few commercial uses (B2, B2A, and B4 zoning codes) intermixed in the area. As it stands, the maximum allowable height allows for 6-story development.

While development is subject to compliance with Arlington's zoning bylaws, recent codification of design standards has served as the first step in improving and updating the bylaws that present difficulties for new development. As is, the language of the allowed zoning uses presents difficulties for moving forward with mixed-use development, namely the descriptions associated with the residential uses, which "discourage uses which would detract from the desired residential character."<sup>1</sup> In the 2016 Housing Production Plan study carried out by MAPC, zoning was targeted as needing to be amended in order to facilitate more robust affordable housing measures.<sup>2</sup>

The recently approved 117 Broadway development was the result of amendments to the zoning bylaws, allowing mixed-use development along the commercial corridor with a special permit.<sup>3</sup> In the next section, we explore similar actions that can be taken to bypass obstacles from zoning bylaws.

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1 Town of Arlington, Town of Arlington Zoning Bylaw.

2 Metropolitan Area Planning Council, JM Goldson, and Town of Arlington, "Arlington Housing Production Plan."

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3 Greenhalgh, "Arlington Food Pantry Lands Home in Planned Affordable Housing Building."



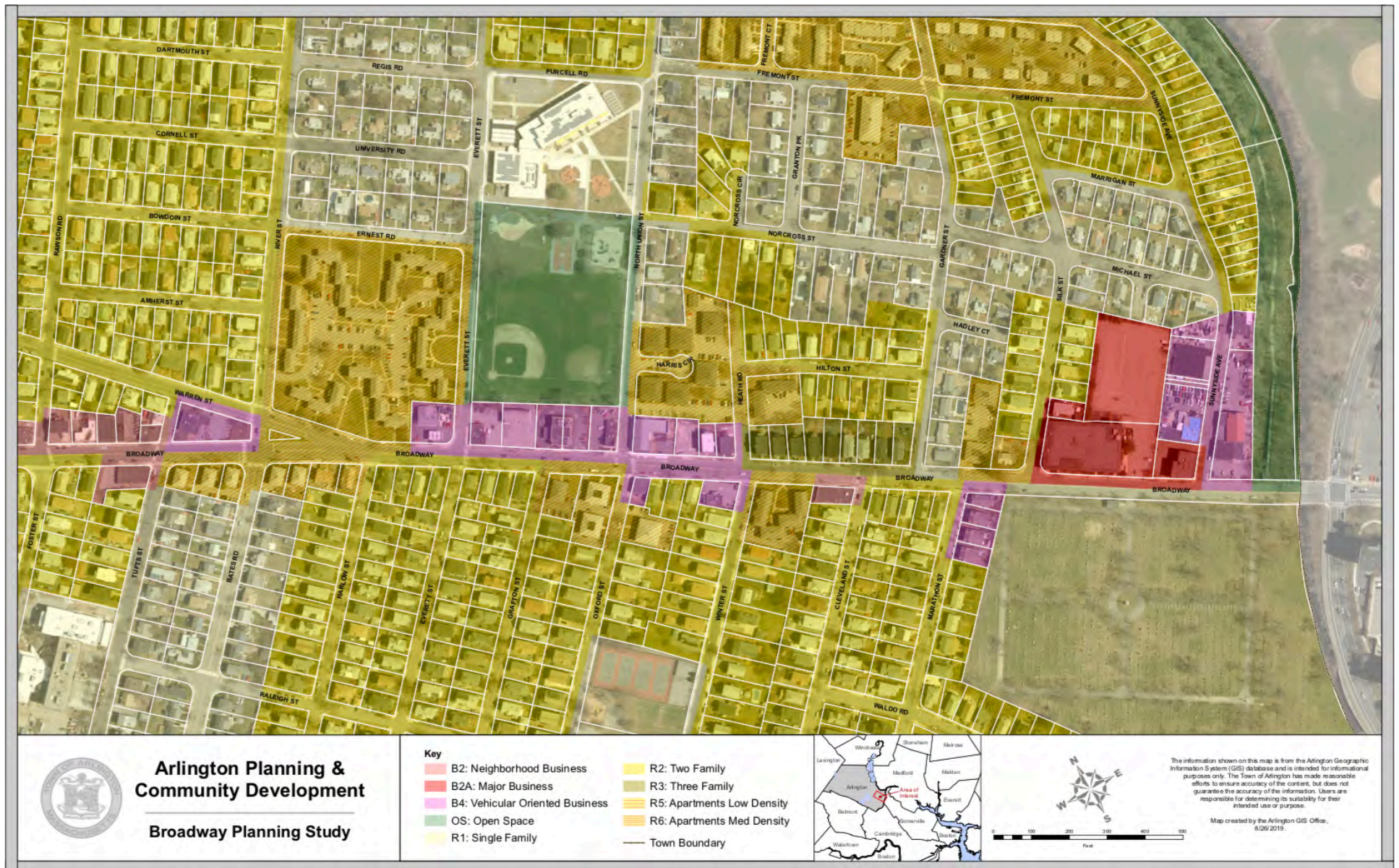


Figure 39. Current zoning of the study area.



# EQUITABLE REDEVELOPMENT THROUGH ZONING

The goals and strategies in the MAPC report suggest amending the current zoning bylaws in order to allow for more variety in housing type and for fewer restrictions on mixed-use development. Currently, the zoning bylaws make it difficult to initiate development projects geared toward increasing density. Additionally, in our tabling sessions, residents expressed that current zoning regulations make it difficult to get potential projects off the ground, citing complications to redevelop the vacant Arlington Automatic Transmission Garage near 111 Broadway.

Recent amendments, however, enable mixed-use development along the Massachusetts Avenue and Broadway commercial corridors once developers obtain a special permit from the Arlington Redevelopment Board (ARB). The study team recommends the following changes to the town's current zoning, building off the work done by the ARB, and guided by the goal of increasing the supply of affordable housing and creating a more walkable and amenity-rich environment along Broadway:

- Review dimensional restrictions on height and density requirements to improve viability of affordable and mixed-income housing developments. While density can be achieved by building taller developments with more units, affordable housing can also be created through techniques like enabling accessory dwelling units.
- Host community processes about how public land may be acquired for affordable housing.



*Figure 40. Residents expressed that current zoning regulations make it difficult to get potential projects off the ground, citing complications to redevelop the vacant Arlington Automatic Transmission garage near 111 Broadway.*

- Examine underutilized parcels for redevelopment (such as the aforementioned garage).
- Prioritize affordable housing development on surplus public land.
- Ensure a high quality of life by activating street life with strategic urban design standards for new development that prioritize pedestrian traffic.



Figure 41. Town design standards for mixed-use development. The study team recommends reviewing dimensional restrictions to improve viability of affordable & mixed-income housing developments. Source: Design standards for the Town of Arlington.

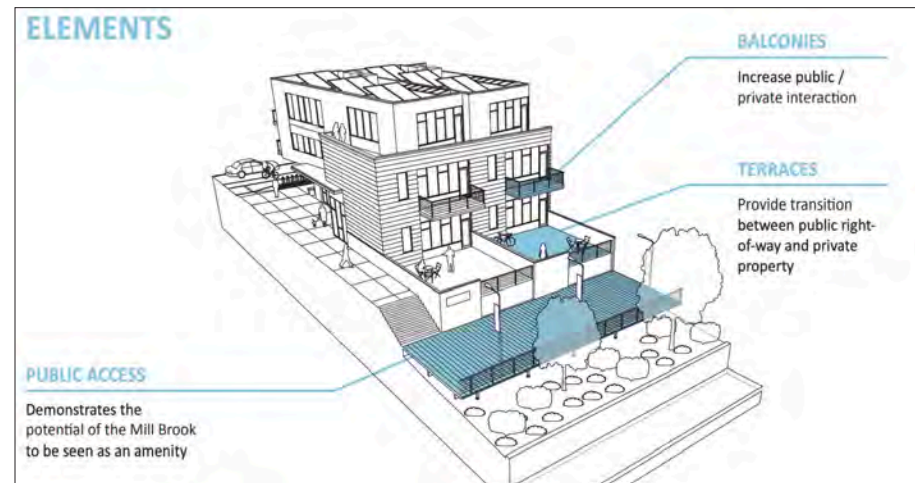


Figure 42. Town design standards for the public realm. This report recommends ensuring a high quality of life by activating street activity with new developments. Source: Design standards for the Town of Arlington.

These recommendations are far from exhaustive and are informed by precedents implemented in other communities to tackle affordable housing issues. Density bonuses have been discussed by the ARB, with proposals to change zoning laws to enable extensive development of R4-R7 areas by reducing requirements on minimum lot area and frontage for those residential areas.<sup>1</sup> Such changes would grant developers more flexibility to build if they agree to make provisions to increase affordable units.

<sup>1</sup> Lefferts, "Arlington Considers Zoning Changes to Boost Affordable Housing."

# ENVIRONMENTAL CONCERNS AND RECOMMENDATIONS

New developments along Broadway can be an opportunity to encourage more environmentally-friendly building design. Arlington should consider:

- Limiting impermeable surfaces to absorb storm water and mitigate urban heat islands.
- Expanding the existing tree canopy to mitigate the urban heat islands and greenhouse gas emissions.
- Encouraging green infrastructure like rain gardens on private properties to absorb storm water.
- Incentivizing green building technologies like green or high-albedo roofing materials to further limit the environmental impact of development.

Any new construction should also be adapted to face future environmental hazards induced by climate change. The Town should consider reviewing the zoning bylaws, and using them as a tool to limit development in future risk-prone areas. One specific recommendation the town should consider is adding guidelines for elevating new construction in the floodplain district to the zoning bylaws (section 5.7).

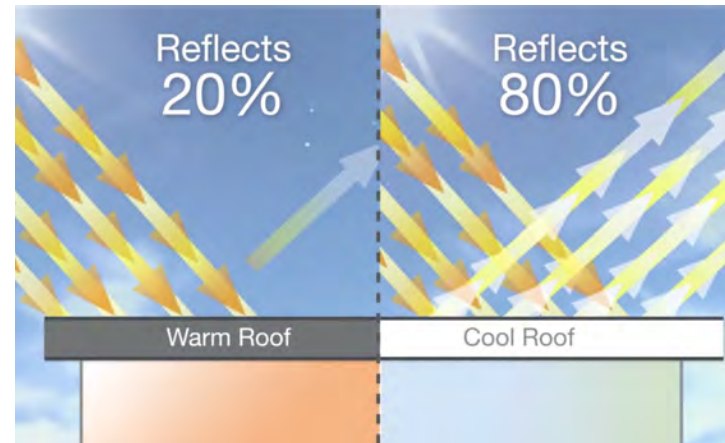


Figure 43. "Cool roof" technology that reflects solar heat might be a good choice for large structures like our proposal for the Lahey site. Source: smmirror.com.

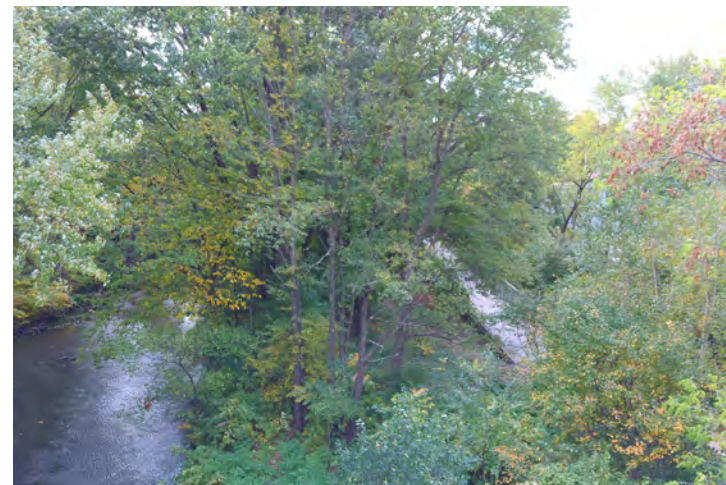


Figure 44. Buildings along Sunnyside Ave and adjacent to the Alewife Greenway (pictured) could face increased flood risk due to climate change.



## FOCUS AREA - THE LAHEY SITE

This planning study has highlighted the Lahey site, situated on the eastern part of the corridor, as a site with high potential for redevelopment. Currently, the site and relevant adjacent parcels along Sunnyside Avenue are zoned for B2A (Major Business) and B4 (Vehicular-Oriented Business), respectively.

The current zoning bylaws for B2A parcels already make allowances for mixed-use development, since this district is proximate to residential areas as is. The current language does not make exhaustive restrictions on uses, but does name automotive, office, and wholesale and storage use as strictly prohibited.

The B4 parcels along Sunnyside Avenue are more restrictive in terms of what may be developed in compliance with the existing zoning designation, since they are narrowly catered to the sale and service of automobiles. Nonetheless, the language of the bylaw “encourage[s] conversion of the property to other retail, service, office or residential use, particularly as part of mixed-use development.”<sup>1</sup>

<sup>1</sup> Town of Arlington, Town of Arlington Zoning Bylaw.

The language of the bylaws noted above reveals the potential of the Lahey site in facilitating more equitable development for mixed uses to spur affordable housing and commercial uses for the corridor. The language further notes: “These areas generally contain retail and service uses that serve the needs of a large neighborhood area.”<sup>2</sup> However, correspondences with residents have revealed the perceived lack of a “vibe” in this section of the town. By offering proposals for the Lahey site, we seek to fulfill desires for a more vibrant Broadway catered to neighborhood desires.

<sup>2</sup> Ibid.



Figure 45. The language of the bylaws noted above reveals the potential of the Lahey site in facilitating more equitable development for mixed uses to spur affordable housing and commercial uses for the corridor.

