



## **Town of Arlington, MA Redevelopment Board**

### **Agenda & Meeting Notice September 27, 2021**

This meeting is being held remotely in accordance with the Governor's March 12, 2020 Order Suspending Certain Provisions of the Open Meeting Law G.L. c. 30A, Section 20. Public comments will be accepted during the public comment periods designated in the agenda. Per Board Rules and Regulations, public comments will be accepted during the public comment periods designated on the agenda. Written comments may be provided by email to [jraitt@town.arlington.ma.us](mailto:jraitt@town.arlington.ma.us) by September 27, 2021 at 4:00 p.m. The Board requests that correspondence that includes visual information should be provided by September 24, 2021 at 12:00 p.m.

The Arlington Redevelopment Board will meet Monday, September 27, 2021 at 7:30 PM in the **Join Zoom Meeting with audio and video by connecting using this link and Meeting ID: <https://town-arlington-ma-us.zoom.us/j/81902246161> | Meeting ID: 819 0224 6161 or join by calling: 1-646-876-9923, enter Meeting ID 819 0224 6161 then #.**

#### **1. Public Hearings**

7:30 p.m. - **Continued Public Hearing**

9:15 p.m. **Docket #3665, 645 Massachusetts Avenue**

Board will continue hearing Special Permit Docket #3665 in accordance with the provisions of MGL Chapter 40A § 11, and the Town of Arlington Zoning Bylaw Section 3.4, Environmental Design Review. The applicant proposes to establish a Chase Bank location on the premises at 645 Massachusetts Avenue, Arlington, MA in the B5 Business District. The continued hearing provides for additional Board review and public comment on the project under Section 3.4, Environmental Design Review and Section 6.2, Signs.

#### **Public Hearing**

**Docket #3673, 455-457 Massachusetts Avenue**

Notice is herewith given that an application has been filed on August 30, 2021 by 2-14 Medford Street, LLC, 455 Massachusetts Avenue, Suite 1, Arlington, MA, to open Special Permit Docket #3673 in accordance with the provisions of MGL Chapter 40A § 11, and the Town of Arlington Zoning Bylaw Section 3.4, Environmental Design Review. The applicant proposes to renovate existing building spaces and construct a mixed-use building containing retail, restaurants, and 13 residential units, including two affordable units, at 455-457 Massachusetts Avenue and 2-14 Medford Street in the B3 Village Business District. The opening of the Special Permit is to allow the Board to review and approve the development under Section 3.4, Environmental Design Review. This hearing is advertised to begin at 8:00 p.m.

### **Continued Public Hearing**

#### **Docket #3348, 833 Massachusetts Avenue**

Board will continue hearing Special Permit Docket #3348 in accordance with the provisions of M.G.L. Chapter 40A § 11, and the Town of Arlington Zoning Bylaw Section 3.4, Environmental Design Review, in order to review compliance with special condition 5 of the Special Permit Decision, dated April 13, 2009, and in order to hear from the property owner regarding such compliance. Special condition 5 of the Special Permit Decision refers to the future redevelopment of the Atwood House at 851 Mass Ave.

*Please be advised: for the first two public hearings, the following will occur: - applicants will be provided 15 minutes for a presentation*

*- DPCD staff will discuss public hearing memo*

*-Members of the public will be provided time to comment.*

*-Board members will discuss each docket and may vote.*

*For the third hearing, the Board will receive an update on the status of the property and review compliance with the EDR Special Permit conditions*

### **2. ARB Committee Appointments for ARB Members and Designees**

9:15 p.m.      *Board will review and approve ARB representation on various Town committees and vote to approve appointments of Board members and designees on various Town committees*

### **3. Draft FY22 ARB Updated Goals**

9:20 p.m.      *Board will review and approve goals*

### **4. Zoning Warrant Article filing process/ coordination with Redevelopment Board**

9:35 p.m.      *Board will review their approved Warrant Article filing process discussed in February 2021 and update to prepare for the next Town Meeting*

### **5. Meeting Minutes (8/30/21, 9/11/21)**

9:50 p.m.      *Board will review and approve meeting minutes*

### **6. Open Forum**

9:55 p.m.      *Except in unusual circumstances, any matter presented for consideration of the Board shall neither be acted upon, nor a decision made the night of the presentation. There is a three-minute time limit to present a concern or request.*

### **7. Adjourn**

10:15 p.m.      *Estimated time of adjournment*

### **8. Correspondence Received:**

S. Mraz 09082021

S. Tuttle 09122021

M. Guyton 09142021

D. Seltzer 09212021







## Town of Arlington, Massachusetts

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### Public Hearings

#### Summary:

7:30 p.m. -  
9:15 p.m.

#### **Continued Public Hearing**

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#### **ATTACHMENTS:**

Type	File Name	Description
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Reference Material	Continuation_Request__Docket_3665__645_Massachusetts_Avenue_received_9-27-2021.pdf	Continuation Request 645 Mass Ave. 9-27-2021
Reference Material	Agenda_Item_1_-_Memo_from_Town_Counsel_re_EDR_Process.pdf	New - Memo from Town Counsel re EDR Process
Reference Material	Updated_Memo_to_ARB_re_Docket_3665_09-22-21.pdf	New - Updated Memo to ARB re Docket #3665 09-22-21
Reference Material	20210922_Memo_Banks_in_Arlington_Center.pdf	New - 09222021 Memo Banks in Arlington Center
Reference Material	Docket#_3665_Request_to_be_continued_to_9-27-21_meeting.pdf	New - Docket #3665 Request to be continued to 9-27-21 meeting
Reference Material	B95122_CHS.NB.961_Arlington_Mass_Ave_r3_r1.pdf	New - 645 Mass Ave Docket 3665 Updated Application Materials
Reference Material	Chase_Arlington_MA_-_Response_to_ARB_Meeting_Review_Comments.pdf	New - Chase Arlington - Response to ARB Meeting Review Comments
Reference Material	Agenda_Item_1B_-_645_Mass_Ave_Combined_Application.pdf	645 Mass Ave Combined Application 8-11-21
Reference Material	Agenda_Item_1B_-_EDR_Public_Hearing_Memo_#3665_645_Mass_Ave.pdf	EDR Public Hearing Memo #3665 645 Mass Ave. - 8-25-21
Reference Material	EDR_Public_Hearing_Memo_Docket_3673_455-457_Mass_Ave.pdf	New - EDR Public Hearing Memo 455-457 Mass Ave Docket #3673
Reference Material	Memo_History_of_Commercial_Vacancies.pdf	New - Memo History of Commercial Vacancies
		New - Docket

Reference Material	Docket_3673_455-457_Mass_Ave_Application_Materials_-_time_stamped.pdf	#3673 455-457 Mass Ave Application Materials
Reference Material	FAR_calculation_by_applicant_via_email_083021.pdf	New - 455-457 Mass Ave. FAR calculation by applicant
Reference Material	Commercial_Area.pdf	New - 455-457 Mass Ave. Commercial Area
Reference Material	New_-_455_Mass_Ave._bike_racks_and_lighting_intent_update.pdf	New - 455 Mass Ave Bike Rack and lighting update 9-27-2021
Reference Material	NEW_Notification_of_Bylaw_Violations_821_Mass._Ave._to_G._Noyes_7-21-21.pdf	Notification of Bylaw Violations 821 Mass Ave. to G. Noyes 7-21-21
Reference Material	Letter_from_Annese_re_Atwood_House.pdf	Letter from Annese re Atwood House 9-23-20
Reference Material	ARB_vote_regarding_821_Mass_Ave_050420.pdf	ARB Vote Regarding 821 Mass Ave. 5-4-20
Reference Material	Letter_to_Noyes_re_special_permit_#3348_12-16-19_public_hearing_11182019.pdf	Letter to Noyes re special permit #3348 12-16-19 public hearing 11-18-19
Reference Material	Cover_Letter_Special_Permit_833_Mass_Ave.pdf	Cover Letter Special Permit 833 Mass Ave 12-10-19
Reference Material	Decision_Docket_3348_833_Mass_Ave.pdf	Decision Docket #3348 833 Mass Ave. 12-9-19
Reference Material	Poyant_Continuance_Letter__CVS_Arlington_MA.pdf	Poyant Continuance letter, CVS Arlington MA
Reference Material	EDR_Public_Hearing_Memo_833_Mass_Ave.pdf	EDR Public Hearing Memo 833

Material

Mass Ave 8-16-19

- ▢ Reference Material Letter\_to\_Noyes\_re\_Atwood\_House\_081519.pdf

Letter to Noyes re Atwood House 8-15-19

- ▢ Reference Material CVS\_Arlington\_MA\_Special\_Permit\_6.27.19\_reduced.pdf

CVS Arlington Special Permit 6-27-19

- ▢ Reference Material 2009\_Decision\_Docket\_3348.pdf

2009 Decision Docket #3348

From: Ian Heanue <iheanue@peconsultingcorp.com>  
To: Jenny Raitt <JRaitt@town.arlington.ma.us>, Mary Muszynski <MMuszynski@town.arlington.ma.us>, Kelly Lynema <KLynema@town.arlington.ma.us>  
Cc: "Marc Sides <msides@core-states.com>" <msides@core-states.com>, "Cabrera, Brooke C" <brooke.c.cabrera@chase.com>, "Steve Tomasello <stomasello@atlanticretail.com>" <stomasello@atlanticretail.com>, "brendan@noonanrealestate.com" <brendan@noonanrealestate.com>, "richard.ramsey@wtphelan.com" <richard.ramsey@wtphelan.com>  
Date: Mon, 27 Sep 2021 19:45:46 +0000  
Subject: Docket 3665, 645 Massachusetts Avenue

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**CAUTION:** This email originated from outside of the Town of Arlington's email system. Do not click links or open attachments unless you recognize the REAL sender (whose email address in the From: line in "< >" brackets) and you know the content is safe.

Good Afternoon,

Please accept this email as an official request to have our special permit application (Docket #3665) for JPMorgan Chase NA Bank, at 645 Massachusetts Avenue, Arlington MA, continued to the October 25<sup>th</sup>, 2021 board hearing.

This continuance will allow the team time to prepare our response in greater detail and have Bank representatives present.

If you have any questions or require any additional documentation please feel free to reach out.