# CHARACTERISTICS OF PARCELS FOR LAHEY SITE

Parcel	Bldg Gross Area (Sq. Ft.)	Land Size (Sq. Ft.)	Zoning	Use Description	Height Restrictions?
25 Sunnyside Ave.	4,651	5,648	B4	Auto Repair	Yes
1 Broadway	17,669	29,909	B4	Health Spa	No
10 Sunnyside Ave.	8,223	16,500	B4	Auto Repair	Yes
22 Sunnyside Ave.	7,232	11,556	B4	Auto Repair	Yes
0 Sunnyside Ave.	N/A	5,442	B4	Parking Lot	Yes
0 Broadway	N/A	59,020	B2A	Parking Lot	Yes
33 Broadway	50,836	41,476	B2A	Office	Yes

*Figure 46. Zoning regulations and limitations of parcels on the Lahey site. As the Town moves forward with suggestions for this focus site, such limitations on height must be addressed. Note: 0 Sunnyside Ave. and 0 Broadway refer to the parking lots adjacent to Arlmont Fuel and the Lahey building, respectively.*

The parcels for the Lahey site are near single- and two-family housing, medium density apartments, and open space (R1, R2, and OS zones, respectively). Section 5.3.19 of the Zoning Bylaws restricts the height of buildings within 150 feet near the OS zone and within 200 feet near the R1 zone.<sup>1</sup> As the study moves forward with suggestions for this focus site, such limitations on height must be addressed.

<sup>1</sup> Town of Arlington, Town of Arlington Zoning Bylaw.

# LAHEY AREA IMPROVEMENT IDEAS

In addition to the suggestions offered to amend zoning bylaws to better facilitate mixed-used development, this study offers a conceptual proposal for the Lahey site, drawing on the creative skills of the team, community feedback, and projects implemented in other locations.

The current proposal intends to activate the street edge along Broadway by introducing ground-level retail in a five-story mixed-use development with residential units on the remaining floors. To promote integration with the residential neighborhood surrounding the site, much of the proposed height increases are focused along the Broadway street-edge, while buildings along the other site edges taper down to three-story row houses. By activating the street-edge along Broadway, we hope that pedestrians will not only be attracted by the amenities, but also be prompted to stay for a longer period. As such, we also incorporate more public spaces into the site for visitors. In proposing mixed-use development in addition to row houses, the site concept balances enhanced amenities with increased housing supply.<sup>1</sup> Conversations with residents revealed traffic congestion issues, so the proposal also offers a vision for traffic flow and a new traffic signal to alleviate congestion during peak hours.

The site's topography and proximity to the Mystic River and Alewife Brook pose a flooding risk. Future construction along Sunnyside Ave should be elevated, and prevented from constructing basements, in order to mitigate flood risk. The site's proximity to Alewife requires that any development take care to mitigate storm water contamination, by limiting impervious surface area through the incorporation of green space and green building technologies. Development proposals for this site should also address urban heat islands in the Broadway corridor. For the Lahey site in particular, peak land surface temperatures range from 94°F to 97°F.<sup>2</sup> As we are seeking to increase building density at this site, the new development's contribution to localized heat islands should be addressed at a minimum with assurances to increase tree canopy cover, reduce asphalt coverage, and incorporate high-albedo roofing technology.

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<sup>1</sup> Dain, "The State of Zoning for Multi-Family Housing In Greater Boston."

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<sup>2</sup> Cabrera, "Arlington Tree Inventory Project."



# LAHEY AREA IMPROVEMENT IDEAS

## *Envisioning an Entrance to Broadway*

In addition to the Lahey site's potential to shoulder a share of new housing production coupled with new commercial activity, this study envisions the site as fostering community connection for current and future residents. We note the residential units already neighboring the site, and seek to offer a vision that welcomes them to a vibrant node along Broadway. Our suggestions also incorporate ideas of sustainability, noting the current flooding issues at the site.

By activating the street edge to allow for commercial activity and space for new housing, we seek to make the site a more welcoming place for all.

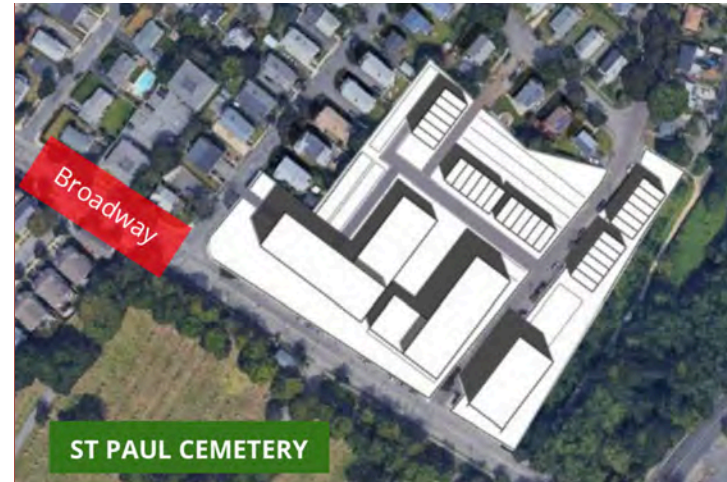


Figure 47. The master plan of our proposal envisions the Lahey site fostering community connection for current and future residents.

Use Type	Total Area Size (sq ft)	Estimated Total Units	Parcels
Retail	31,800		Distributed on the ground floor of apartment buildings.
Apartments	133,200	144	19-23, 33 Broadway; 1 Broadway 0 LOT Broadway; 22 Sunnyside Ave
Townhouses	108,000	30	0 LOT Broadway; 25 Sunnyside Ave
Parking spaces		160	Distributed across the whole site.

Figure 48. Programming for the Lahey site. By activating the street-edge to allow for commercial activity and space for new housing, we seek to make the site a more welcoming place for all.



Figure 49. Parking would be provided on the North side of Broadway for Lahey visitors, but would be removed from the South side of the street to make space for a bus queue-jump lane approaching Alewife Brook Parkway.



# CONCEPTUAL DIAGRAMS FOR DISCUSSION



Figure 50. The northern and eastern part of the site should reserve sufficient open space as community assets for the benefit of existing and future residents. Building heights along Broadway and Sunnyside Ave. can be denser than the interior of the site. The massing of buildings should be planned to promote sunlight exposure of the open space and apartment units.



Purple = flow of traffic

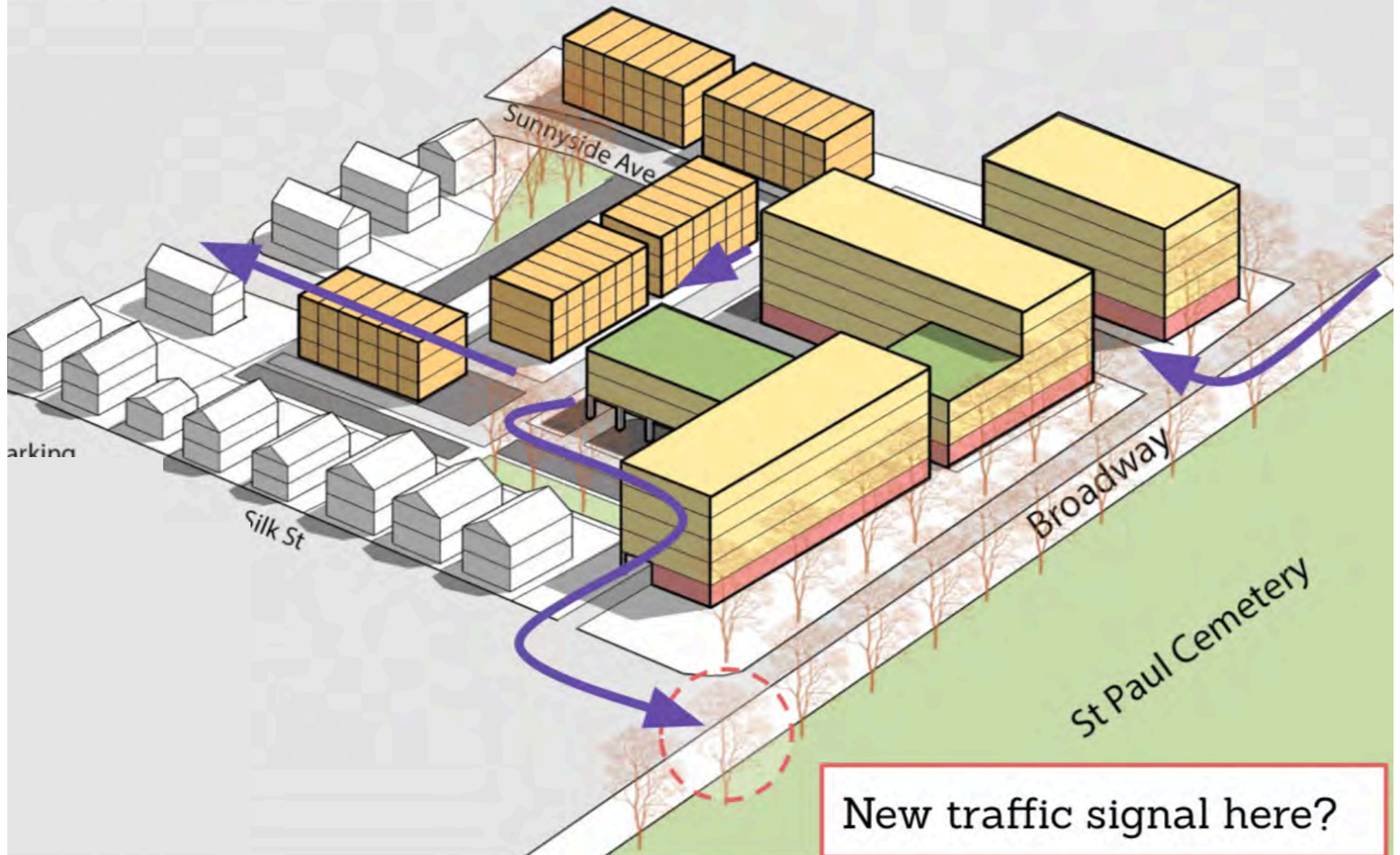


Figure 51. Sufficient buffer areas should be provided for existing houses to the east and north of the site. Circulation plans for future development in this area should avoid exacerbating congestion issues at the intersection of Sunnyside Avenue and Broadway.

# SITE RECOMMENDATIONS

## *Site Planning and Design Principles*

As part of zoning changes, we suggest that the site planning and design for the Lahey site shall prioritize the following principles:

- Sufficient buffer areas should be provided for existing houses to the east and north of the site.
- The northern and eastern part of the site should reserve sufficient open space as community assets for the benefits of existing and future residents, while developments along Broadway and Sunnyside Ave. can be denser than the inner site.
- Based on the orientation of the site, the massing of buildings should be planned to promote sunlight exposure of the open space and apartment units.
- The circulation of vehicles within the future developments should follow the paths outlined in our site proposal to avoid exacerbating congestion at the intersection of Sunnyside Avenue and Broadway.
- Future mixed-use developments should activate the ground-floor by orienting building entrances to face Broadway and by limiting building setbacks.
- Parking spaces for apartments should be planned for the rear side of buildings rather than the side facing Broadway to create a better pedestrian experience.



Figure 52. This planning study has highlighted the Lahey site, situated on the eastern extreme of the corridor, as a site with high potential for redevelopment.

# Housing Recommendations

The following items represent a summary of our housing recommendations for the Broadway corridor:

- Review dimensional restrictions on height and density requirements to improve viability of affordable and mixed-income housing developments.
- Examine underutilized parcels for redevelopment.
- Ensure a high quality of life by activating street activity with new developments.
- Incorporate environmental hazard mitigation techniques like permeable surface requirements and tree planting into all new construction.
- Add housing density to the neighborhood by redeveloping the Lahey Building and adjacent parcels.



An aerial photograph of a city street intersection. The street is paved and has white crosswalk markings. Several cars are visible, including a red car, a white van, and a silver car. The surrounding area is filled with trees, many of which have yellow and orange autumn foliage. In the background, there are buildings, including a large red brick building and a smaller white building. The overall scene is a typical urban environment.

# **IV. NEIGHBORHOOD CHARACTER**



# WHAT WE HEARD

Talking to people is always useful when you are studying a place. People populate it, use it, like it or dislike it, actively engage in it or simply disregard it. Throughout our community outreach process, the emptiness of the street and the lack of amenities and ‘things to do’ was one of the most commented and agreed upon observations.

If you look at the urban typology on both sides of the street, you realize the corridor is the meeting point of two different worlds: a subdivision landscape and a slightly more dense, suburban typology. This mix does not naturally facilitate pedestrian-friendly street life. At present the design of the corridor and the lots around it are not inviting people to get out of their cars, or indeed, go out of their way. One resident pointed out that he deliberately chooses other streets for his daily chores as Broadway does not seem inviting.

*“It’d be great to bring more of the vibe of Arlington Center down here.”*

This comment was in line with the preferences expressed by many of the people we engaged with.

In the following pages we summarize what we see as present conditions and how they can be improved to animate the corridor to make it a more lively and friendly to pedestrians.



Figure 53. The lack of life on the street is one of the first things that you notice when you walk down the corridor.



Figure 54. The most walkable part of Broadway is where it meets Mass Ave and creates a “hub” for people to gather.