Thank You,

Ian Heanue



**Project Expeditors Consulting Corp., WBE, WBENC Certified**  
Project Expeditors Inc, WBE, WBENC Certified  
Faneuil Hall Marketplace  
4 South Market Street  
Suite S-4035, Third Floor  
Boston, MA 02109  
Office: 617-227-0159  
Cell: 908-907-0484  
Fax: 617-227-0501



**Town of Arlington  
Legal Department**

Douglas W. Heim  
Town Counsel

50 Pleasant Street  
Arlington, MA 02476  
Phone: 781.316.3150  
Fax: 781.316.3159  
E-mail: [dheim@town.arlington.ma.us](mailto:dheim@town.arlington.ma.us)  
Website: [www.arlingtonma.gov](http://www.arlingtonma.gov)

To: Arlington Redevelopment Board;  
Jennifer Raitt, Director of Planning and Community Development

From: Douglas W. Heim, Town Counsel

Date: September 23, 2021

**Re: Special Permit Process for EDR Applications**

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Members of the Arlington Redevelopment Board (“ARB” or “Board”), you inquired as to the appropriate process and standards for hearing special permit applications subject to Environmental Design Review (“EDR”) under your purview; specifically, whether or not the Board should evaluate EDR standards under §3.4.4 if and when the Board (or some of its members) believes that a permit application should be denied for failure to satisfy the more general special permit criteria of §3.3.3. Articulated another way, should the Board essentially bifurcate its “regular” special permit criteria from its EDR standards to implement a rounded or “phased” permitting process where it anticipates a denial? As set forth fully below, this Office does not recommend adopting a phased special permit process for EDR-qualified applications at this time. If the Board were inclined to consider such an approach, it is recommended that your regulations and application materials be updated to more clearly reflect the purpose, parameters, and timing of phased or rounded special permit hearings.

## **Background and Context**

To this Office's understanding, the specific context of the Board's inquiry is a pending special permit application before it subject to EDR which may or may not present an undesirable or excessive use under special permit criteria 3.3.3(B) and (G). Board members inquired whether an initial determination that the proposed use was undesirable and/or excessive might foreclose further review at hearing of EDR standards and form the sufficient basis for a denial. The Director of Planning and Community Development responded that the Board's current practice under the Zoning Bylaw and ARB is not to bifurcate your consideration and decision making, and that a denial should incorporate the Board's full consideration of EDR standards as well as baseline special permit criteria of § 3.3.3. The Board, through its Chair sought further review of the issue and the opinion of this Office.

## **The Bylaw, Rules & Regulations, & EDR**

As noted in prior memos to the Board, you are a body of limited, but special jurisdiction, functioning as a Redevelopment Authority, Planning Board, and Special Permit Granting Authority (SPGA) through the lens of Environmental Design Review ("EDR") as codified in the Zoning Bylaw. Accordingly, approximately 10 percent of the Town's special permit applications are submitted to you, each involving commercial, industrial, larger scale residential, or mixed uses "which have a substantial impact on the character of the town and on traffic, utilities, and property values, thereby affecting the public health, safety and general welfare."

In order to accomplish your goals and realize the ARB's purpose as set forth in the Bylaw, the Board utilizes the more rigorous, but also more flexible and subjective toolkit of the EDR process, which adds to special permitting standards and processes established for predominantly (though not exclusively) residential uses currently governed by the Zoning Board Appeals ("ZBA"). Indeed, §3.4.3 of the Zoning Bylaw outlines a specific procedure for EDR projects which does not apply to "regular" special permits as follows:

### **Procedures**

A. Application. Applicants shall submit an application for Environmental Design Review in accordance with the Arlington Redevelopment Board's ("Board") rules and regulations.

B. The Board shall hold a public hearing in *accordance with Section 3.3 of this Bylaw* and G.L. c. 40A, §§ 9 and 11.

C. The Board shall refer the application to the Department of Planning and Community Development ("Department"), which shall prepare and submit written reports with recommendations to the Board before or at the public hearing. The Board shall not take final action on the special permit application until it has received the Department's report or until 35 days have elapsed after submittal of the proposal to the Department. Failure of the Department to submit written reports



or to give an oral report at the public hearing shall not invalidate action by the Board.

D. A favorable decision by the Board shall require the votes of at least four members.

*E. The Board shall not deny a special permit under this Section 3.4 unless it finds that the proposed use does not comply with the Environmental Design Review Standards listed below to such a degree that such use would result in a substantial adverse impact upon the character of the neighborhood or the town, and upon traffic, utilities, and public or private investments, thereby conflicting with the purposes of this Bylaw.*

(emphasis added).

Reading §§ 3.4.3 (B) and (E) harmoniously suggests that EDR special permitting reflects a holistic approach whereby the ARB requires information fully responsive to both the Special Permit Criteria set forth in §3.3.3 and the twelve (12) EDR criteria set forth in §3.4.4. These requirements are echoed in Rule 14 of your Rules and Regulations, and further clarified by Rule 15, which states:

#### RULE 15 : BOARD DECISIONS

The ARB shall review the plans and may grant a special permit subject to the conditions and safeguards listed in the Arlington Zoning Bylaw *Section 3.3 and 3.3.4*. For stated reasons the ARB may deny approval of a special permit or may approve a special permit without a finding of hardship. As required by M.G.L. c. 40A, §9, a positive vote of at least four members of the Redevelopment Board is needed to issue a special permit. Upon the Board's approval, the Secretary Ex-Officio may sign decisions following a vote of the Board and file decisions per requirements of M.G.L. c. 40A. The final decision shall be emailed and may receive administrative corrections following the Board's votes.

(emphasis added).

Based on the Bylaw, ARB Rules and Regulations, your application requirements, as well as a review of a collection of past decisions of the Board (and c. 40A), it is clear that in any application for an EDR permit, the applicant must address all the Special Permit Criteria set forth in §3.3.3 *and* the twelve (12) EDR criteria set forth in §3.4.4; and further, any approval decision by the ARB must at least assess and address same.<sup>1</sup> The only question therefore is whether or not a denial (or anticipated denial) would be excused from the same process and standard if rooted in

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<sup>1</sup> It may well be, and indeed several of your decisions reflect, that one or even several specific EDR standards are not central to the ARB's thinking on any given application, and/or that conditions on a permit focus on some EDR standards more than others.

a failure to satisfy §3.3.3. For the reasons discussed below, this Office concurs with the Director of Planning and Community Development's assessment that your best practice is to utilize the same process in any instance where denial is possible, but not objectively certain on the face of the application.

## Analysis

As an initial matter, the nuance of denials under c. 40A should be noted. On one hand, c. 40A §9 provides that a Special Permit Granting Authority "shall cause to be made a detailed record of its proceedings, indicating the vote of each member upon each question, or if absent or failing to vote, indicating such fact, and setting forth clearly the reason for its decision and of its official actions..." Further, a decision granting a special permit must include any findings required by the municipal ordinance or bylaw, as well as the findings required by the applicable provisions of the Zoning Act. *Sheehan v. Zoning Bd. of Appeals of Plymouth*, 65 Mass. App. Ct. 52, 56 (2005).

On the other, favorable actions require more vigorous support articulating the basis for the grant of a special permit than a denial of same. *Gamache v. Town of Acushnet*, 14 Mass. App. Ct. 215 (1982) (denial of variance based on town's policy against trailer parks is sufficient absent a clear record to the contrary); *Board of Aldermen of Newton v. Maniace*, 429 Mass. 726 (1999) (even failure to obtain requisite affirmative votes for a draft decision constitutes a sufficient basis for denial). Nonetheless, as a general rule of practice it should be rare that a process is determined by a potential (or even likely) outcome absent a very clear roadmap for fast-tracking a decision or failure to meet entirely objective threshold criteria. In other words, the difficulty in making a decision solely on the basis of §3.3.3 criteria is standardizing the Board's level of certainty that a vote on a subject criteria – the desirability or concentration of a particular use – preempts all further development of the record.

There are examples where boards and bodies adopt a "phased" or "rounded" process" whereby only some facets of an application are considered in different stages. Under such processes, applications essentially pass or fail (typically on a more narrow set of criteria) before proceeding on to the next stage of analysis with the specific goals identified and served by evaluating only portions of an application. In most of those cases however, there is no prejudicial impact of a denial akin to the two (2) year prohibition on repeat applications found in c. 40A §16. The ARB theoretically could implement such a process. However, at present the ARB's Rules and Regulations and application materials do not provide a clear roadmap for the goals, timing, or tools necessary of bifurcating EDR special permit applications into §3.3.3 analysis and *then* §3.4.4 analysis at some later phase.

For example, it is not clear when and how the Board would assess an application and take a vote to make a threshold determination on the baseline Special Permit criteria under §3.3 of the Zoning Bylaw. Would an unsuccessful motion to deny based solely on §3.3.3 criteria preclude later denial on the same grounds after application of EDR standards? If a member of the Board

has further bases for denial that have not yet been addressed on the record under EDR, may those concerns be articulated in the decision as well? Are they sufficiently supported in the record?

It is similarly unclear when and to what extent an applicant's response to EDR standards under §3.4.4 can help or hinder the Board's assessment of §3.3 criteria under a bifurcated review. Both your Bylaw and your Rules and Regulations imply interplay between these criteria. There may be circumstances where after the more robust application of EDR, a member of the ARB is persuaded or dissuaded that a given project is more or less responsive to being "essential or desirable to the public convenience or welfare" – one of the baseline §3.3.4 criteria. Similarly, the Board might be deterred by or impressed with an applicants' proposal with regard to EDR criteria "J" ("[w]ith respect to Arlington's heritage, removal or disruption of historic, traditional or significant uses, structures, or architectural elements shall be minimized insofar as practicable, whether these exist on the site or on adjacent properties") in such a manner as to inform the desirability of the use under §3.3.3(B).

Moreover, while the standards for denials of special permit applications are more modest than approvals, in order to best defend its decisions and convey the basis for denials to future applicants, the Board may articulate any and all reasons for denial in the most comprehensive manner practicable. Alternately stated, if the Board is denying a permit for *both* causing an excess of use detrimental to the character of a neighborhood (3.3.3(G)) *and* negative findings with respect to relation of buildings to the environment (3.4.4(B)), such denial is all the better supported. Without engaging in EDR, it begs the question of why EDR supporting materials were required in the first instance and could undermine an otherwise valid denial with respect to any basis from the desirability of use (3.3.3(B)) to unduly impairing pedestrian safety (3.3.3(C)).

The foregoing should not be read to imply that the Board may not deny a special permit under EDR for one of the reasons set forth in §3.3.3 such as an excessive use, or that only EDR standards ought to form the basis for approval or denial to the exclusion of §3.3.3. Such a determination however typically involves some subjective, qualitative judgment, which may be informed positively or negatively by application of full EDR standards and process. There may also be rare instances where an application cannot reasonably proceed because the use requested is not permitted in a district and an applicant has submitted despite efforts to persuade them otherwise. This Office is sensitive to the demands upon the ARB's time and attention. It remains however in the Board's interest to fully examine EDR applications under a full EDR process unless objective or procedural denials are merited, and/or Rules and Regulations harmonious to the Zoning Bylaw are developed to support a bifurcated or staged application review.

### **Conclusion**

For the reasons set forth herein, this Office agrees with the Director of Planning and Community Development's recommendation to assess Special Permit Applications before you with both "Special Permit" Criteria under §3.3.3 and EDR Standards under §3.4.4 before voting upon your decisions unless and until the Board commits to a more detailed bifurcated or phased process in your Rules and Regulations.



**Town of Arlington, Massachusetts**  
Department of Planning & Community Development  
730 Massachusetts Avenue, Arlington, Massachusetts 02476

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**Public Hearing Memorandum - Update**

*The purpose of this memorandum is to provide the Arlington Redevelopment Board and public with technical information and a planning analysis to assist with the regulatory decision-making process.*

**To:** Arlington Redevelopment Board

**From:** Jennifer Raitt, Secretary Ex Officio

**Subject:** Environmental Design Review, Docket #3665  
645 Massachusetts Avenue

**Date:** September 22, 2021

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This memo is provided as an update to the previous memo provided on August 30, 2021. The materials provided for the continued public hearing are not responsive to the ARB's comments. The Applicant provided the following materials:

- Cover letter to the Redevelopment Board, dated September 18, 2021; and
- Sign Package, including elevations and renderings, prepared by Philadelphia Sign, revised September 9, 2021.

The Board requested that the Applicant install clear-glazed windows along the streetfront façade that align with the commercial district and use; move the proposed blade sign; maintain the existing sidewalk seating and planters from the prior tenant; address the rear access point for accessibility; and incorporate the required bike parking. The Board also asked the Applicant to provide a better description of their services and how they differ from similar business uses in Arlington Center.

Regarding the windows along the Massachusetts Avenue and David Lamson Way facades, the renderings and elevations in the sign package show that the gridded windows will be replaced with commercial windows. It appears from the renderings that the glazing has also been addressed. Specifications on window type and glazing have not been provided.

With regard to signage, the Board can find that the request to move the proposed blade has been addressed.

Regarding sidewalk seating and planters, the applicant has revised the renderings to show seating and planters along the Massachusetts Avenue façade. The applicant will need to provide updated site and/or floor plans indicating the locations of these items.

Regarding bicycle parking, the applicant states that submission materials have been updated to show short term bicycle parking in front of the site. A bicycle rack is shown on the rendering on DRC Page 8 of the updated sign package. The applicant should update site and/or floor plans accordingly, indicate the amount of short-term bicycle parking provided. While the applicant indicates that long-term parking will be integrated into the project, the applicant will need to provide updated floor plans identifying the location, quantity, and type of long-term bicycle parking. Bike rack specifications for short- and long-term parking are needed to determine compliance with Section 6.1.12.E.

Regarding providing an accessible entrance at the rear of the building, the applicant concluded that the existing conditions prohibit their ability to improve accessibility through the rear entrance. The closest HP placard parking space to the main entry and ATM is in front of the Coldwell Banker property at 635 Massachusetts Avenue.

The applicant should provide a more detailed update regarding any of the above items at the continued hearing.

Representatives from Chase Bank will be in attendance at the September 27, 2021 hearing to present information on the services they intend to provide at this location, as well as to answer questions regarding any market studies that were completed as part of the bank's decision to establish a branch at this location.

Finally, staff were asked to calculate the total linear feet of banks within Arlington Center. For economic development purposes, the Town defines the Arlington Center as 375 to 1056 Massachusetts Avenue; 283 to 327 Broadway; Medford, Mill, Mystic Streets; Schouler Court; Summer Street. Within these limits are a total of 264 commercial units<sup>1</sup>.

There are a total of nine banks or ATMs in Arlington Center, or 3.4% of total commercial units. The table below provides the bank name, type, address, parcel frontage, and building width. Parcels on corner lots are indicated with an asterisk; for corner lots, the total parcel and building frontage along both streets are listed first, followed by the frontage specific to Massachusetts Avenue in parentheses.

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<sup>1</sup> According to Arlington Town Assessor Data. This figure does not include the 180 individual rental units and commercial condos within commercial properties like 22 Mill, 29 Mill, etc.

Bank name	Type	Address	Parcel frontage (ft)	Street-facing building width (ft)
Chase Bank	ATM	323 Broadway	n/a <sup>2</sup>	23
Leader Bank	Bank Branch	449 Mass Ave*	n/a <sup>3</sup>	70 (26)
Cambridge Savings Bank	Bank Branch and Drive-up	626 Mass Ave*	285 (115)	187 (133)
Bank of America	Bank Branch	655 Mass Ave	51	48
Citizens Bank	Bank Branch and Drive-up	699 Mass Ave*	278 (151)	139 (82)
Brookline Bank	Bank Branch and Drive-up	856 Mass Ave*	204 (91)	135 (67)
Leader Bank	Residential Lending	864 Mass Ave*	218 (107)	154 (103)
TD Bank North	Bank Branch and Drive-up	880 Mass Ave*	408 (179)	182 (80)
Watertown Savings Bank	Bank Branch and Drive-up	980 Mass Ave*	268 (110)	155 (69)
<b>TOTAL</b>			<b>1,712 (804)</b>	<b>1,093 (631)</b>
* Corner lot				

Linear calculations have been estimated using the Town's GIS data. Should precise numbers be required, individual site plans would need to be requested from each property.

<sup>2</sup> Tenant in a structure with multiple units on one lot

<sup>3</sup> Tenant in a structure with multiple units on one lot



September 18, 2021

Town of Arlington  
Redevelopment Board  
730 Mass Ave. Annex  
Arlington, MA 02476

Re: Application for Special Permit Review, 645 Massachusetts Avenue – Second Review

The whom it may concern;

We would like to thank the board for their time during the August 30, 20201 Arlington Redevelopment Board meeting. The input that was provided during this meeting was taken into consideration in preparation for our second appearance on the topic of the special permits requested for this site.

To respond to some of the comments issued, we offer the following, in conjunction with the attached revised documents:

**Automobile Parking Relief:** It appeared, from the response from the board, that they were in favor of granting this relief, on the grounds of the large municipal lot behind the proposed site and the presence of on-street parking in front. NO RESPONSE REQUIRED

**Bicycle Parking Relief:** Per the board, no relief would be granted on the bicycle parking requirement. THE SUBMISSION MATERIALS HAVE BEEN REVISED TO REFLECT SHORT TERM BICYCLE PARKING IN FRONT OF THE PROPOSED SITE. LONG TERM PARKING WILL BE INTEGRATED INTO THE PROJECT AS WELL TO MEET THE REQUIREMENT.

**Special Permit for the Installation of a Bank:**

Change of Use: Chase bank is seeking a special permit to allow for a bank use greater than 2,000 sf within the B5 District, as required by the zoning by-laws. Additionally, the previous use was a restaurant, requiring a change of use to the proposed bank (business) use. THE BOARD WAS SPLIT ON THE TOPIC. CHASE BANK REQUESTED A CONTINUATION IN ORDER TO ASSEMBLE ADDITIONAL PRESENTATION MATERIALS AND HAVE REPRESENTATIVES FROM CHASE BANK PRESENT TO DISCUSS THE PROJECT.

Façade: The board recommended changing the existing gridded windows to match the proposed new windows. THE RENDERINGS HAVE BEEN REVISED TO INCORPORATE THIS CHANGE

Sidewalk Amenities: The previously proposed plan reflected the removal of the existing benches and planters. The board recommended retaining this component of the exterior. THE RENDERINGS HAVE BEEN REVISED TO INCORPORATE THIS CHANGE



Accessibility: The board questioned why the vestibule at the rear of the building was not being made accessible and requested studying the addition of a ramp or a wheelchair lift. THERE ARE MULTIPLE EXISTING CONSTRAINTS THAT WILL LIMIT THE ABILITY TO MAKE THE REAR DOOR FULLY ACCESSIBLE, INCLUDING IMPACTS TO THE UPPER-LEVEL TENANT ENTRANCE, EXISTING (MUNICIPAL) PARKING LOT, AND SIDEWALK ALONG DAVID LAMSON WAY. IN SPITE OF THESE CONSTRAINTS, CHASE WILL CONTINUE TO ENDEAVOR TO STUDY THESE CONDITIONS AND EVALUATE THE VIABILITY OF AN ACCESSIBLE REAR ENTRANCE. PLEASE NOTE THAT THERE IS RESERVED ACCESSIBLE STREET PARKING ON MASS AVE NEAR THE FRONT ENTRANCE, WHICH WOULD PROVIDE ACCESS TO THE BANK AND ATM VESTIBULE.

Signage: The board was not in favor of the proposed blade sign. They believed the sign was not in keeping with other signage in the Central Business District as asked if we could consider moving the sign on the gray band along the alley side of the building. THE RENDERINGS HAVE BEEN REVISED TO INCORPORATE THIS CHANGE

We look forward to the opportunity to meet with the board again to review this project. Please let us know if anything additional is required.