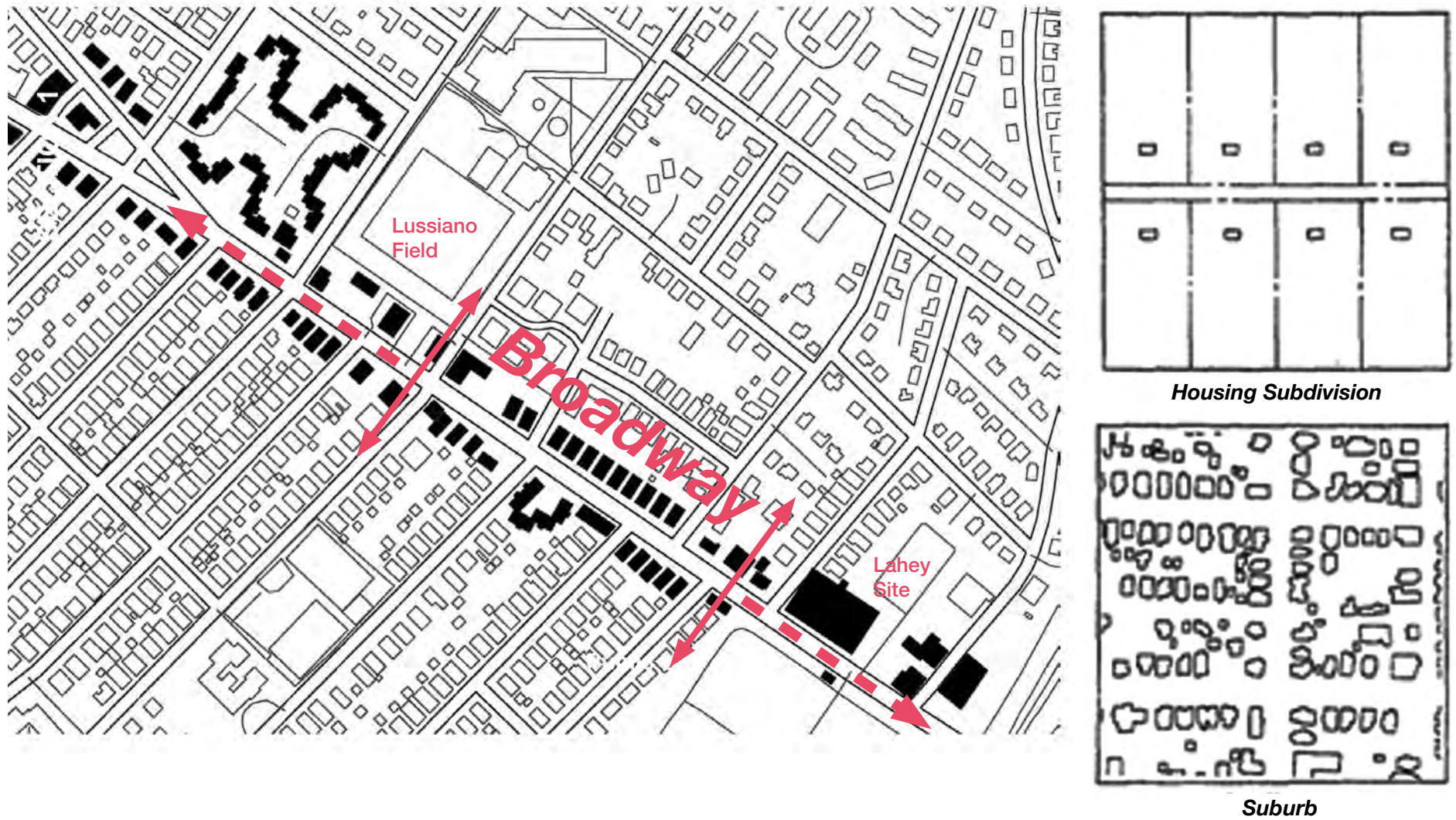


Figure 55. The density of the surrounding housing differs on the north and south sides of Broadway. The north side is most similar to a housing subdivision, while the south side looks like an older suburb. Strategic interventions along Broadway could facilitate greater cohesion of these two different neighborhoods, and extend a coherent neighborhood character along the entire corridor.



# CURRENT CONDITIONS

The Broadway corridor has many positive features that residents appreciate. In our public outreach, residents mentioned that Lussiano Field is a wonderful open space that will only get better when the new splash pad is constructed. Moreover, the cluster of schools and kid-oriented uses (like the dance studio and daycare center) give the neighborhood a family-friendly feel, particularly right when school gets out.

At the same time, Broadway is lacking many amenities that residents desire. It is under-served compared to most other hubs in Arlington. With only a few food and drink options in the neighborhood, residents need to travel to Arlington Center or Massachusetts Avenue for their daily needs. These areas are well beyond half a mile for parts of the corridor, which motivates car-use rather than pedestrian access.

Broadway's main gathering point — the “Dunkin’” on 115 Broadway — is an example an existing amenity of the corridor. It is the most visited spot for residents and passers-by throughout the day but its design and purpose does not encourage people to hangout and stay a while.

*“Why don’t we have the kind of coffee shops and restaurants that others have?”*

The overall feel of Broadway is “auto-oriented,” with a very wide street lane with parking on both sides, lots designed with drive-thrus, sidewalk curb cuts for firetrucks and a lack of bike parking (see the Mobility section).

The study area’s biggest public asset lies hidden behind “Dunkin’” and several neighboring lots — the Lussiano Field. It is a valuable and underused space. Notwithstanding the fact that it is visually and physically disconnected from the corridor, parts of the field are not well-maintained and lack basic facilities.



Figure 56. “Dunkin’” is the most-often visited spot in the corridor, but it is designed as a pass-through place.

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# ENHANCING NEIGHBORHOOD CHARACTER

Broadway offers enough space and opportunities for better use of this corridor. It provides a vital connection between Somerville, Cambridge and the other parts of Arlington.

The street has the possibility to simultaneously be both a “stop-by” but also a “go-to” place. It can provide a welcome break in a busy day, as well as a good social environment.

Many residents expressed concern that the area should not become like Mass Ave, but also desired some changes to make it a more active place.

Some of the steps to address the needs of the community for a safer, more walkable Broadway, with a range of amenities that serve the surrounding neighborhood are as follows:

## 1. Improving the streetscape

The Mobility chapter introduced some ways to achieve a more accommodating streetscape, in terms of pedestrian and bicycle safety. Protected bike lanes, safer, shorter, more visible crosswalks, hospitable bus stop furniture, and a greener “mobility environment” are intended to attract residents onto the street and keep them safe while there. However, an attractive streetscape can also emerge in the ways typical pedestrian facilities are implemented, through techniques like patterned sidewalk paving and shade tree plantings.



Figure 57. Residents need a more accommodating streetscape for pedestrian and bicycle safety.

## 2. Providing better amenities

Dunkin' is an illustration of the need for new places to gather. Another, even better example of the use of space is the nearby wine shop. It holds a weekly tasting, which attracts people and is a gathering opportunity. Providing connections to other walkable amenities and facilities nearby would create a positive impact on the community.



Figure 58. More informal gathering places, like the existing local wine shop, are desired by local residents.

## 3. Using available space

Adding new amenities depends in part on new development. New development may be more likely to happen in the lots in front of Lussiano Field. It is in the B4 zone (vehicular oriented business), which means a large amount of land in proportion to building coverage. The biggest impact to public realm from the existing zoning is heavy vehicular usage in this area contributing to congestion along the corridor and low utilization of valuable land.



Figure 59. More can be done with the available space in the neighborhood.



# ENVIRONMENTAL CONCERNS AND RECOMMENDATIONS

Residents of the Broadway corridor have ready access to green space in their immediate neighborhood (Lussiano Field, the St. Paul Cemetery, Crosby Field, Alewife Greenway), and within walking or biking distance (the Minuteman Bikeway, Spy Pond, Magnolia Park). However, the neighborhood is challenged by heat in the summer, which makes it difficult to spend time outdoors. The splash pad at Lussiano Field was spoken of as a major destination for parents with children in the hot summer months, but Arlington should do more to make spending time outdoors more comfortable.

In addition to ideas mentioned in previous chapters, the town should:

- Add more trees to the north side of Lussiano Field.
- Partner with local businesses on a tree planting campaign, where the town could pay for saplings planted by business owners who have the space and ability to maintain trees.



Figure 60. Map of existing green space (highlighted in green) near the Broadway corridor (study area circled in blue).

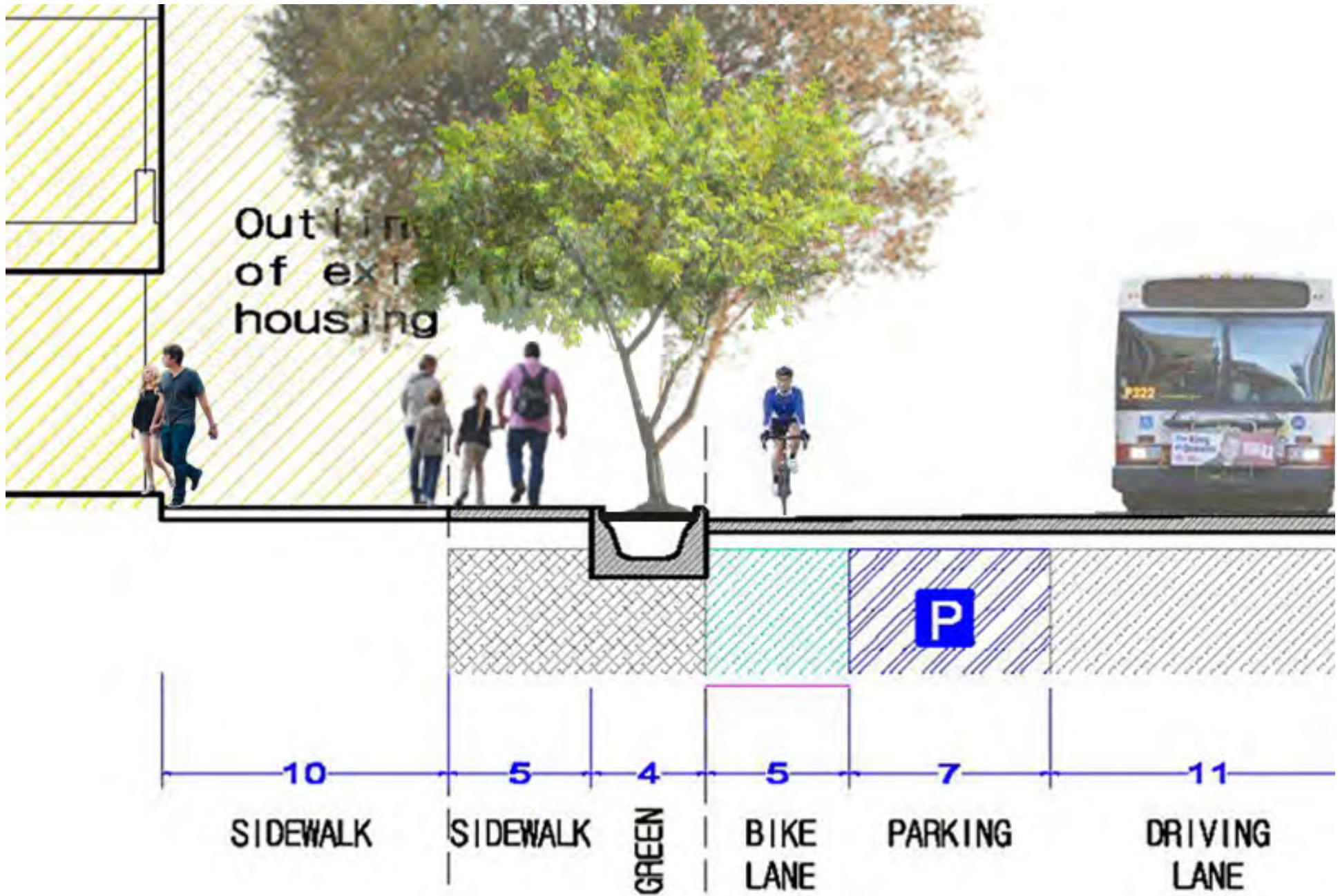


Figure 61. Leaving room along the corridor for street trees will add shade, bringing more people onto the street during the temperate months.



# THE FOCUS SITE - LUSSIANO FIELD

Lussiano Field is located between the Thompson Elementary School and Broadway. A small part of it is now redeveloped as a playground but the main facilities there are a basketball court, a soccer field and an old baseball diamond.

Lussiano Field is the property of the Town of Arlington and is maintained for recreational uses. Yet it could use better facilities to make it more inviting. There are no spots for bike parking, which limits accessibility via that mode of transportation. There are not many available spaces to sit down and the existing benches are not well maintained. There is a lack of proper lighting, drinking fountains, appropriate signs and public restrooms.

It is also a prime example of how open spaces can “disappear” in cities.<sup>1</sup> The field itself is lower than the streets surrounding it, so it visually “sinks” beneath the eye level. It is surrounded by a fence and has a sharp “edge” on the southwestern side where it meets the lots on Broadway. The lower topography of the field compared to the surrounding streets makes it difficult to access. While there are staircases on Everett and North Union St., both are steep and in need of maintenance.

<sup>1</sup> Whyte, The Social Life of Small Open Spaces.



Figure 63. Blocked pedestrian access from the Broadway side. As there are no spots for bike parking, cyclists are forced to leave their bikes unsecured against the wall.



Figure 62. Lussiano Field sinks below curb along N Union St.





Figure 65. Physical and visual access to the park is blocked from Broadway.

Figure 64. The amenities at Lussiano Field, particularly on the south side of the park, could be improved.

# NEW VISION FOR LUSSIANO FIELD

After assessing the preexisting condition of Lussiano Field, we decided that there is an opportunity for enhancement at that site. As inspiration, we looked at various examples of how public spaces are connected to the built environment in other cities and towns.<sup>1</sup>

Our vision for the field is based on the idea that an improved connection with Broadway through a well-planned development can create a positive impact to the quality of life in the neighborhood.

The lots between the park and Broadway can serve as a gateway to the park and invite people in while also providing a good public space for various activities that are presently lacking. If they are developed together, which should be possible with the necessary incentives from the Town, or even developed separately but with an overall emphasis on connectivity, the new site design could enhance the character of the corridor.



Figure 66. A good example of connectivity between a street and a park is this Chilean library "Biblioteca pública parque Bustamante."

<sup>1</sup>Fleming, "Questions to Ask a Space."





Figure 67. One alternative scenario for outdoor open space.

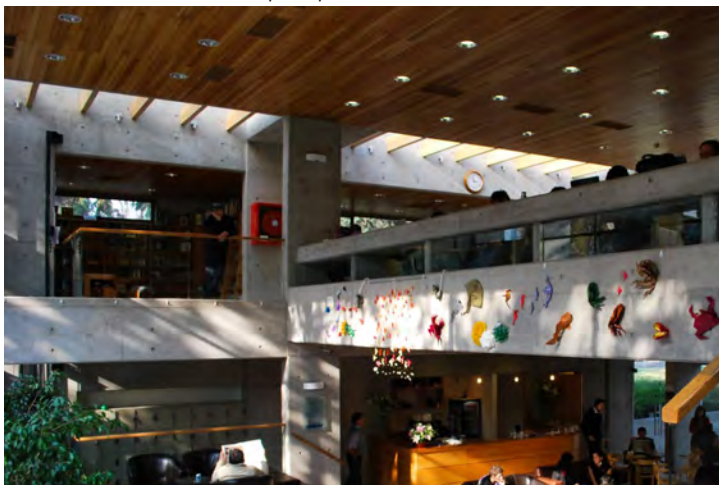


Figure 68. An indoor communal gathering space.