Sincerely,

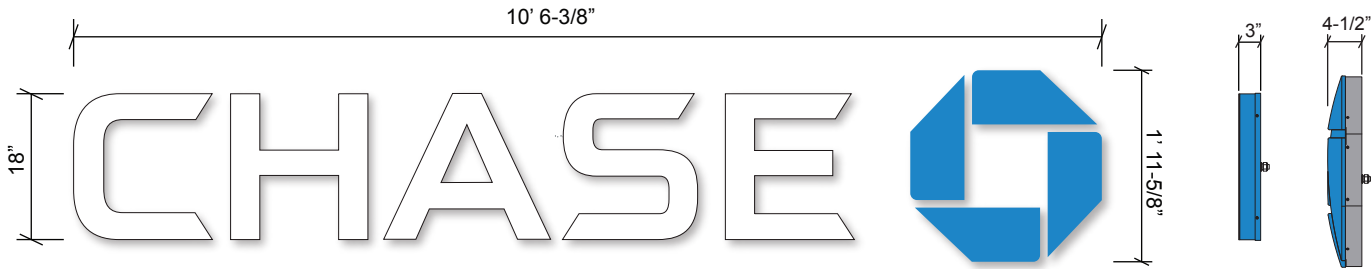
James Lalli  
Director of Architecture - Financial  
908.462.9949 | jlalli@core-states.com





South Elevation - Massachusetts Ave

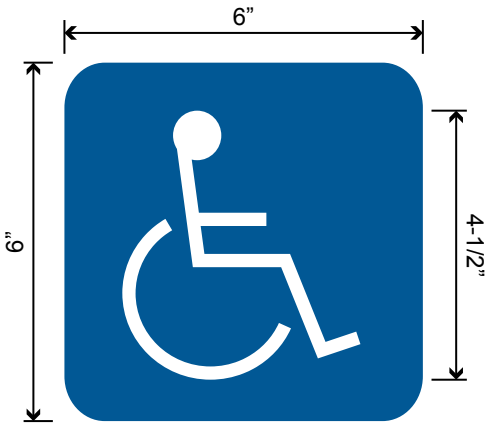
SIGN E10 NOT SEEN FROM THIS ANGLE



LIF-WBO-18-LED  
WHITE W/ BLUE OCTAGON ILLUM CHANNEL LETTERS - 20.7 SF  
SCALE: NTS



NMP-XX  
DARK NICKEL METAL PAN - DIMENSIONS TBD  
SCALE: NTS



ADA-EP  
HANDICAPPED ENTRANCE PLAQUE  
SCALE: NTS  
  
CUST-VIN  
MATCHING BLUE VINYL BACKER  
SCALE: NTS



SIGN E10 MOUNTS BACK-TO-BACK WITH SIGN I01.



CHS.NB.961 - Arlington Mass Avenue  
645 Massachusetts Avenue  
Arlington, MA 02476

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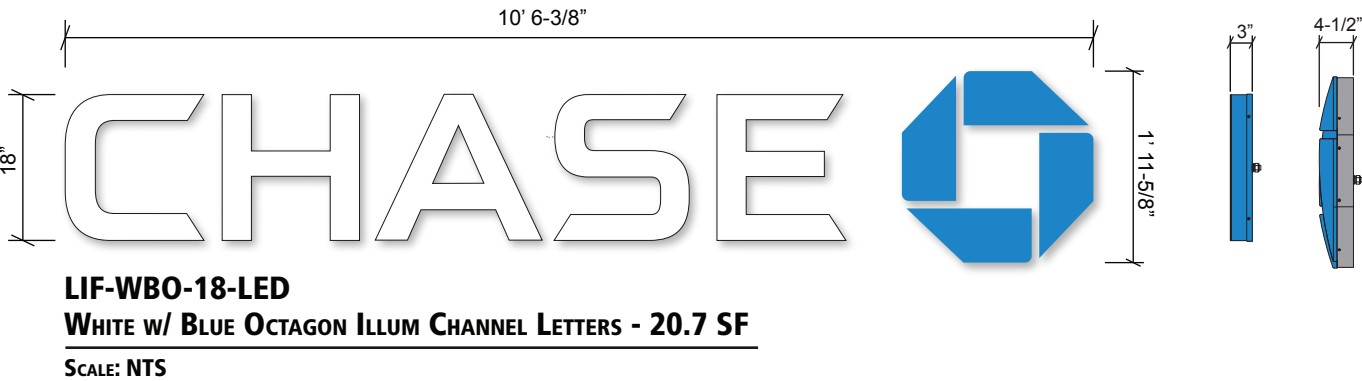
DESIGNER - JM  
  
CREATED - 06.17.21  
  
DRAWING - B95122

 PHILADELPHIASIGN  
BRINGING THE WORLD'S BRANDS TO LIFE  
  
707 WEST SPRING GARDEN ST • PALMYRA, NJ • 08065  
P: 856-829-1460 • F: 856-829-8549 • WEB: <http://www.philadelphiasign.com>

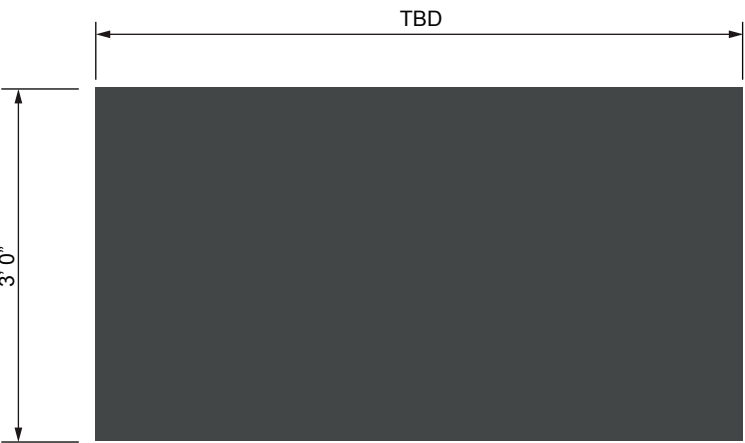
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East Elevation - David Lamson Way



**LIF-WBO-18-LED**  
**WHITE W/ BLUE OCTAGON ILLUM CHANNEL LETTERS - 20.7 SF**  
SCALE: NTS



**NMP-XX**  
**DARK NICKEL METAL PAN - DIMENSIONS TBD**  
SCALE: NTS



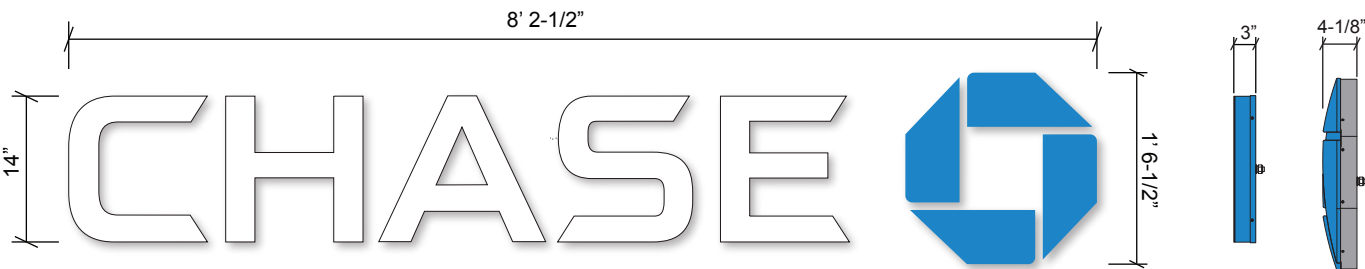
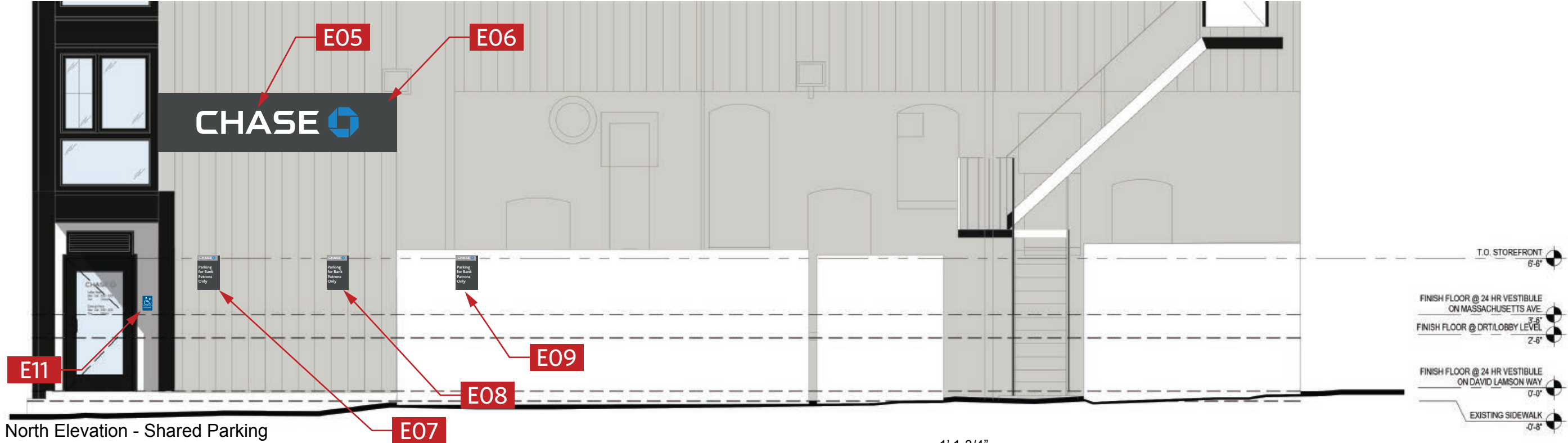
CHS.NB.961 - Arlington Mass Avenue  
645 Massachusetts Avenue  
Arlington, MA 02476