## Our Vision

The lots in front of the field and facing Broadway are the biggest opportunity create a communal public space along the corridor. All of them are in the B4 zoning district.<sup>2</sup> One was recently acquired and will be soon developed by the Housing Corporation of Arlington with a new 4-story building with apartments, commercial space and parking.<sup>3</sup>

If the other three lots are developed together, they can “unlock” the entrance to the field and turn the “hard” edge between the street and Lussiano into a more welcoming environment. With the same height as the currently approved building on 117 Broadway, this development could include public spaces, amenities like a restaurant, and a community space for residents.

This new development could serve as a meeting spot for locals, as well as an attractive place to walk and bike to. Parents with children could easily use the new location and the retail spaces, which would also bring new income into the town.

There is a bus stop at the intersection of Broadway and North Union St., as well as on Broadway and Harlow St. that connects this site to Arlington Center and also to Somerville, Davis Square and the Red Line. The transit connection makes the Field accessible for local residents as well as people from outside of Arlington.

The following are conceptual examples of designs that can be used to activate the lots in front of the Field. The exact structure and building design will depend on potential zoning changes, town requirements and developer conditions.

<sup>2</sup> Town of Arlington, Town of Arlington Zoning Bylaw.

<sup>3</sup> YourArlington.com, “Affordable Housing at Downing Square, Broadway Gets Funding.”



# DESIGN PROPOSAL

This concept proposes two mixed-use buildings combining housing, commercial uses, a place for community gathering and an open public realm. The commercial zone would be on the ground floor, and would include retail spaces and a community center. Forty-five residential units, split across the two main buildings, occupy the upper floors developed to a maximum of five stories. The buildings should be placed to provide an open meeting space between the structures that also functions as a pathway to the park.

Parking should be considered on the basis of one parking space per unit. It could be placed in the back of the parcels, lower than the ground-level, so as to not act as a visual barrier between the park, the development and Broadway. This design will maintain the idea of the development as a gateway to the park. Bike parking should also be included, as well as small, private courtyards for each building.

Lussiano Field is a large park, yet it does not welcome everyone to take part in the use of this space. Changing the design and programming of the Field can offer a range of options for individuals or groups of different sizes — people who want to enjoy it in solitude, as a couple, in intimate groups, or as part of a larger event.



Figure 69. This concept proposes two mixed-use buildings, with public space in the center of the site that provides access to Lussiano Field.

Level	Use	Square feet (ft²)	Number of units	Total Square feet (ft²)
First floor	Commercial use	6900	—	6900
	Community space	2100	—	2100
Second and Third floor	Residential	35400	34	49000
Fourth floor		10300	10	
Fifth floor		3300	3	
TOTAL UNITS			47	

Figure 70. Uses of the proposed new development includes a community space where locals can gather for meetings or events.

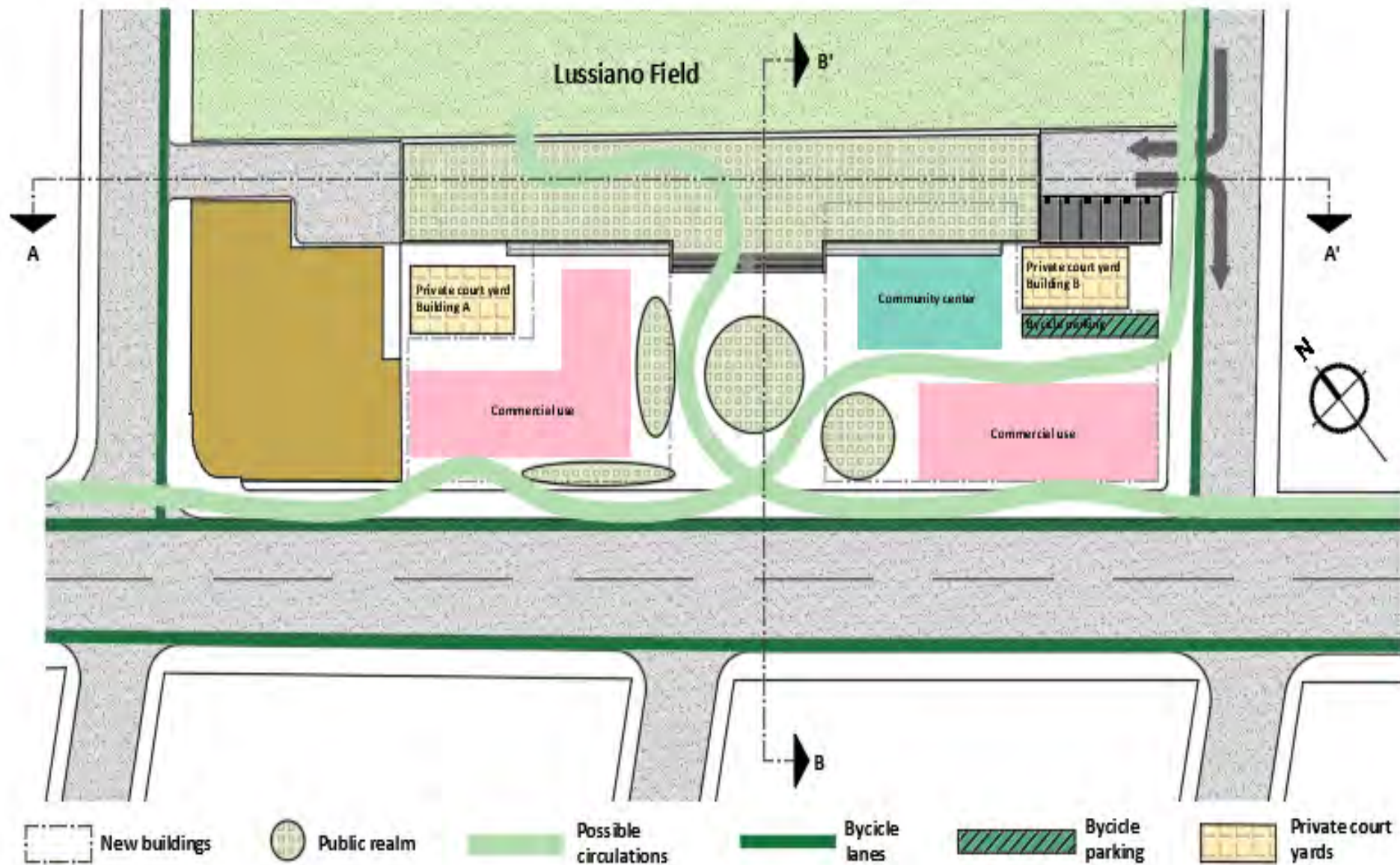


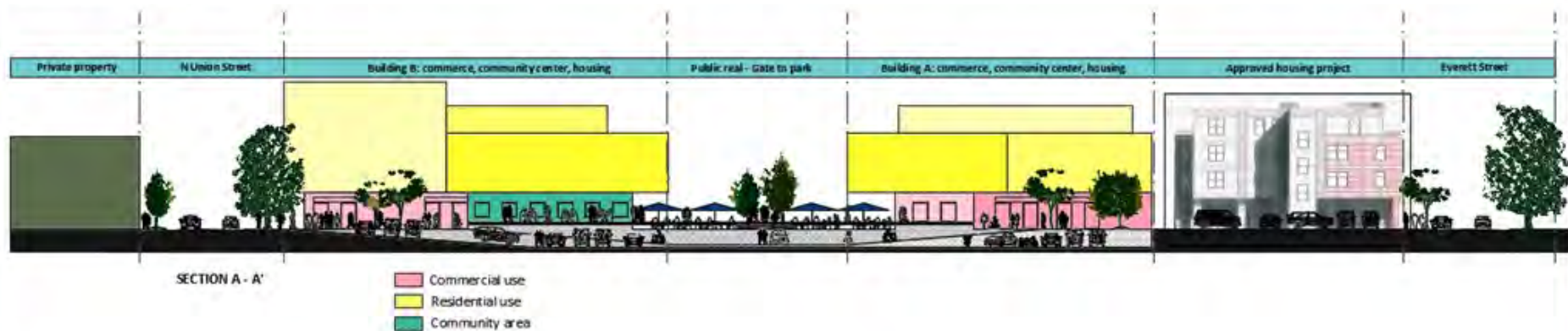
Figure 71. The Lussiano Field future site vision is an idea for how a future development could help open the park to Broadway, and includes mixed-use buildings with housing, retail and restaurant space.



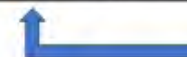


Figure 72. Possible design configurations for the Lussiano site. Commercial edges encourage people to gather along the sidewalk, while landscaping and outdoor furniture prompt visitors to spend time outdoors on the site.





*Detailed view of parking access*



*Concept of parking under public terrace*

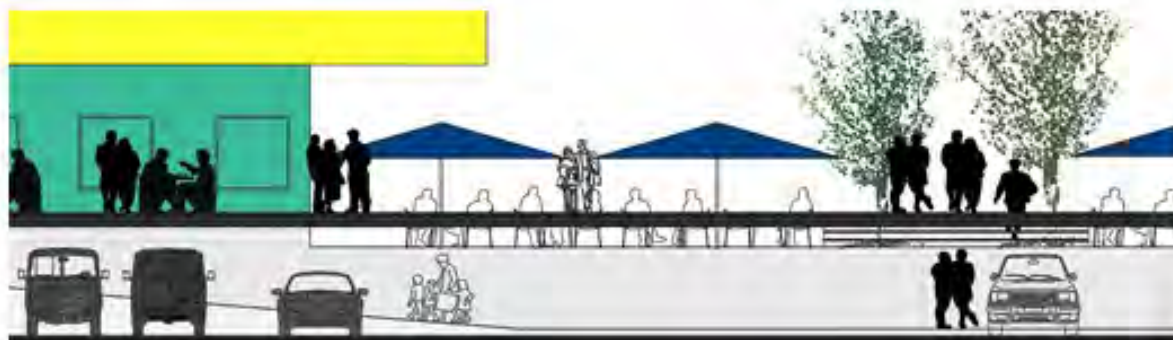


Figure 73. Section A-A' shows the possibility of accommodating parking for residents of the mixed use development project. Parking could be placed along the backside of parcels facing Broadway and Lussiano Field, allowing the public to access the field without passing through a parking lot.

# PUBLIC SPACE DESIGN PRINCIPLES

A well-maintained public space is not only inviting for people but serves as a message about the dedication of the local community to a certain quality of life. Town officials should consider the following points regarding Lussiano Field:

- Users should be able to easily navigate any public space. Currently, the park maintains clear visual lines from the side streets but it does not necessarily integrate with the surrounding neighborhood. There are no signs or elements that provide information about the park, and nothing on Broadway indicating that the park exists.
- This public space is directly adjacent to the busy Broadway roadway. The development of Lussiano Field should go hand in hand with streetscape improvements and allow for uninterrupted pedestrian traffic across Broadway while slowing down any car traffic. Given the idea of redeveloping the lots along Broadway and opening the park to the street, it is inevitable that visits to the park will increase. The Town should consider improvements to the bike and pedestrian infrastructure as well as better transit connections, so that most of the new traffic is done without cars.
- Arlington is a cycling community and Broadway itself is a fairly popular route (see Mobility section). Installing bike racks, combined with spaces to sit down, relax, and enjoy the view would be a good fit for the area.
- The field itself needs better lighting. Presently, it lacks both sufficient nighttime lighting and daytime shading, which contributes to its underutilization. After dark, it disappears even more into the neighborhood. We recommend using lighting that avoids contrasts between excessively bright and dark areas, and includes some ambient lighting in addition to floodlights for the sport fields.

# Neighborhood Recommendations

- Activate the ground floor along Broadway, through improvements to the built environment and encouraging street-frontage retail spaces in new developments for restaurants and small businesses.
- Encourage temporary and tactical activation of the streetscape, such as parklets and street festivals.
- Activate Lussiano Field
  - Engage future developments to provide visual and physical access to the field from Broadway.
  - Change the zoning code of the lots to allow for greater density in return for more public space and amenities if developed together.
  - Create bike parking and public spaces, and renovate facilities.
  - Preserve and expand the existing tree canopy on the corridor.
  - Ensure that new construction responds to current and future climate hazards.



# V. CONCLUSIONS

# THE FUTURE OF THE BROADWAY CORRIDOR

The Broadway corridor needs a new vision to guide its evolution and help the entire neighborhood thrive. Our analysis and conversations with community members highlighted the need to rethink safety and walkability on the street, maintain a healthy housing supply in the surrounding neighborhoods, as well as improve and preserve the corridor's vibrancy and residents' quality of life. The recommendations we made to attain these goals are summarized below:

## ***Mobility***

Broadway as a street hosts many different modes of transportation—including auto, transit, bike, and foot travel—but has minimal infrastructure for bikes or transit. Additionally, many aspects of its existing pedestrian network are unsafe. We envision adding bike lanes on Broadway in both directions, using street space from a removed lane of on-street parking where necessary and appropriate along the corridor. For pedestrians, we recommend adding high-visibility upgrades to crosswalks at key intersections, which are mindful of school walking routes. New trees and sidewalk furniture would benefit pedestrians as well as bus riders waiting at stops. In the long term, communications with the MBTA and the city of Somerville are warranted. We recommend one intersection traffic study and one intersection redesign, to comprehensively address the safety and congestion problems borne by multiple modes of transportation at key nodes.

## ***Housing***

As Arlington prepares for growth envisioned in its Master Plan, and housing affordability in particular, zoning will remain a vital tool for the path forward.

Recent attempts to amend zoning bylaws, however contentious they have proven to be, present an opportunity to examine how growth can be fostered with full consideration of the needs of current residents. Moreover, while more recent projects to increase the housing stock have rightfully targeted the concerns of low-income residents, future efforts should also seek to increase available housing for middle-income residents who may also find it difficult to afford existing market prices in Arlington.

We recommend that the Town leadership continues to plan for increased density through zoning changes, but keep an eye on how future climate changes might impact development patterns. The Town already has tremendous assets that can be leveraged to meet the goals outlined in its housing production plan. This planning study has targeted the Lahey Building as a potential site of intervention. However, the town should target broader zoning revisions to increase density along the corridor.



### ***Neighborhood Vibe***

The residential feel of the neighborhood surrounding Broadway is beloved by its residents, but locals want to see more amenities along their main thoroughfare. We recommend redesigning the streetscape to get people out of their cars, onto the sidewalks and into local businesses.

We believe that new businesses established along the corridor could function as useful community amenities for locals, informing our recommendations to encourage redevelopment of underutilized space along the street. This report re-envisioned parcels currently adjacent to Lussiano Field as a core community gathering space contributing housing variety, providing new retail space, and creating an outdoor living area for the entire neighborhood to enjoy.

Our recommendations do not embody a comprehensive neighborhood plan, but rather an ambitious end-state-driven vision for the Broadway community and Town planners upon which to build in the future. Some ideas may be manifested in near-term pilot projects, while others may need more study and political finesse. We were impressed by the level of community engagement and interest in this neighborhood study, and hope that the Town will adopt some of the community's requests as new neighborhood improvement projects.





An aerial photograph of a city area. A diagonal yellow line, representing a street, runs from the top left towards the bottom right. Along this line, several rectangular areas are highlighted with a yellow-to-magenta gradient. In the top left, there is a large green field. In the bottom right, there is a large building complex. The rest of the image shows a dense urban grid with many small buildings and trees.

Lussiano Field

Broadway

Silk St.

Lahey  
Building

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## Town of Arlington, Massachusetts

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### Meeting Minutes (12/2/19)

#### Summary:

9:20 p.m. -

- Board members will review and may approve minutes

9:25 p.m.

#### ATTACHMENTS:

Type	File Name	Description
▢ Reference Material	Agenda_Item_9_-_12022019_Draft_Minutes_ARB.pdf	12022019 Draft Minutes ARB

**Arlington Redevelopment Board**  
**Monday, December 2, 2019, 7:30 PM**  
**Second Floor Conference Room, Town Hall Annex**  
**Meeting Minutes**

This meeting was recorded by ACMI.

**PRESENT:** Andrew Bunnell (Chair), Eugene Benson, David Watson, Kin Lau, Rachel Zsebery

**STAFF:** Jennifer Raitt, Director of Planning and Community Development and Erin Zwirko, Assistant Director

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The Chair called the meeting to order and notified all attending that the meeting is being recorded by ACMI.

The Chair introduced the first agenda item, Broadway Corridor Student Project Presentation. The MIT Department of Urban Studies and Planning Broadway Corridor Student project. Ian Ollis, Griffin Kantz, and Kendrick Manymules are among the group of students from MIT and Harvard who worked together on a practicum at MIT in planning. The Broadway Corridor was their project for this semester, which has not been studied by the town for some time. The students are investigating the corridor and working on suggestions for improvements in the future.

The students began the project in September by meeting with the Planning and Community Development Department staff, attending Town Day, conducting interviews, and met with community groups. In November the students began meeting with stake-holders. The students planned community engagement events, spoke to residents, businesses, and sent emails to collect information and understand what they like and dislike about the area. The student in-reach group met with Tree committee, Transportation Advisory Committee, Police Department, and the Recreation Department. The areas of focus were housing, mobility, and neighbor character.

Griffin Kantz introduced the housing findings related to housing around the corridor. The students found that 37% of residents are cost burdened and over 11% of households spend over 50% of their income on housing in this area. They found the number of residents that rent versus own is even in the Broadway Corridor area. The students found that the Broadway Corridor is one of the densest areas of Town but there are not many attractions in the area. The students learned from a demonstration at Town Day, conducted by Sustainable Arlington, that housing affordability is one of the top concerns for Arlington residents. There is room to allow the corridor to grow and allow for more density for the Town.

The students' housing recommendations are: 1) Incentivize and shape affordable housing growth along the corridor by reviewing height and density restrictions, host community processes about how public land may be acquired for affordable housing, and examine underutilized land parcels for redevelopment. 2) Ensure high quality of life on Broadway by addressing hazards due to flooding and extreme weather, and add ground-floor retail to contribute to street life.

The students found reliable regional transportation is important for the corridor. The corridor has sidewalks on every street, tree shading on side streets, and crosswalks. There are some safety concerns which include a long street width to a lack of crosswalks, insufficient tree shading on Broadway, and safety concerns at intersections such as Warren and River Street. Suggestions to improve mobility include: curb extensions, and daylighting, which is taking parking spots one or two spaces before the cross walk to increase visibility. The students found that the amount of bike traffic along Broadway is almost as busy as Mass. Ave. and the bikeway but there is no in bike infrastructure of any kind on Broadway. There is space for bike lanes on Broadway and there is a safety need. Mass. Transit busses 87, 88, and 89 travel through the Broadway corridor but there are no benches or bus shelters like there are elsewhere in Arlington. The students recommend a bus only lane approaching Alewife Brook Parkway from Broadway to help mitigate traffic congestion east-bound in the morning. Additional corridor-wide recommendations include: make walking routes to school safer, advocate for Sunday service on the 87 MBTA bus,

improve safety and traffic flow for all modes at Sunnyside Ave. and Alewife Brook Parkway, improve safety for all transportation modes at the Broadway/Warren St. intersection and possibly redesigning the intersection.

Kendrick Manymules presented the final focus area, neighborhood character. Mr. Manymules said the residents interviewed liked the current neighborhood, but would add more trees along the streets. They also like “traditional- style” residential buildings and Lussiano Field. The students suggested working to enhance the street scape, make use of vacant or unused lots, “dead” facades, and parking surrounding buildings. The students said they expect new vibrancy with the addition of businesses and new construction at 117 Broadway. Residents have access to several green spaces within walking distance. Environmental challenges include flooding along the brook, urban heat island effect, and stormwater contamination. The students reported that residents said that they would like the corridor to reflect more of the Arlington Center vibe. The students’ recommendations include changing the area from a “Grab and Go” to a “Come and Stay” atmosphere by activating the ground floor of the street scape. The suggestions are to: encourage temporary and tactical activation of streetscapes; activate Lussiano Field and open up the space to the neighborhood; preserve the neighborhood environment; and preserve and expand the existing tree canopy along the corridor.

The report will focus on specific sites are Lussiano Field, the Lahey site, and the Broadway Streetscape as a whole. Further outreach, research, field visits and classwork with design principals will be conducted by the students to finalize concept designs. No park access to Lussiano Field from Broadway, Mr. Manymules reviewed some possible designs for future corridor improvements for the Lussiano Field area. For the Lahey site, the team suggested design improvements include ground level retail, apartments, and townhomes. To address the street scape, a design with bike lanes and more open pedestrian friendly uses for the street were suggested.

The Chair asked the Board if they had any follow up questions for the students.

Mr. Lau thanked the students for their recommendations. Mr. Lau said that he did not see a balance between their recommendations and the needs of the residents, with parking for example. Mr. Lau said he likes the suggestions for the Lahey property but that would be one massive project with many owners involved. Mr. Lau asked if the students figure out a way to phase any development. Mr. Kantz said that the student group wanted to present a vision of what might be possible for the corridor, something visionary and inspiring. Ognyan Georgiev said that the students did meet with the owner of the Lahey building to do some ground work for the project. Mr. Kantz acknowledged that there are conflicting needs between the need for increased density, additional parking, and space for bicyclists. Mr. Kantz suggested not removing parking for bike lanes in front of businesses and residences.

The Chair said that he appreciates that the students talked to so many stakeholders involved to propose improvements for an area of Arlington that has not been thought of in a while. Mr. Watson said that he appreciates the amount of work the team completed in a semester and the amount and range of public engagement activities. Mr. Benson said he thinks the challenge for the town would be to take the aspects of the final report that they like and make it happen over a period of time with community input. Mr. Benson said that he feels that this study will be an important building block to help guide the town. Ms. Zsembery said she thinks the amount of public input is great. She asked if there were any ideas that were interesting but deemed too aspirational to make it into the final report. Mr. Gorgiev said the Warren intersection was the third site they were looking at and can really be opened up for public space.

The Chair opened the floor to comments and questions from the public.

Barbara Thornton said she loved that the students presented a vision. Ms. Thornton said that she was blown away by the statistics, especially by the number of one and two person households. Ms. Thornton said that Arlington is thought of as a town of families but those statics show clearly it is not only that. Ms. Thornton said she would like to have the students come back with a vision that provides more details about these people and their housing



needs. Ms. Thornton asked if that represents a real demand. Ms. Thornton asked if people may be living in inappropriate housing units, and if so is there a demand for additional housing units.

Leo McCue from Arlington Taxi said that at Broadway and Gardner Street roadway may need to be resurfaced. He also noted a street, which is a private way, should be improved because it is a big problem and has a huge effect on traffic.

Christopher Lorette said he was not surprised by the number of single person households found in the study. Mr. Lorette said that area is the most affordable area for single units. Mr. Lorette asked the students if the proposed buildings are five-stories and if they are allowed by current zoning code. Mr. Kantz said that the entire proposal for the Lahey site would not be possible with the current zoning regulations. Mr. Lorette asked about the bicycle use on Broadway, being the same volume as Mass. Ave. and the Bikeway. Mr. Kantz said that the data is from the Strata and Lime Bike applications, those were the data sets the team used to draw the bike usage totals.

Patricia Worden said that the need for affordable housing in the entire metropolitan area is great. Ms. Worden said that Arlington has over 1,000 subsidized affordable units and probably several thousand naturally affordable units. With the addition of more dense construction there would be incentive for developers to demolish those naturally occurring affordable units which will lead to evictions and displacement of those tenants. Mr. Kantz said they should keep in mind the effects of evictions due to gentrification. Mr. Georgiev said that the students did not propose re-zoning.

Beth Melofchik asked if the presentation is available to the public. Ms. Raitt said that the presentation is available with the online agenda for this meeting and with a news item on the Redevelopment Board page on the Town website. Ms. Melofchik asked if the written report would be available to the public and asked if the Planning Department would edit the written report and Ms. Raitt said that the presentation from the students would be posted.

Mara Vatz asked if the students thought about communicating with Somerville to help solve the traffic issues at the Broadway intersection. Ms. Vatz stated that the intersection from Arlington is reduced to one lane in Somerville and that communication with Somerville would be required for making improvements to that intersection. Ms. Raitt said the Town Manager's office initiated a conversation with the Somerville Mayor's and Planning offices to discuss opportunities to work together along that corridor connecting both communities. Ms. Vatz then asked to consider how the Green Line extension would impact the Broadway corridor since it will change how a lot of residents will be getting to Kendall Square. Mr. Kantz said he spoke to the MBTA and was told that the MBTA is looking to review bus routes in the area after the Green Line extension is in place.

Barbara Thornton asked if there are Arlington Housing Authority units in the area. Menotomy Manor has 176 housing units.

Don Seltzer asked if the students had any data about who uses Lussiano Field and what the rationale is for opening the field up to Broadway. Mary Hannah Smith, a member of the student team, said they did not ask where people lived but did find that the field is one of the most beloved areas in the neighborhood. Ms. Hannah Smith said the students did not have a good sense if people were crossing Broadway to visit the field. Mr. Seltzer said that it may be an advantage to have the field buffered from the busy Broadway street. A member of the student team said that part of the student's recommendations is to make it easier and more inviting to cross Broadway. Ms. Hannah Smith reported that there are unsafe intersections on the streets connecting to Lussiano Field and there is work to make side street access safer also. Mara Vatz stated that the access to the sports fields is not ADA compliant. Student, Paulo Perez, said the idea of opening Lussiano Field to Broadway was to encourage more vibrancy to the corridor. Opening the park would be a catalyst for more people to gather in the area. Based on feedback from the residents, there is a lack of places to go or gather in the community for those residents that live within the

Broadway Corridor.

The Chair thanked the students for their presentation and that the Town will continue these conversations.

The Chair introduced the second Agenda Item, Housing Plan Implementation Committee (HPIC) update. Ms. Raitt introduced Karen Kelleher and Patricia Worden. Ms. Raitt gave an overview of the committee and their focus related to issues other than zoning. The proposed Municipal Affordable Housing Trust Fund (MAHTF), which would be under the purview of the Select Board, and a proposed real estate transfer fee are the two main things that the group has been discussing. Ms. Zwirko said that the real estate transfer fees would need to be deposited into a MAHTF. Ms. Zwirko said that they are waiting for more information from Town Counsel on ways to format the warrant articles that may go along with this proposal. Ms. Raitt would like to have the Board's support of the HPIC's efforts to advance these articles as they may be filed for spring Town Meeting.

Ms. Worden, a member of the HPIC, said she thinks the Trust Fund would be beneficial for the Town as many affordable contributions are required to be deposited to a MAHTF. The Chair asked if this is a missed opportunity and now it would be corrected. Ms. Raitt said that it was not necessarily a missed opportunity as much as it is a positive for future financial contributions toward affordable housing and the Town's ability to advance those opportunities. Ms. Worden said that the Town has already lost money by not having a MAHTF.

Mr. Lau expressed concern that this would place an additional burden on developers building in town. Mr. Lau asked if with the MAHTF the goal will be to build developments with only affordable housing units or to work with developers to create developments where affordable housing is integrated with market rate housing units. Ms. Raitt provided an overview of the types of funds and what the MAHTF might look like. She referenced the MAHTF Guide that was posted with this agenda item and explained that the Trust goals would be part of the charter. Mr. Benson said the guidebook was very helpful and the Trust Fund is a good idea to give the Town another tool and more flexibility to spend on affordable housing.

Mr. Benson said he is concerned if any decisions to access the funds would have to go back to Town Meeting for approval. Mr. Benson said he would not like it if the Town was to take funds from developers so they do not have to build affordable housing units. Mr. Benson said that it is a great opportunity for the Town.

Ms. Zsemberly asked what specifically are we trying to accomplish with the MAHTF and where will the funds be coming from. Ms. Raitt explained the goals to ultimately create and preserve affordable housing and the range of potential funding sources that might be part of the Trust Fund.

The Chair said he would like the Board to be able to review any Trust Fund plan and make recommendations to the Select Board if necessary. The Chair said the Trust Fund would provide another resource and may encourage affordable housing development in town.