DESIGNER - JM  
CREATED - 06.17.21  
DRAWING - B95122

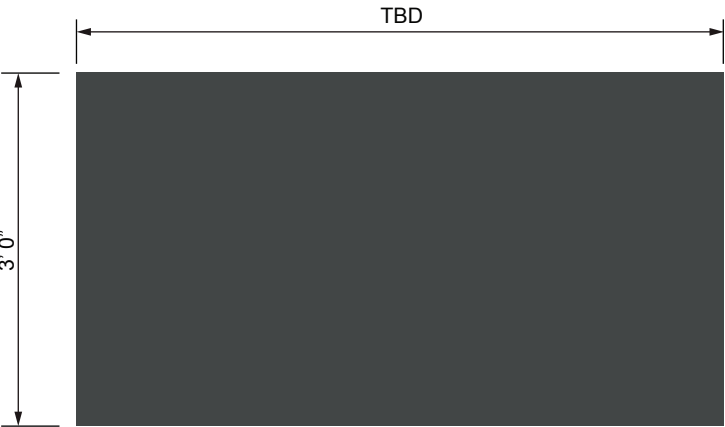


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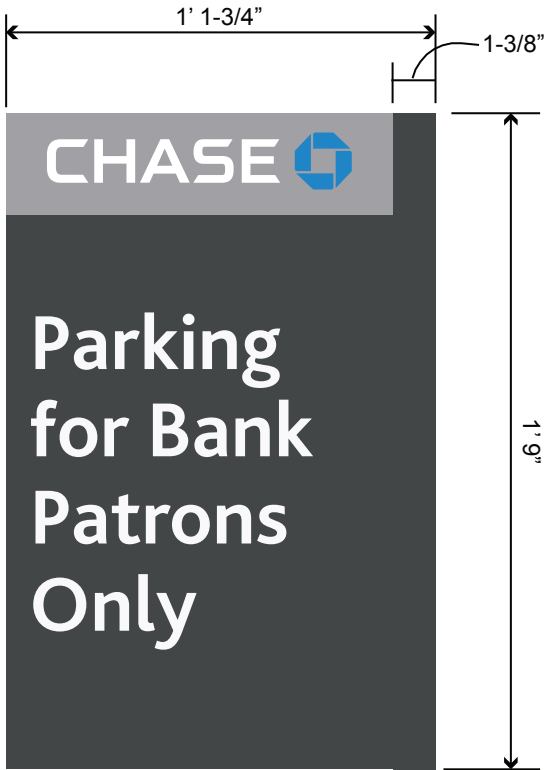
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**LIF-WBO-14-LED**  
**WHITE W/ BLUE OCTAGON ILLUM CHANNEL LETTERS - 12.7 SF**  
SCALE: NTS



**NMP-XX**  
**DARK NICKEL METAL PAN - DIMENSIONS TBD**  
SCALE: NTS



**TC-W-A-RE**  
**WALL MOUNTED REGULATORY SIGN (RE-ENGINEERED) - 2SF**  
SCALE: NTS



**ADA-EP-NA**  
**HANDICAPPED ENTRANCE PLAQUE - NOT ACCESSIBLE**  
SCALE: NTS



CHS.NB.961 - Arlington Mass Avenue  
645 Massachusetts Avenue  
Arlington, MA 02476

DESIGNER - JM  
CREATED - 06.17.21  
DRAWING - B95122

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ARCHITECTURAL RENDERING - CORNER ELEVATION



**CHS.NB.961 - Arlington Mass Avenue**  
645 Massachusetts Avenue  
Arlington, MA 02476

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**DESIGNER** - JM  
**CREATED** - 06.17.21  
**DRAWING** - B95122

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ARCHITECTURAL ELEVATION - MASSACHUSETTS AVENUE



**CHS.NB.961 - Arlington Mass Avenue**  
645 Massachusetts Avenue  
Arlington, MA 02476

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**DESIGNER** - JM  
**CREATED** - 06.17.21  
**DRAWING** - B95122



**PHILADELPHIASIGN**

BRINGING THE WORLD'S BRANDS TO LIFE

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ARCHITECTURAL ELEVATION - DAVID LAMSON WAY ELEVATION



CHS.NB.961 - Arlington Mass Avenue  
645 Massachusetts Avenue  
Arlington, MA 02476

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DESIGNER - JM  
CREATED - 06.17.21  
DRAWING - B95122



PHILADELPHIASIGN  
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Survey Photos



CHS.NB.961 - Arlington Mass Avenue  
645 Massachusetts Avenue  
Arlington, MA 02476

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DESIGNER - JM  
CREATED - 06.17.21  
DRAWING - B95122



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Approach Photos



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645 Massachusetts Avenue  
Arlington, MA 02476

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DESIGNER - JM  
CREATED - 06.17.21  
DRAWING - B95122



PHILADELPHIASIGN

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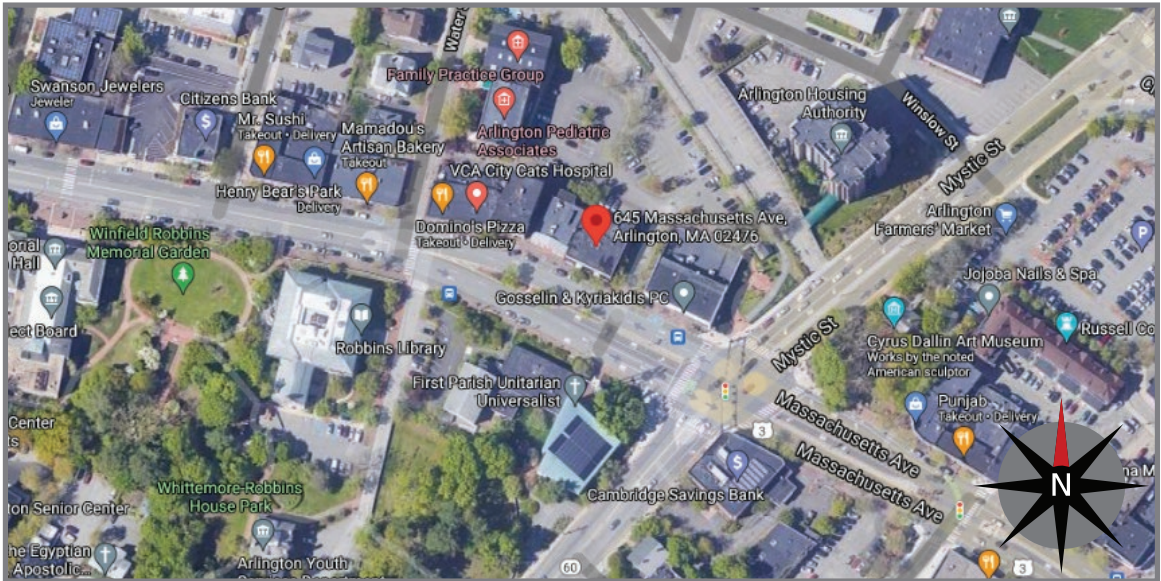
ARLINGTON MASS AVENUE

645 Massachusetts Avenue  
Arlington, MA 02476

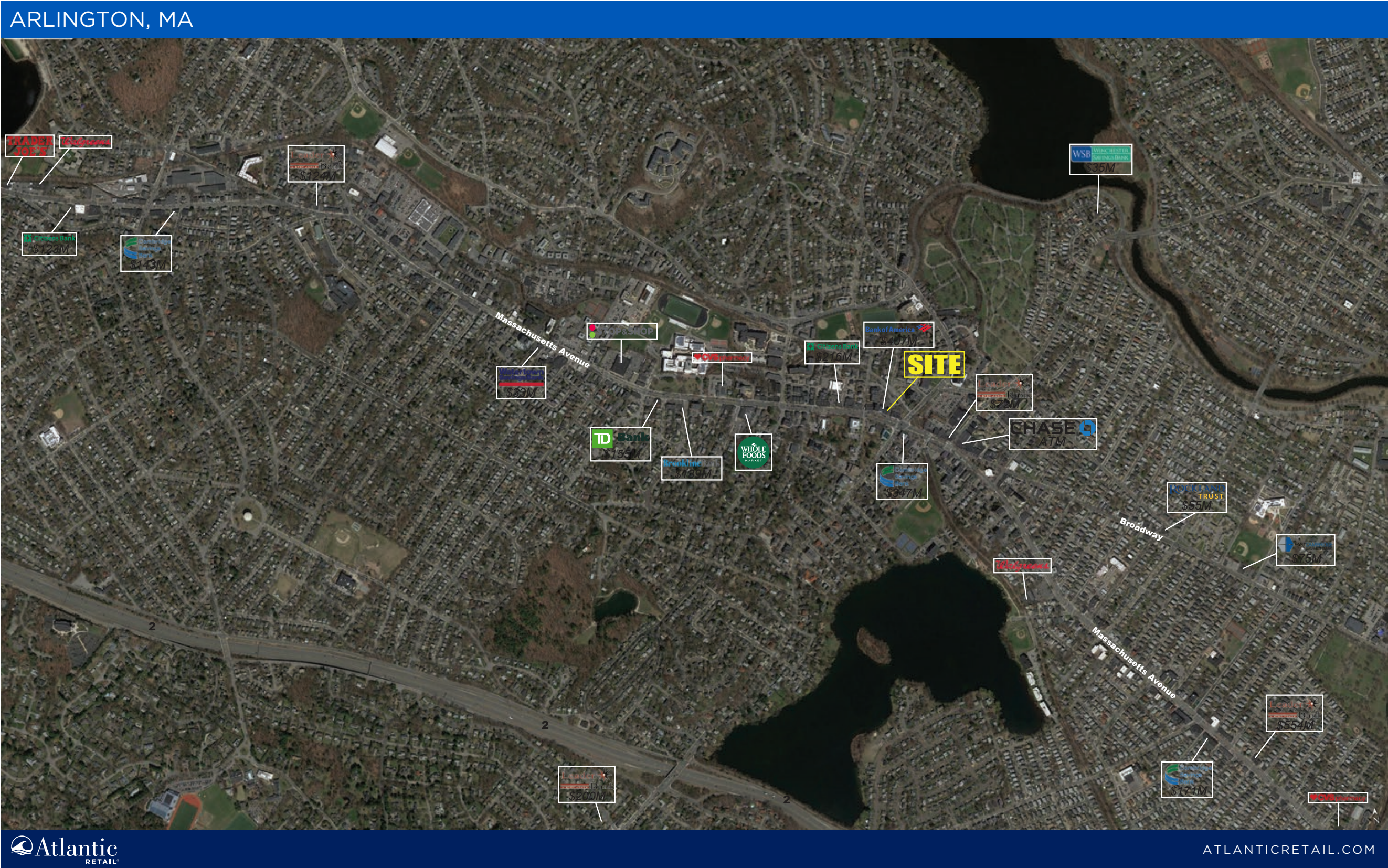


**REVISION NOTES:**

06.23.21	JM	Delete Octagons from All Awnings. Add Option 2, E12 and E13
07.01.21	RJW	Revised as noted.
09.09.21	RJW	Delete E03, add lettersets & elevations.







**CHS.NB.961 - Arlington Mass Avenue**  
645 Massachusetts Avenue  
Arlington, MA 02476

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**DESIGNER** - JM  
**CREATED** - 06.17.21  
**DRAWING** - B95122



PHILADELPHIASIGN

BRINGING THE WORLD'S BRANDS TO LIFE

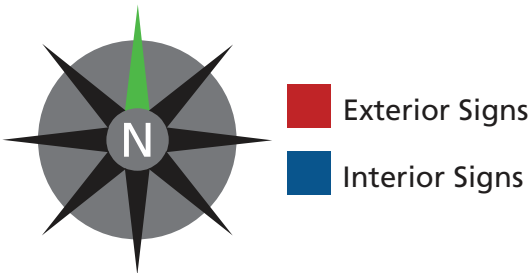
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Exterior Scope of Work			
E01	LIF-WBO-18-LED	WHITE W/ BLUE OCTAGON ILLUM CHANNEL LETTERS	20.7SF
E02	NMP-XX	DARK NICKEL METAL PANEL	
E04	NMP-XX	DARK NICKEL METAL PANEL	
E05	LIF-WBO-14-LED	WHITE W/ BLUE OCTAGON ILLUM CHANNEL LETTERS	12.7SF
E06	NMP-XX	DARK NICKEL METAL PANEL	
E07	TC-W-A-RE	WALL MTD REGULATORY SIGN - BANK PATRONS ONLY (Re-ENGINEERED)	2SF
E08	TC-W-A-RE	WALL MTD REGULATORY SIGN - BANK PATRONS ONLY (Re-ENGINEERED)	2SF
E09	TC-W-A-RE	WALL MTD REGULATORY SIGN - BANK PATRONS ONLY (Re-ENGINEERED)	2SF
E12	LIF-WBO-18-LED	WHITE W/ BLUE OCTAGON ILLUM CHANNEL LETTERS	20.7SF

ALL SIGNS TO BE  
REVIEWED/APPROVED  
BY HRC



CHS.NB.961 - Arlington Mass Avenue  
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Arlington, MA 02476

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DESIGNER - JM  
CREATED - 06.17.21  
DRAWING - B95122



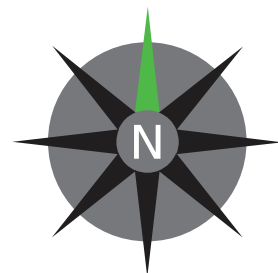
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## Floor Plan

## Interior Scope of Work

I01	ADA-EX	ADA HANDICAPPED EXIT PLAQUE	.25SF
I01.1	CUST-VIN	MATCHING BRONZE VINYL BACKER	
I02	ADA-EX	ADA HANDICAPPED EXIT PLAQUE	.25SF
I02.1	CUST-VIN	MATCHING BRONZE VINYL BACKER	
I03	ADA-RRAG-A-G	ADA ALL GENDER RESTROOM SIGN - ACCESSIBLE	1.4SF
I04	ADA-RRAG-A-G	ADA ALL GENDER RESTROOM SIGN - ACCESSIBLE	1.4SF
I05	ADA-TW	ADA TELLER WALL SIGN	.1SF
I06	ADA-TW-ALS	ADA TELLER WINDOW - ASSISTIVE LISTENING SYSTEM	.1SF
I07	TPL-BTR-B-24	24" THIN PROFILE ILLUMINATED INTERIOR BLUE OCTAGON	4SF
I08	eATM-PANEL-SINGLE-SIDE-CAR	BRANDING PANEL FOR SINGLE SIDE CAR eATM 3.0	
I09	SUR-TTW-U-4-TP	ILLUMINATED THIN PROFILE ATM SURROUND	33SF
E10	ADA-EP	ADA HANDICAPPED ENTRANCE PLAQUE	.25SF
E10.1	CUST-VIN	MATCHING BLUE VINYL BACKER	
E11	ADA-EP-NA	ADA HANDICAPPED ENTRANCE PLAQUE - NOT ACCESSIBLE	.38SF



## Exterior Signs

## Interior Signs



## CHS.NB.961 - Arlington Mass Avenue

645 Massachusetts Avenue  
Arlington, MA 02476

30 of 347

**DESIGNER - JM**

**CREATED** - 06.17.21

**DRAWING - B95122**

PHILADELPHIA **SIGN**

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South Elevation - Massachusetts Ave

SIGN E10 NOT SEEN FROM THIS ANGLE

10' 6-3/8"

CHASE

1' 11-5/8"

3"

4-1/2"

LIF-WBO-18-LED

WHITE W/ BLUE OCTAGON ILLUM CHANNEL LETTERS - 20.7 SF

SCALE: NTS

TBD

3' 0"

NMP-XX

DARK NICKEL METAL PAN - DIMENSIONS TBD

SCALE: NTS

6"

9"

4-1/2"

ADA-EP

HANDICAPPED ENTRANCE PLAQUE

SCALE: NTS

CUST-VIN

MATCHING BLUE VINYL BACKER

SCALE: NTS

SIGN E10 MOUNTS BACK-TO-BACK WITH SIGN I01.



CHS.NB.961 - Arlington Mass Avenue  
645 Massachusetts Avenue  
Arlington, MA 02476

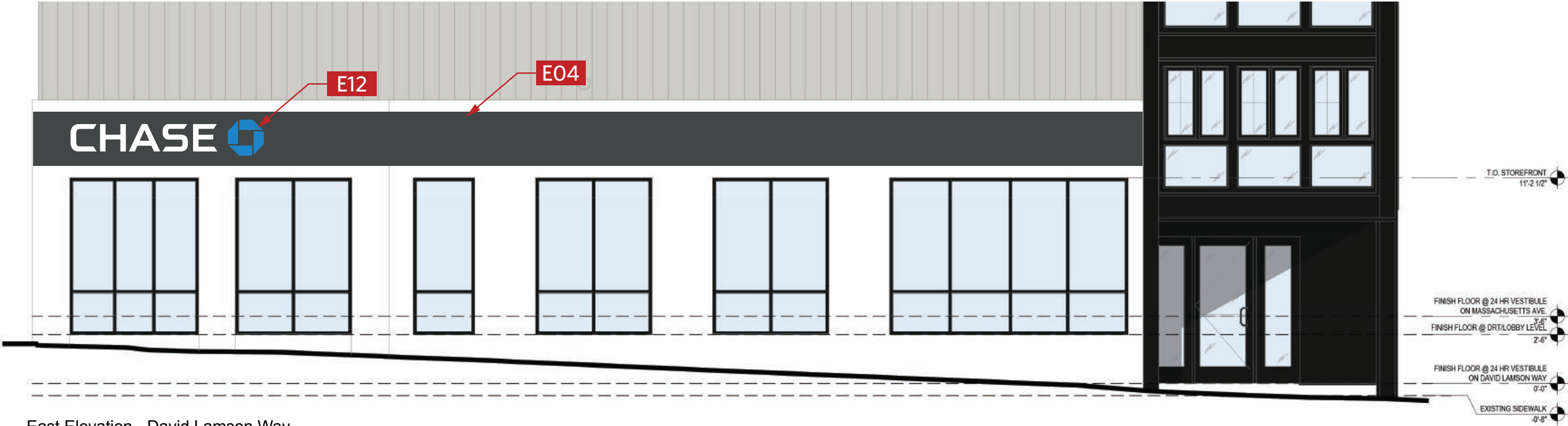
DESIGNER - JM  
CREATED - 06.17.21  
DRAWING - B95122

PHILADELPHIASIGN

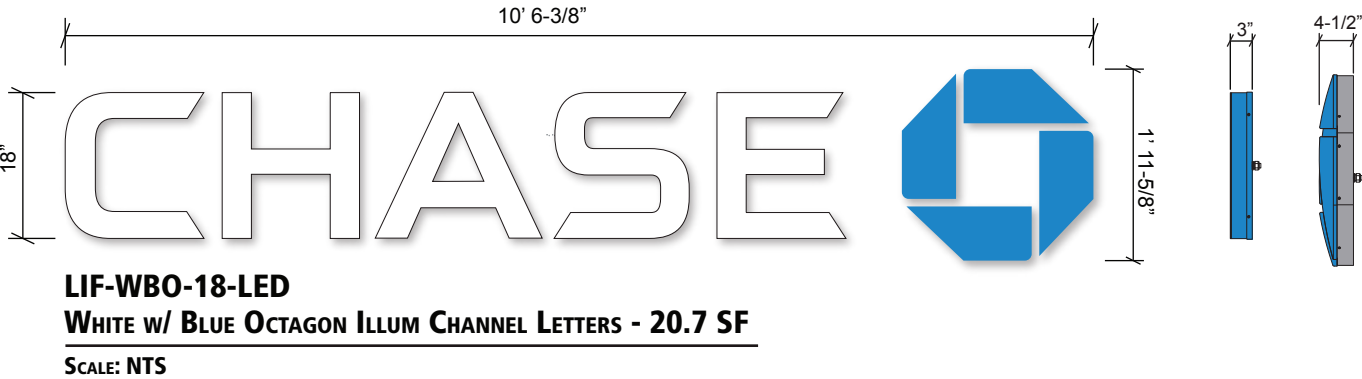
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East Elevation - David Lamson Way