The Chair introduced the third Agenda Item, Potential Zoning Bylaw amendments for 2020 Annual Town Meeting. Ms. Raitt said her memo is an overview of potential zoning amendments from the Town led initiatives from groups or committees that the DPCD is working with, which includes mostly administrative items that need updating. Ms. Raitt said that she included memos from Chris Loretto and Patricia Worden. Ms. Raitt said she also included an update of the work the DPCD is doing on the Town's Stormwater bylaw along with compliance with the MS4 General Permit. Ms. Raitt said items that the Board may need to address include amendments to the Environmental Design Review criteria and other items recommended by Horsley Witten Group.

Ms. Raitt said that a follow up is required to the conversation started at spring Town Meeting regarding housing

recommendations, the status of the recommendations, what will be happening next, and the status of the ARB meeting with the Select Board. Ms. Raitt said the joint ARB and SB meeting is intended to be an opportunity to discuss the plan for how to move forward regarding housing actions, some zoning and some non-zoning, in addition to the community participation plan.

Mr. Benson said the current EDR standards are broad enough where the Board may want to draft a guidance document once the Stormwater bylaw is completed. Mr. Benson said that there may not be a need for a change to the bylaw. Mr. Benson said that the Clean Energy Future Committee is doing a good job considering what we can do to improve existing infrastructure to increase energy efficiency and lower greenhouse gas emissions. Mr. Benson said that is he disappointed the Town is missing an opportunity to update the bylaw regarding new construction regulations to ensure new construction meets the standards the Town wants met. Mr. Benson said he feels that the Town should follow the Watertown ordinance requiring solar on larger buildings and no longer allow fossil fuel infrastructure for new development. Ms. Raitt said the Brookline bylaw is still pending the Attorney General's office approval.

Mr. Watson said he is cautious about recommending the articles to put on the warrant when we do not have a clear understanding of what the actual warrant article will be as that has complicated the Board's progress in the past. Mr. Watson said that he did wish the Board could have met with the Select Board earlier and moved forward with the public engagement around housing and zoning leading up to last year's Town Meeting. Mr. Watson said that he is worried that the Board is pushing the limits of the level of being able to do the type of engagement the Board envisioned before next fall.

Mr. Lau asked if the Board could set up an agenda item to discuss any potential warrant articles. The Chair asked to add time for the Board discuss potential warrant articles during the January, 6, 2020 ARB meeting. Mr. Lau said he would like to opportunity to discuss Planned Unit Developments or PUDs. Mr. Benson agreed, as the students' presentation showed that some of the area needs to be rezoned to allow for development. Ms. Raitt noted that the Select Board voted to change the order of the warrant articles for Town Meeting and that zoning will be last.

The Chair opened the floor to the public to comment regarding any zoning items. Ms. Worden asked that the Board include a request to clarify the definition of the term foundation in the Zoning Bylaw.

Chris Lorette wanted clarify the Board understood that the suggestions Mr. Lorette made for proposed zoning changes including changes to tables and suggested update to the definition of mixed-use.

The Chair introduced the Fourth Agenda Item, Meeting Minutes 11/4/19. Mr. Benson moved to accept the minutes for November, 4, 2019 with amendments suggested by the Board, Ms. Zsembry seconded, all voted in favor 5-0.

The Chair introduced the Fifth Agenda Item, Open Forum. The Chair opened the floor to the public for comments.

Mr. Benson moved to adjourn the meeting, Mr. Watson seconded, all voted in favor 5-0.

Meeting adjourned.





## Town of Arlington, Massachusetts

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### Correspondence received:

#### Summary:

Correspondence received from:  
Don Seltzer (1/24/20)  
Barbara Thornton (1/27/20)  
John Worden (1/15/20)  
Christopher Loreti (1/27/20)

#### ATTACHMENTS:

Type	File Name	Description
Reference Material	Correspondence_from_B._Thornton_re__Warrant_Article_updates_with_attachments_received_1-27-20.pdf	Correspondence from B. Thornton re Warrant Article updates with attachments received 1-27-20
Reference Material	Correspondence_from_B._Thornton_Attachment_1_received_1-27-20.pdf	Correspondence from B. Thornton Attachment 1 received 1-27-20
Reference Material	Correspondence_from_B._Thornton_Attachment_2_received_1-27-20.pdf	Correspondence from B. Thornton Attachment 2 received 1-27-20
Reference Material	Correspondence_from_B._Thornton_Attachment_3_received_1-27-20.pdf	Correspondence from B. Thornton Attachment 3 received 1-27-20
Reference Material	Correspondence_from_C._Loreti_re_Special_Permit_Docket_3602_received_1-27-20.pdf	Correspondence from C. Loreti re Special Permit Docket #3602 received 1-27-20
Reference Material	Correspondence_from_D._Seltzer_012420.pdf	Correspondence from D. Seltzer received 1-24-20
Reference Material	Correspondence_from_J._Worden_re_Warrant_Articles_with_attachments_received_1-15-20.pdf	Correspondence from J. Worden re Warrant Articles with attachments received 1-15-20
Reference Material	Correspondence from J. Worden Attachment 1 received 1-15-20.pdf	Correspondence from J. Worden Attachment 1

- Material Correspondence\_from\_J.\_Worden\_Attachment\_2\_received\_1-15-20.pdf

received 1-15-20  
Correspondence  
from J. Worden  
Attachment 2  
received 1-15-20

▢ Reference  
Material Correspondence\_from\_J.\_Worden\_Attachment\_2\_received\_1-15-20.pdf

From: Andrew Bunnell <abunnell@gmail.com>  
To: Jenny Raitt <JRaitt@town.arlington.ma.us>, Erin Zwirko  
<EZwirko@town.arlington.ma.us>  
Date: Mon, 27 Jan 2020 08:48:03 -0500  
Subject: Fwd: Monday night ARB meeting

Could you provide the attachments to the rest of the board ahead  
of  
tonight's meeting?

----- Forwarded message -----

From: Barbara Thornton <[bthornton@assetstewardship.com](mailto:bthornton@assetstewardship.com)>  
Date: Sat, Jan 25, 2020 at 9:13 PM  
Subject: Monday night ARB meeting  
To: Andrew Bunnell <[ABunnell@town.arlington.ma.us](mailto:ABunnell@town.arlington.ma.us)>, Andrew  
Bunnell  
<[abunnell@gmail.com](mailto:abunnell@gmail.com)>

Andrew-

This is a brief update on my progress on the three warrant  
articles I  
will be bringing in for the ARB's questions on Monday night.

ADU's

I have met with Doug Heim, Michael Byrne, Bldg Inspector and  
Kevin  
Kelley, Fire Chief, over an hour with each of them. We've had  
some  
good conversations. I've rewritten the Article with Heim's  
advice and  
have shared both the older and the updated article with Byrne  
and  
Kelley. I have asked all of them to give me edits on the "final  
draft". I've pressed Heim to get back to me by tomorrow.

Kelley is fine with it and will not oppose, he tells me. Byrne  
is ok  
with it but that he will be inspecting these units as if they  
were two  
family dwellings. I've pressed hard on clarification about what  
this  
means and the short answer is: 1) every property is going to be  
different and 2) it behooves the home owner to get expert  
(architect)  
advice to understand the potential costs of code compliance



before  
going too far down the road.

I have also been in touch with people at the state level and the  
local  
building inspector level to understand why the perspectives on  
bldg  
code enforcement seem to vary from place to place on this. But  
I'm  
fine with Byrne's more cautious interpretation. This will be  
worked  
out over time.

Broadway Corridor Design Project  
I'm waiting for any final comments from Doug Heim. He's had a  
big  
project, unexpected, that has been taking a lot of his time. I  
did  
make changes in the article after my first meeting with Heim.

Non-Conforming Parcels/ Permanently Affordable Housing  
I reviewed this with Doug Heim Jan. 14. I then reached out to  
Don  
Selzer to enlist him in a cooperative effort to bring this  
forward.  
We worked on the language to get it to where I'm presenting it  
Monday.  
He had separate conversations with Heim. Unfortunately a few  
days ago  
Don concluded that he didn't have the time to do the depth of  
research  
he'd wanted to do on this so he backed out, politely, as a co-  
sponsor  
but assures me that he will support it.

I look forward to answering questions from the Board on Monday.  
Although I haven't received a final response from Heim on these  
(expected by Monday afternoon), I am attaching the semi-final  
versions, with notes, that I believe represent Heim's major  
concerns.

Best regards,  
Barbara Thornton

\*\*\*\*\*

BARBARA THORNTON| Founder/ CEO

617.699.2213

AssetStewardship.com

@assetstewards    LinkedIn

Offering best practices to enhance our public assets through  
stewardship

of our urban future using government, technology & private  
sector collaboration.

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Andrew Bunnell

## Warrant Article Submission Form

### PETITION OF TEN REGISTERED VOTERS FOR INSERTION OF ARTICLE INTO THE WARRANT FOR THE ANNUAL (SPECIAL) TOWN MEETING.

**File Completed Form with the Board of Selectmen's Office no later than Friday, January 31, 2020, 12:00/Noon.**

We, the undersigned registered voters (10 for Annual, 100 for Special) of the Town of Arlington, hereby petition the Board of Selectmen pursuant to MGL c.. 39, § 10 to insert the following article(s) into the warrant for the Annual (Special) Town Meeting.

#### ARTICLE

Proposed Title:

**Creation of a Process Allowing Permanently Affordable Housing to be Built on Privately Owned Parcels of "Non-Conforming" Size**

Subject Matter:

**Affordable Housing on Non-Conforming Parcels**

The purpose of this article is to see if the town will vote to allow the development of new sources of permanently affordable housing (affordable in perpetuity and affordability as defined in Arlington Zoning By-Laws) by modifying the requirements for constructing housing units to enable construction on smaller lots as long as those units are permanently committed to be available for rental or ownership according to official regional guidelines (see (Zoning Bylaw Section 2, Basic Provisions, Definitions Associated with Affordable Housing) of affordability.

Such construction would be permissible in all zoning districts allowing residential use, providing the tracts were laid out prior to July 1, 2019, and receive a special permit from the ZBA.

Ownership, sale, repurchase and rentals of each property would be overseen by the Arlington Housing Trust Fund or a comparable entity that would have the authority to enforce the affordability guidelines in perpetuity.

*Requested by:*

*Barbara Thornton*

*Precinct 16 TMM*

*223 Park Ave.*

*[bthornton@assetstewardship.com](mailto:bthornton@assetstewardship.com)*

*617-699-2213*

*1/5/20*



**For Office Use Only – Do Not Write In This Area**

Proposed Name/Subject Matter: \_\_\_\_\_

The primary motion for this article will come from:

Board of Selectmen

Redevelopment Board

Finance Committee

The following groups will comment on this article:

Board of Selectmen

Redevelopment Board

Finance Committee

Proposed Title/Subject Matter: **Affordable Housing on Non-Conforming Parcels**

**Signatures**

**Printed Name & Address**

1.	_____	_____
2.	_____	_____
3.	_____	_____
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9.	_____	_____
10.	_____	_____
11.	_____	_____
12.	_____	_____

(please use additional sheets of paper for signatures if needed)

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### ARTICLE

Proposed Title:

**Article to Propose the Adoption of Accessory Dwelling Units (ADUs)**

Subject Matter: **Accessory Dwelling Units**

The purpose of this article is to see if the town will vote to allow Accessory Dwelling Units (ADUs) in single and two-family dwellings in residential districts, or take any action related thereto by amending

**Section 1.2 Purposes** to add “to encourage housing for persons at all income levels *and stages of life*”.

and

**Section 2 5 Definitions Associated with Dwelling:** to add *An ACCESSORY DWELLING UNIT, OR ACCESSORY APARTMENT, “Accessory dwelling unit”, four or more rooms constituting a self-contained accessory housing unit, inclusive of sleeping, cooking and sanitary facilities on the same premises as the principal dwelling, subject to otherwise applicable dimensional and parking requirements, that: (i) maintains a separate entrance, either directly from the outside or through an entry hall or corridor shared with the principal dwelling sufficient to meet the requirements of the state building code and state fire safety code; (ii) is not larger in floor area than 1/2 the floor area of the principal dwelling.*

and

**Section 3.3.3 Special Permits** to add “*in the case of requests for special permits for Accessory Dwelling Units, the use will add to the need for a range of affordable housing opportunities for the Town.*”

and

**Section 5.2.3 Districts and Uses:** to add “to allow for the creation of accessory dwelling units in all zoning districts which allow residential use. (Include in Section 5.4 Residential Districts)

### NOTE:

This warrant article is intended to recognize and build on the majority support for ADU's from both the 2019 town meeting and from town residents who recognize a clear need to:

1. make available more reasonably priced housing opportunities for people need housing of a type or for a price not currently available to them and



2. offer homeowners with larger homes and available space (“overhoused”), but who have limited incomes, an opportunity to monetize that space in order to continue to live in their homes and be able to pay their rising Arlington property taxes
3. create a space in their home to care for elderly or disabled family members or to be cared for themselves as they age

The attached draft language draws on ADU legislation from several municipalities near and comparable to Arlington. It is intended to emphasize the purpose of ADUs in helping to resolve the above problems currently facing town citizens.

Accessory Dwelling Units. The purpose of this is to allow for the creation of accessory apartments in single and two family residential districts. These districts contain a number of large single family homes that are underutilized, often occupied by one or two people who are “overhoused” in homes with three or more bedrooms. Under current law, alteration of these homes to provide additional dwelling units would be prohibited in most cases due to the existing zoning standards.

The provision of accessory dwelling units in owner occupied one and two family dwellings is intended to:

1. Increase the number of small dwelling units available for rent in the town,
2. Encourage the availability of lower cost housing opportunities
3. Increase, the diversity of housing options for town residents, in response to demographic changes such as smaller households, older households, an increase in single parent and single person households;
4. Encourage better utilization of existing housing stock, particularly for older owners of larger homes;
5. Bring existing Accessory Apartments up to code for health and safety, and eliminate apartments that cannot be made safe, or do not comply with the provisions of this Zoning By-Law, and
6. Eliminate the continued construction of illegal unregulated apartment units.
7. Take advantage of the “overhousing” that exists as a result of “empty nesters” not moving out of their larger, single family, multi bedroom, long time homes.
8. Encourage greater diversity of population with particular attention to young adults and senior citizens, and
9. Encourage a more economic and energy-efficient use of the town's housing supply while maintaining the appearance and character of the town's single family neighborhoods.

Given contemporary lifestyles, housing needs and energy and maintenance costs, it is beneficial to the Town to allow greater flexibility in the use of such dwellings without substantially altering the environmental quality of such residential districts. This Subsection gives the Board of Zoning appeal authority to relax such zoning requirements in certain instances consistent with the above objectives and as enumerated below.

Subsection 2: In a single or two family residential district the Board of Zoning Appeals may grant a special permit for alteration of a single family, detached dwelling legally in existence as of the effective date of this Subsection to provide one accessory apartment if the following conditions are met:

1. The dwelling was constructed prior to **Jan. 1, 2020**,
2. Prior to alteration the dwelling contains at least fifteen hundred (1,500) square feet of gross floor area.
3. Such accessory apartment shall not occupy more than fifty (50) percent of the gross floor area of the principal dwelling in existence prior to the effective date of this Subsection.

In granting a special permit the Board may impose such conditions, including requirements

for off street parking and limitations on other accessory uses of the premises, as it may deem appropriate to avoid detriment to the neighborhood or to nearby persons or property. The Board of Zoning Appeals shall evaluate each special permit application which involves exterior changes with the appearance of and character of the neighborhood and may require that there be no change or minimal change to any face of a building oriented toward a public way or visible from a public way.

#### SPECIAL PERMIT GRANTING PROCEDURES:

1. The proposed Accessory Apartment is in harmony with and will promote the purposes of the Zoning By-Law;
2. The specific site is an appropriate location for such a use, structure or condition;
3. The use as developed will not adversely affect the neighborhood;
4. There will be no nuisance or serious hazard to vehicles or pedestrians;
5. Adequate and appropriate facilities will be provided for the proper operation of the proposed use.
6. In granting a special permit, the TOWN may impose such additional conditions as it may deem necessary to protect the single or **two** family appearance of the dwelling, and to bring the dwelling as close to conformity with the conditions and requirements for new accessory apartments, as is feasible.
7. A special permit granted by the TOWN shall include a condition that a certificate of occupancy shall be obtained for periods not to exceed three years. No subsequent certificate of occupancy shall be issued unless there is compliance with the plans and conditions approved by the TOWN.
8. If a special permit is granted and corrective changes are required, they must be completed within 90 days of the date of granting the permit. When required changes are completed, the building commissioner will issue a certificate of occupancy.
9. If a special permit is denied, the second dwelling unit shall be terminated within one year of the date of the denial.
10. Upon a conveyance of the property, the subsequent owner shall submit to the Zoning Enforcement Officer a certificate of compliance with prior conditions.
11. A Special Permit-Accessory Apartment shall be required for all units meeting the definition of an Accessory Dwelling Unit and designated in this Zoning By-Law as requiring a Special Permit-Accessory Apartment before the Inspector of Buildings may issue a building permit or an occupancy permit. The Board of Appeals shall not approve any such application unless it finds that in its judgement all of the criteria for a Special Permit set forth in above Sections of the By-Law have been satisfied and, in addition, that the following conditions are met:
  - a) The proposed Accessory Apartment is in harmony with and will promote the purposes of the Zoning By-Law (PURPOSE);
  - b) The specific site is an appropriate location for such a use, structure or condition;
  - c) The use as developed will not adversely affect the neighborhood;
  - d) There will be no nuisance or serious hazard to vehicles or pedestrians;
  - e) Adequate and appropriate facilities will be provided for the proper operation of the proposed use.

Requested by:  
Barbara Thornton  
Precinct 16,  
223 Park Ave, Arlington  
617-699-2213, barbarathornton1@gmail.com

**For Office Use Only – Do Not Write In This Area**

Proposed Name/Subject Matter: \_\_\_\_\_

The primary motion for this article will come from:

Board of Selectmen

Redevelopment Board

Finance Committee

The following groups will comment on this article:

Board of Selectmen

Redevelopment Board

Finance Committee



Proposed Title/Subject Matter: **Accessory Dwelling Units**

**Signatures**

**Printed Name & Address**

1.	_____	_____
2.	_____	_____
3.	_____	_____
4.	_____	_____
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6.	_____	_____
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9.	_____	_____
10.	_____	_____
11.	_____	_____
12.	_____	_____

(please use additional sheets of paper for signatures if needed)

## Warrant Article Submission Form

### PETITION OF TEN REGISTERED VOTERS FOR INSERTION OF ARTICLE INTO THE WARRANT FOR THE ANNUAL (SPECIAL) TOWN MEETING.

**File Completed Form with the Board of Selectmen's Office no later than Friday, January 31, 2020, 12:00/Noon.**

We, the undersigned registered voters (10 for Annual, 100 for Special) of the Town of Arlington, hereby petition the Board of Selectmen pursuant to MGL c.. 39, § 10 to insert the following article(s) into the warrant for the Annual (Special) Town Meeting.

#### ARTICLE

##### **Proposed Title:**

**Resolution to Create a Design Competition**

**Subject Matter: Resolution to Create a Broadway Corridor Design Competition to Encourage the Development of New Housing Styles Appropriate for Arlington**

### **Article to Propose a Broadway Corridor Design Competition to Encourage the Development of New Housing Styles Appropriate for Arlington**

The purpose of this article is to see if the town will vote to allow the Town to sponsor a design competition to encourage new housing and mixed use construction in the Broadway Corridor area of Arlington. This warrant article builds on findings from the Fall 2019 [Broadway Corridor Study](#), and creates a "demonstration area project".

#### NOTES:

Purpose: Build on the need to provide a broader range of mid-priced housing types for single and two person households, and to maximize the transit corridor benefits provided by the location near major bus routes and the subway at Alewife and the planned green line subway stop.

Situation: 71% of the current residential units in the Broadway Corridor study area are inhabited by only one or two people. That housing, mostly over 60 years old, was designed to house more people in larger families. We need to build new units appropriate for the peoples' needs who are now using those family size units. There is clearly an unmet market for housing for individuals and couples in that area. There is also a market in Arlington for older residents who want to stay in town but want a smaller unit that may be handicapped accessible, near public transit and near shopping and cafes.

Design Competition Standards:

- 1) Identify site(s) in the Broadway Corridor area where the Town can encourage the following use characteristics:
  - a) 50 to 200 residential units per project
  - b) 25% of units affordable according to regional standards
  - c) Building to LEED or Net Zero requirements
  - d) 75%-100% one bedroom units
  - e) mixed use space including cafe, etc.
  - f) Average FAR 3.2
  - g) Access to daylight for buildings (see: <https://youtu.be/YAeCvUZmUrl>)
  - h) No height restrictions, waive other residential zoning restrictions
  - i) Inclusion of microgrid (see: <http://integratedgrid.com/wp-content/uploads/2017/01/8a-Maitra-Microgrid-Design-Consideration.pdf>) for power
  - j) Public space with permeable materials to facilitate storm water retention

The winning project could build with a long term loan from a local bank and financing assistance from the Town. Additional points would be given for more middle income and certified affordable units. Town would waive height, density and set back requirements and offer a speedy approvals process to attract owner/developer teams who will suggest creative new approaches to housing in Arlington.

Requested by:  
Barbara Thornton  
Precinct 16,  
223 Park Ave, Arlington  
617-699-2213, barbarathornton1@gmail.com

**For Office Use Only – Do Not Write In This Area**

Proposed Name/Subject Matter: \_\_\_\_\_

The primary motion for this article will come from:

Board of Selectmen

Redevelopment Board

Finance Committee

The following groups will comment on this article:

Board of Selectmen

Redevelopment Board

Finance Committee



Proposed Title/Subject Matter: **Create a Broadway Corridor Design Competition to Encourage the Development of New Housing Styles Appropriate for Arlington**

**Signatures**

**Printed Name & Address**

1. \_\_\_\_\_

\_\_\_\_\_

2. \_\_\_\_\_

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12. \_\_\_\_\_


\_\_\_\_\_

Documentation in Support of the Testimony of Christopher  
Loreti for the Arlington Redevelopment Board Hearing on  
Special Permit Docket #3602  
(1207-1211 Massachusetts Avenue)

January 27, 2020

The attached pages are excerpts from the certified transcript of Article 6 of the April 25, 2016 Annual Town Meeting, which amended Arlington's Zoning Bylaw to allow mixed-use developments. These excerpts demonstrate that at least three times members of the Arlington Redevelopment Board (Chair Andrew Bunnell and then member Michael Cayer) testified that only uses individually allowed in a zoning district could be permitted as part of a mixed-use development in the same zoning district. Thus a hotel use, which is not allowed in the B2 zoning district, cannot be permitted as part of a mixed-use development in the B2 zoning district as proposed in Docket 3602. See statements in brackets followed by an asterisk on pages 48, 50, and 67.

I respectfully request that this documentation be entered into the public record for this docket as part of this public hearing.

A handwritten signature in cursive script, reading "Christopher Loreti". The signature is written in dark ink on a white background.

**TOWN OF ARLINGTON  
ANNUAL TOWN MEETING**

**MONDAY, APRIL 25, 2016**

**Session 1**

Robbins Memorial Town Hall Auditorium

730 Massachusetts Avenue

Arlington, Massachusetts 02476



**CAMBRIDGE TRANSCRIPTIONS**

675 Massachusetts Avenue

Cambridge, MA 02139

(617) 547 -- 5690

[www.ctran.com](http://www.ctran.com)



1 I live on Lombard Terrace, close to three blocks, two long  
2 blocks from Mass. Ave. I'll be voting against this, I  
3 believe. But I'd like to say a few things. I think it's  
4 dreadful that we're presented with all these changes as one  
5 article. Some I would vote for, some I would vote against.  
6 I attended at least one of the meetings about this,  
7 approximately a week and a half or two weeks ago. I find  
8 all this difficult to absorb, and it's too multifaceted for  
9 me to swallow one vote. And that's part of the reason why  
10 I would vote no. I would recommend that ARB postpone the  
11 vote to give people another vote, at least to give us time  
12 to want to vote yes. But as it is, tonight I would vote  
13 no.

14 What is the neighborhood business district?  
15 There's a paragraph in this thing about a neighborhood  
16 business district, and I'm wondering -- I read it but --

17 MR. JOHN LEONE: Ms. Weiner? Or Mr. Bunnell  
18 (Indiscernible)

19 MR. ANDREW BUNNELL: The feature of the  
20 neighborhood district, business district --

21 MR. JOHN LEONE: Introduce yourself. -

22 MR. ANDREW BUNNELL: Andrew Bunnell, Chair of the  
23 Redevelopment Board. If you could bring out my slides  
24 again, I could point out where that is on the map.

25 (Indiscernible). It's a little unclear on the map, but the

1 second line { on our key here is B2, neighborhood business  
2 district. And these are interspersed throughout town.  
3 They are traditionally small businesses, districts with  
4 smaller businesses.

5 You won't see major developments going in in this  
6 kind of a district. It usually comes into a neighborhood -  
7 - it has to comply with what's already permitted in that  
8 district. ] \* And it also has to be within the character of  
9 the neighborhood. And part of the reason that the ARB has  
10 decided to keep special permit review over this is so that  
11 we can be assured that we're protecting neighborhoods from  
12 being overrun and seeing that "Palo Alto effect" that the  
13 other speaker talked about. It is important to us that  
14 there is some review over these projects from the  
15 beginning, so that we're not seeing monstrosities coming to  
16 town, and seeing the kinds of things that people don't  
17 want.

18 It is an open process, the special permit is a  
19 collaborative, open process where people do have the  
20 opportunity to come in and speak their case, and advise the  
21 ARB on how we should be voting and what projects we should  
22 be looking at, what projects we should say, maybe time to  
23 go back to the drawing board and come back with something a  
24 little more appropriate for the neighborhood and for the  
25 use that you're requesting.

1 MR. ANDREW FISCHER: -- and the answer was yes,  
2 so I def --

3 MR. ANDREW BUNNELL: Well, that's actually not  
4 true. [Mixed use is any use that would be more than one  
5 use. It can't be sold as residential. Again, it has to  
6 fit with a permitted use; a parking garage won't be  
7 permitted in there, because a parking garage isn't  
8 permitted. A residential on top of a gas station won't be  
9 permitted if that use is not already permitted. It has to  
10 fit what's already allowed under zoning,\*] and it has to fit  
11 within the character of the neighborhood being considered.

12 MR. JOHN LEONARD: At any rate, I would support  
13 Mr. Loreti's amendment, for the reason I just said. And  
14 the other reason I'm going to vote no is that I can't find  
15 anybody that wants higher density in the town, not in my  
16 precinct, anyway, when I talk with people. And the theory  
17 that we're obligated to go higher and higher density  
18 because of the world and greenness, I don't buy it. I  
19 happen to think we're at optimal density right now. I  
20 think we've already done more than our job. There are  
21 equally valid reasons to say high density is not healthy.  
22 So, that's my feeling then. I would repeat everything that  
23 the previous speaker also said. Thank you.

24 MR. JOHN LEONE: Thank you very much. Mr.  
25 Worden.

1 it that said "5,000." There wasn't any intent to change  
2 that. So, instead of the dash, the scrivener's error that  
3 we've corrected now with the Town Clerk and provided to the  
4 Clerk and the Moderator, is to change that dash to a  
5 "5,000." So, hopefully, that's clear.

6 MR. JOHN LEONE: If you'll all make that change  
7 administratively to your report, we'll just go with it as  
8 we proceed. Go ahead, Mr. Cayer.

9 MR. MIKE CAYER: Thank you. So, I want to start  
10 by saying, zoning is hard. It's hard and we do it first,  
11 which, frankly, I think is a disservice to both zoning and  
12 for helping the town move some of these things forward.

13 But, be that as it may, that's what we're doing.  
14 We're here tonight to talk about Articles 6 and 7,  
15 hopefully, eventually.

16 [ So, the first thing I want to talk about is  
17 correct a couple of things that were talked about earlier.  
18 There was a statement made that said that any commercial  
19 use can be snuck in to the mix -- the definition that's  
20 been put forth before you, in a mixed use development. So,  
21 you know, you can put a meat-processing plant on the first  
22 floor if you so choose, and if those rascals on the  
23 Redevelopment Board approve it, then you're going to have a  
24 meat-packing plant on the first floor.