**NMP-XX**  
**DARK NICKEL METAL PAN - DIMENSIONS TBD**  
SCALE: NTS



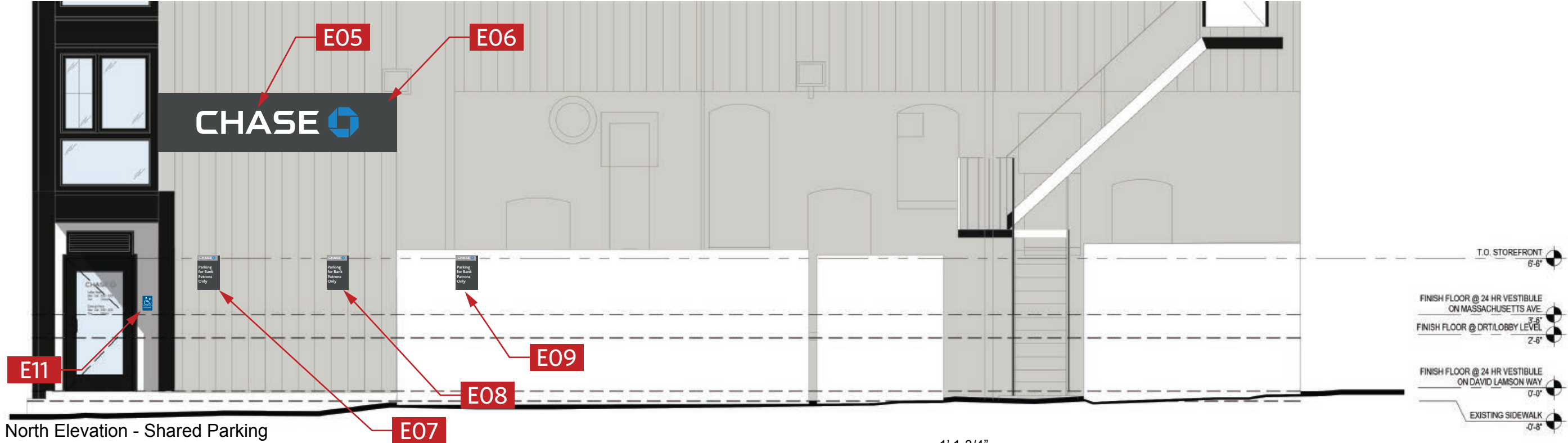
CHS.NB.961 - Arlington Mass Avenue  
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Arlington, MA 02476

DESIGNER - JM  
CREATED - 06.17.21  
DRAWING - B95122

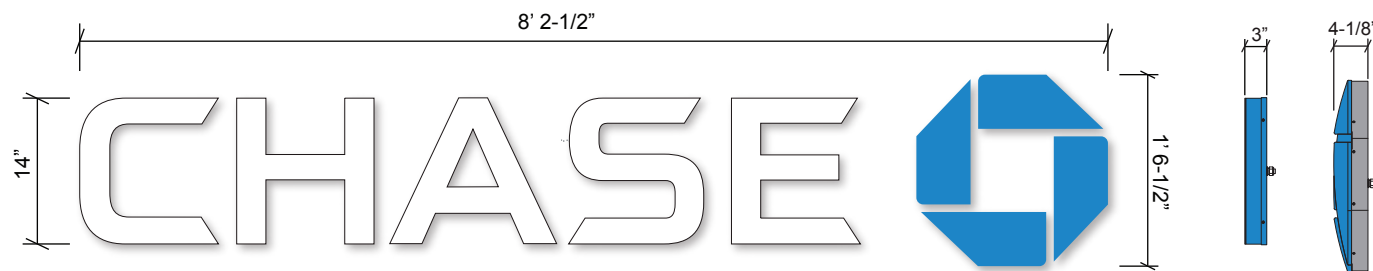


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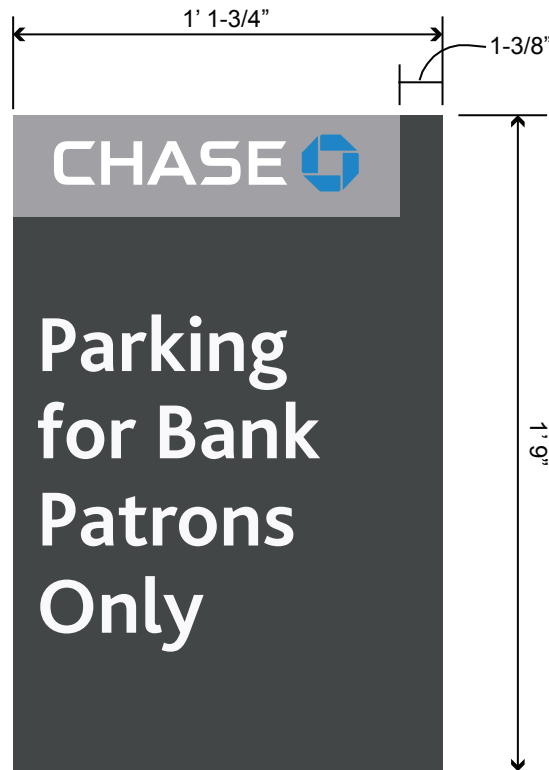
North Elevation - Shared Parking



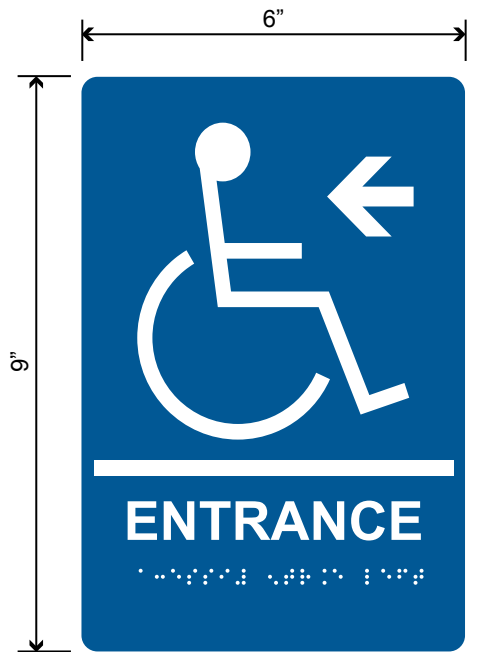
**LIF-WBO-14-LED**  
**WHITE W/ BLUE OCTAGON ILLUM CHANNEL LETTERS - 12.7 SF**  
SCALE: NTS



**NMP-XX**  
**DARK NICKEL METAL PAN - DIMENSIONS TBD**  
SCALE: NTS



**TC-W-A-RE**  
**WALL MOUNTED REGULATORY SIGN (RE-ENGINEERED) - 2SF**  
SCALE: NTS



**ADA-EP-NA**  
**HANDICAPPED ENTRANCE PLAQUE - NOT ACCESSIBLE**  
SCALE: NTS



CHS.NB.961 - Arlington Mass Avenue  
645 Massachusetts Avenue  
Arlington, MA 02476

DESIGNER - JM  
CREATED - 06.17.21  
DRAWING - B95122



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ARCHITECTURAL RENDERING - CORNER ELEVATION



**CHS.NB.961 - Arlington Mass Avenue**  
645 Massachusetts Avenue  
Arlington, MA 02476

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**DESIGNER** - JM  
**CREATED** - 06.17.21  
**DRAWING** - B95122



**PHILADELPHIASIGN**  
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ARCHITECTURAL ELEVATION - MASSACHUSETTS AVENUE



**CHS.NB.961 - Arlington Mass Avenue**  
645 Massachusetts Avenue  
Arlington, MA 02476

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**DESIGNER** - JM  
**CREATED** - 06.17.21  
**DRAWING** - B95122