25 That's not correct. We've worked with both the



1 Inspectional Services, the head of Inspectional Services,  
2 as well as Town Counsel on the wording that's before you.  
3 And only the uses that are permitted in a particular  
4 district are the ones that can happen in a mixed use in  
5 that district. So, just to clarify on that point. ] \*

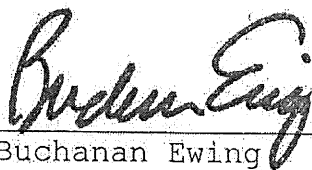
6 The second point I want to bring up is, with  
7 respect to height, I think we've clarified a few things  
8 with respect to height. But I want to clarify two others.

9 Number one is, is, you've heard some people talk  
10 about a four-story buffer, okay? What that is, is what  
11 we're really talking about there is if a proposed mixed use  
12 is next to resident, then, instead of being five stories,  
13 you can only build four. That's a buffer zone, okay? You  
14 cannot go all the way up, and what's already in there stays  
15 in there, okay? It's only in the more commercial spine,  
16 where you've got other big buildings around you, that  
17 you'll be able to go to the maximum height.

18 Now, the important thing on this, though, is that  
19 what this does is it actually, from the streetscape, limits  
20 the height of the buildings even further down, because what  
21 you've also heard is about setbacks. And a setback means  
22 that as you go up to that fifth floor, or as you go above  
23 three, you have to move those next floors back seven and a  
24 half feet. So that from the streetscape now, you're only  
25 going to see three stories.

C E R T I F I C A T E

I, Buchanan Ewing, do hereby certify that the foregoing transcript is a true and accurate record of the aforementioned matter prepared to the best of our knowledge, skill, and ability.



Buchanan Ewing

6/2/16

Date

Notary Public No. 17610 DNP

My commission expires June 15, 2018

CAMBRIDGE TRANSCRIPTIONS

Approved Court Transcriber

**From:** Don Seltzer <timoneer@gmail.com>  
 Jenny Raitt <jraitt@town.arlington.ma.us>, Erin Zwirko <EZwirko@town.arlington.ma.us>,  
**To:** DWatson@town.arlington.ma.us, KLau@town.arlington.ma.us, rzsembery@town.arlington.ma.us,  
 Andrew Bunnell <ABunnell@town.arlington.ma.us>, EBenson@town.arlington.ma.us  
**Date:** 01/24/2020 11:03 AM  
**Subject:** Docket #3602 - Omissions in submitted application

---

Please include the following correspondence in the docket for Monday's hearing.

To: Arlington Redevelopment Board  
 24 Jan 2020

Having reviewed the updated application materials that were posted yesterday evening, I have noticed a number of key omissions which I wish to call to your attention in advance of Monday's hearing.

### **Gross Floor Area**

I could not find in the posted materials any number given for the Gross Floor Area. This is a key factor in determining compliance with several zoning requirements and is fundamental to the applicant's request for relief from the Floor Area Ratio requirements. It had been misstated in the original application last July.

Using the submitted digital drawings, my accounting of Gross Floor Area is 26,021 sf. Broken down, it is:

Basement (stairwells, elevator, bike room) 1,238 sf

First Floor 5,543 sf

Second Floor 7,270 sf

Third Floor 7,270 sf

Fourth Floor (excluding roof decks) 4,700 sf

The applicant is asking for relief from the Floor Area Ratio zoning requirement of 1.5 FAR, which comes to 21,045 sf for this property. The applicant's letter asks for a 'modest' increase, but in actuality it is nearly 5,000 sf over the limit.

The applicant is also incorrect in asking for relief under the 'Bonus Provisions' exceptions:

#### *5.3.6. Exceptions to Maximum Floor Area Ratio Regulations (Bonus Provisions)*

*The Board of Appeals or the Arlington Redevelopment Board, as applicable, may grant a special permit subject to the standards in Section 3.3 or 3.4, as appropriate, to allow a maximum gross floor area higher than is permitted in the district, subject to the procedures, limitations, and conditions specified below, for a lot (or part of a lot) which meets the following basic requirements:*

*(1) The lot (or part of a lot) is in a district with a floor area ratio of 1.2 or greater.*

*(2) The lot (or part of a lot) is not less than 20,000 square feet when the principal use is*

## *residential.*

Such exceptions are limited to properties of 20,000 sf or more, not the 14,000 sf property under review.

## **Elevations**

The submitted Elevation views are lacking in key details. In particular, the only Rear Elevation drawing includes the privacy barrier which clearly does its job because it hides all details about the lower parking level. There are difficult topographic issues regarding the drop off from Clark St that are concealed. The ramp from Clark to the parking area appears dangerously steep, perhaps as much as a 20% to 25% grade. It simply cannot be determined from the inadequate information provided. Nor is it shown what the garage entrance looks like. The limited dimensions provided suggest that the overhead height is very low, perhaps less than eight feet.

The front elevation drawing is odd in that it shows the hotel as being on a level lot. In fact, Mass Ave and the sidewalk in front fall off by four feet from left to right along the frontage of the property.

The applicant appears to be measuring building height only at the front left corner. This is highest point on the property, and a proper determination of building height should be looking at the average finished grade on all sides. This corrected measurement may lead to the uncomfortable conclusion that the proposed building exceeds the allowable zoning requirements.

There are other serious zoning issues that I conveyed to the Board at the July 22 hearing. They remain unaddressed.

**Clark St setback** - 5.3.8 requires a 20 ft setback. *"A corner lot shall have minimum street yards with depths which shall be the same as the required front yard depths for the adjoining lots."*

**Upper Story Step Backs** - There are multiple problems here. 5.3.17 is clear that the upper story step backs are required on all sides with street frontage. This includes the Clark St side. Furthermore, the step back must begin at the third floor, not the fourth as proposed.

5.3.17

*...beginning at the third story level or 30 feet above grade, whichever is less. The upper story step-back shall be provided along all building elevations with street frontage, excluding alleys.*

**Usable Open Space** - There simply isn't any. 20% of gross floor area is required.

**B2 district** - One third of the proposed project is within a B2 Neighborhood Business district. Height limitations are 40 feet, 3 story for this portion of the property. But more fundamental is that a hotel is a prohibited use for a B2 district. The applicant may situate



his restaurant on this section of the property, but a hotel can only be built on the B4 section.

I will be glad to discuss these zoning issues in more depth at Monday's hearing. Because of their detailed and technical nature I wanted to give you a preview to study this weekend prior to the hearing.

Respectfully,

Don Seltzer

PLANNING & COMMUNITY  
DEVELOPMENT

2020 JAN 15 P 12:54

27 JASON STREET  
ARLINGTON, MASSACHUSETTS 02476

TELEPHONE (781) 646-8303

[JWORDEN@SWWALAW.COM](mailto:JWORDEN@SWWALAW.COM)

January 15, 2020

Select Board  
Town Hall  
Arlington, Massachusetts

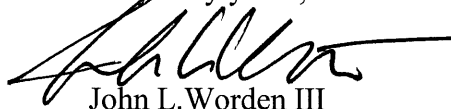
Mrs. Mahon and Gentlemen:

I hand you herewith two articles for insertion in the Warrant for the 2020 Annual Town Meeting:

1. Amend Zoning Bylaw to add a defiant ion of foundation
2. Amend Zoning Bylaw to regulate conversions from commercial to residential

Kindly acknowledge receipt of these articles by date stamping and returning to the bearer the copy of this letter.

Very truly yours,



John L. Worden III

✓ cc: Arlington Redevelopment Board

## Warrant Article Submission Form

### PETITION OF TEN REGISTERED VOTERS FOR INSERTION OF ARTICLE INTO THE WARRANT FOR THE 2020 ANNUAL TOWN MEETING.

**File Completed Form with the Board of Selectmen's Office no later than Friday, January 31, 2020, 12:00 Noon.**

*We, the undersigned registered voters of the Town of Arlington, hereby petition the Board of Selectmen pursuant to MGL c.. 39, § 10 to insert the following article into the warrant for the Annual Town Meeting:*

### Conversion of Commercial to Residential

To see if the Town will vote to amend the Zoning Bylaw in Section 5.2.4, by inserting in the last sentence of said section, after the word *footprint*, the words "if allowed by special permit" and by inserting, after the words *residential use*, the words "provided that the addition or expansion is for affordable housing" so that said sentence will read as follows:

In the case of an existing commercial use, the addition or expansion of residential use within the building footprint if allowed by special permit shall not require adherence to setback regulations for residential uses, provided that the addition or expansion is for affordable housing, even if the residential use becomes the principal use of the building.

or take any action relating thereto.

### Comment

Under the law as it presently stands, a mixed use building, with its minimal setbacks could be converted entirely into residential uses, by right. Since the only kind of additional housing that Arlington needs is affordable housing, the ability to do that would be limited under this amendment, and subject to public review.

*Requested by John L. Worden III*

*(Address: 27 Jason Street Arlington, Massachusetts 02476*

*Telephone: 781-646-8303*

*Email: jworden@swwalaw.com*

### For Office Use Only – Do Not Write In This Area

*Proposed Name/Subject Matter:* \_\_\_\_\_

*The primary motion for this article will come from:*

*Board of Selectmen*

*Redevelopment Board*

*Finance Committee*

*The following groups will comment on this article:*

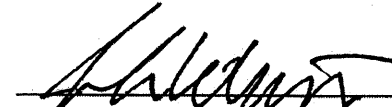

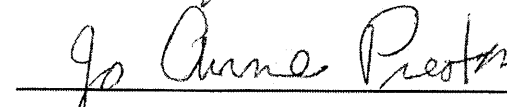

*Board of Selectmen*

*Redevelopment Board*

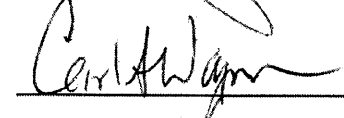
*Finance Committee*

## Signatures


## Printed Name &amp; Address

1. JOHN L. WORDEN 111  
27 Jason St Rt 82. ~~Don~~ DONALD SELTZER 104 IRVING ST.3. Jo Anne Preston 42 Mystic  
Lake Drive4. ARAM HOLLMAN  
12 WHITTEMORE ST.5. 



DON GERSH 24 KIPLING RD

6. 

Carl Wagner 30 Edgehill Road

7. 

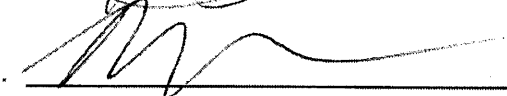
Beth Melchill 20 Russell Street

8. Christopher Loret  
56 Adams St. Arlington  
WYNELIE EVANS9. 

20 ORCHARD PLACE

10. 

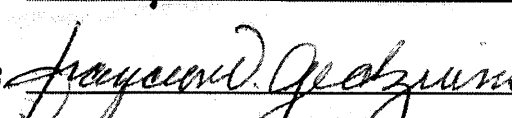
Monica Worden 26 Lockeland Ave

11. 

peter Wolz 26 Lockeland Ave

12. 

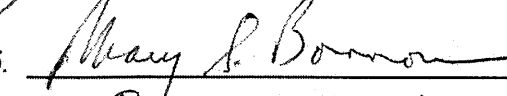
SIDNEY A. SLOBODKIN 1 WATERMILL PLACE

13. 

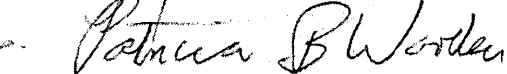
Frances V. Gedzum, Menotomy Rd.

14. 

LAWRENCE W. BANNON

15. 

Mary Bannon

14 Hillcrest St  
Arlington.16. 

PATRICIA B. WORDEN 27 Jason St

(please use additional sheets of paper for signatures if needed)



## Warrant Article Submission Form

### PETITION OF TEN REGISTERED VOTERS FOR INSERTION OF ARTICLE INTO THE WARRANT FOR THE 2020 ANNUAL TOWN MEETING.

**File Completed Form with the Board of Selectmen's Office no later than Friday, January 31, 2020, 12:00 Noon.**

*We, the undersigned registered voters of the Town of Arlington, hereby petition the Board of Selectmen pursuant to MGL c.. 39, § 10 to insert the following article into the warrant for the Annual Town Meeting:*

### Definition of Foundation

To see if the Town will vote to amend the Zoning Bylaw in Section 2, by adding a new definition as follows:

Building Foundation: The masonry or concrete structure in the ground which supports the building. It does not include porches, decks, sheds, patios, one story attached garages, carports, or the like

or take any action related thereto.

*Requested by Patricia B. Worden*

*(Name one person who will be the contact individual for this article and will serve as the person for upcoming hearings regarding this article.).*

*Address: 27 Jason Street Arlington, Massachusetts 02476*

*Telephone: 781-646-8303*

*Email: pbwordeen@gmail.com*

### For Office Use Only – Do Not Write In This Area

*Proposed Name/Subject Matter:* \_\_\_\_\_

*The primary motion for this article will come from:*

*Board of Selectmen*

*Redevelopment Board*

*Finance Committee*

*The following groups will comment on this article:*

*Board of Selectmen*

*Redevelopment Board*

*Finance Committee*

Signatures

Printed Name & Address

1. Patricia B Worden

Patricia B Worden  
27 Jason St

2. Wynelle Evans

WYNELLE EVANS  
20 ORCHARD PLACE

3. Christopher Lorette

Christopher Lorette  
56 Adams St. Arlington

4. Beth Melofchik

Beth Melofchik  
20 Russell Street

5. Carl Wagner

30 Edgill Road  
Carl Wagner

6. [Signature]

JONGERSH 24 KIPLING RD

7. Aram Hollman

ARAM 12 WHITTEMORE ST.  
HOLLMAN

8. Jo Anne Preston

Jo Anne Preston 42 Mystic Lake  
DRIVE

9. [Signature]

DONALD SELTZER 104 IRVING ST

10. [Signature]

Peter Worden 26 Lockeland Ave

11. [Signature]

Monica Paz Worden 26 Lockeland, MA

12. [Signature]

John L Worden 27 Jason St

13. Sidney A. Slobodkin

SIDNEY A. SLOBODKIN / WATERMILL PLACE

14. Frances V. Gedezum

Frances V. Gedezum Menotomy Rd

15. Lawrence W Bannan

Lawrence W Bannan 14 Hillcrest St.  
Marys. Bannan 14 Hillcrest St

16. Mary Bannan

(please use additional sheets of paper for signatures if needed)