**PHILADELPHIASIGN**

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ARCHITECTURAL ELEVATION - DAVID LAMSON WAY ELEVATION



**CHS.NB.961 - Arlington Mass Avenue**  
645 Massachusetts Avenue  
Arlington, MA 02476

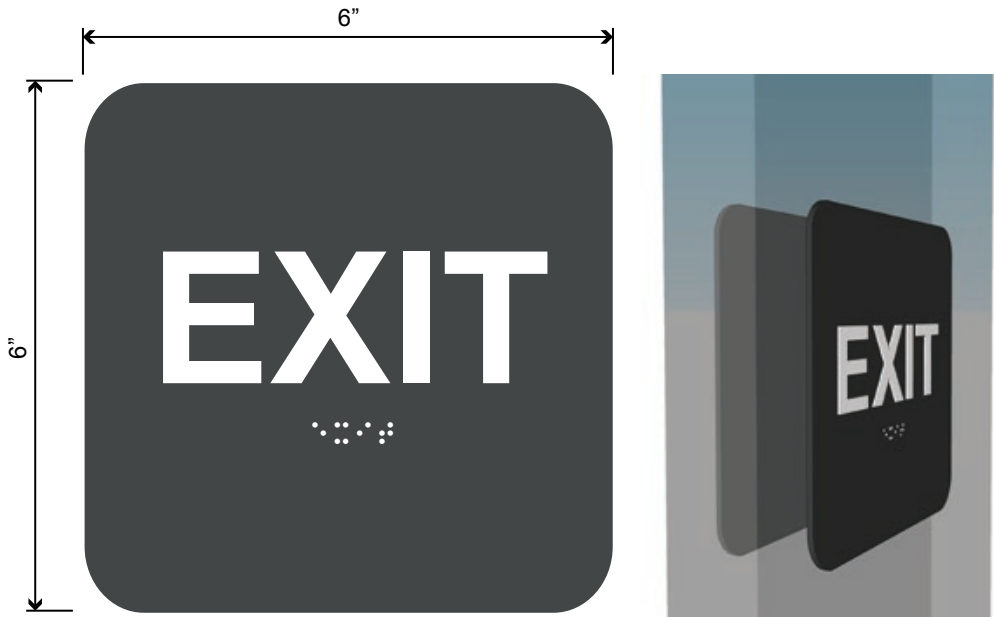
36 of 347

**DESIGNER** - JM  
**CREATED** - 06.17.21  
**DRAWING** - B95122

 **PHILADELPHIASIGN**  
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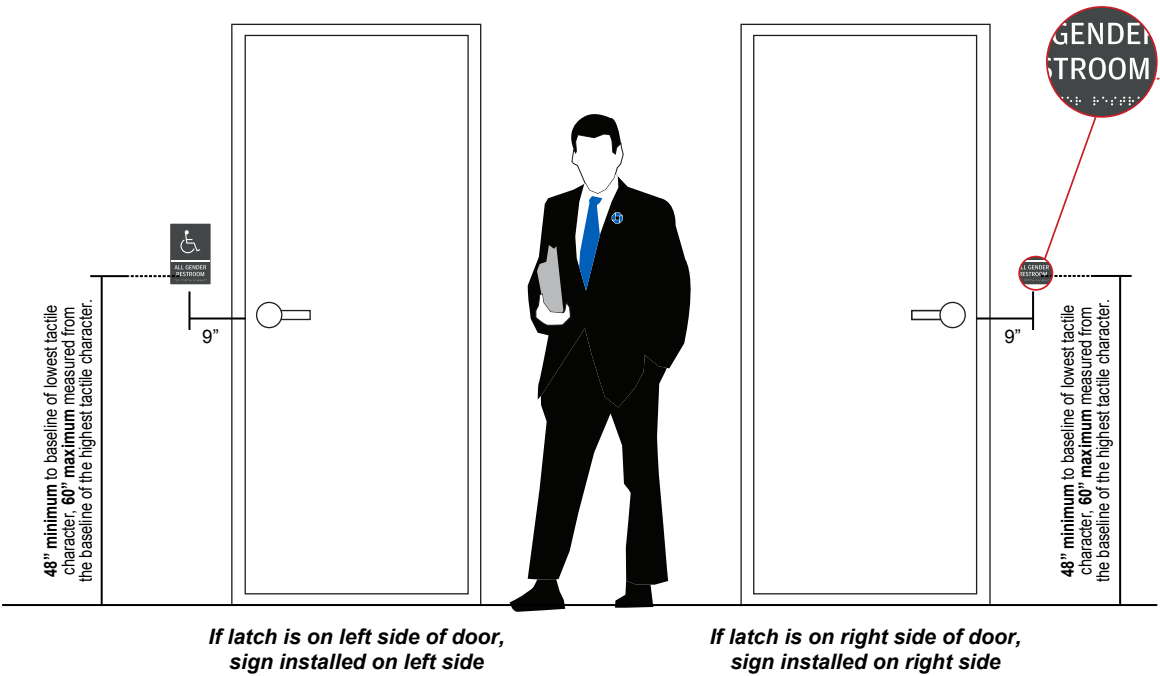
**ADA-EX**  
**HANDICAPPED EXIT PLAQUE**  
SCALE: NTS

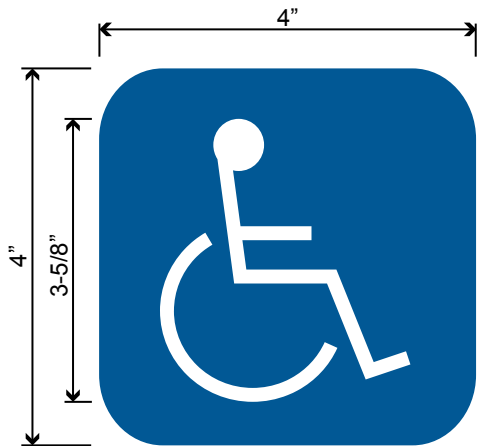
**SIGN I01 MOUNTS  
BACK-TO-BACK WITH  
SIGN E10.**

**CUST-VIN**  
**MATCHING BRONZE VINYL BACKER**  
SCALE: NTS

**ADA-RRAG-A-G** All Gender Neutral Restroom Signage for Restrooms that **ARE** Accessible

- Tactile sign identifying an accessible restroom entrance.
- Mounted on the wall, next to the door, on the latch side of the door.
- Acrylic tactile signs designed to meet Federal ADA 2010 ADAAG standards.

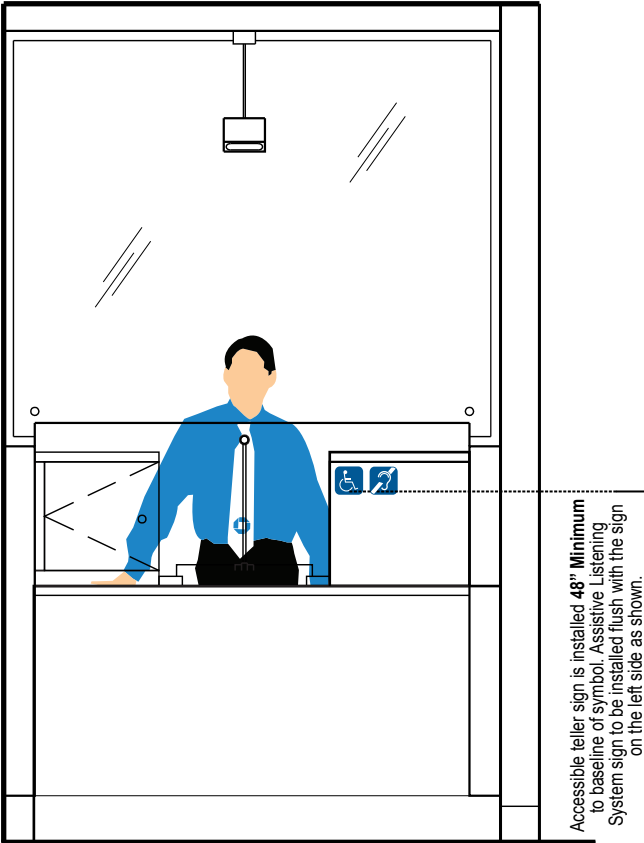




**ADA-TW**  
**ADA TELLER WALL SIGN**  
SCALE: NTS



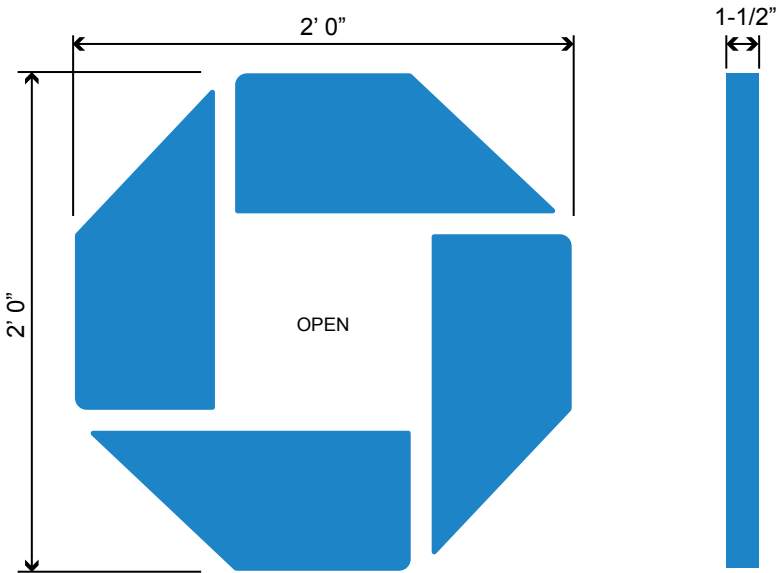
**ADA-TW-ALS**  
**ADA TELLER WALL SIGN - ASSISTIVE LISTENING SYSTEM**  
SCALE: NTS



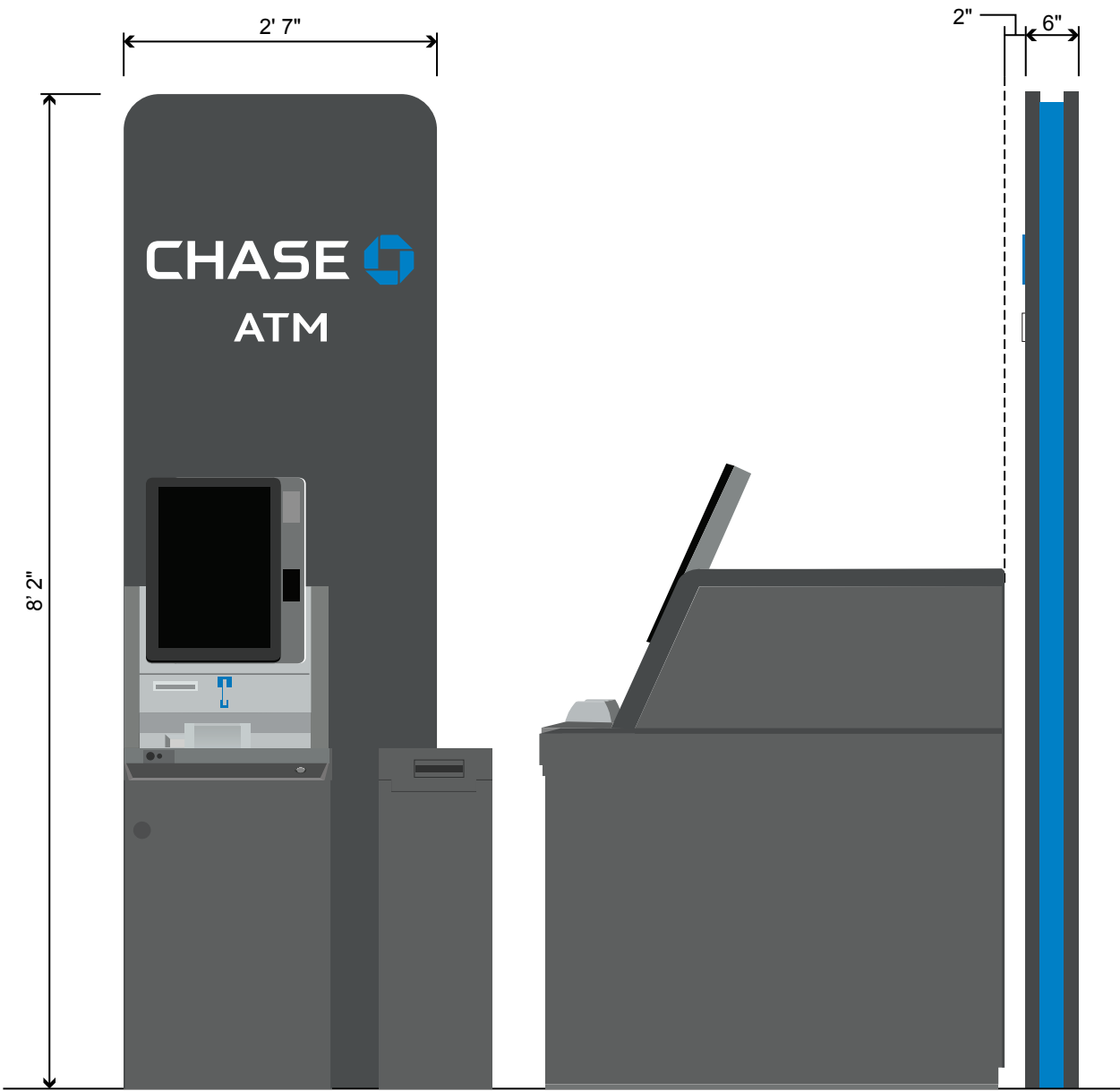
**Placement at Modular Teller Stations  
with Bullet-Resistant Glass**



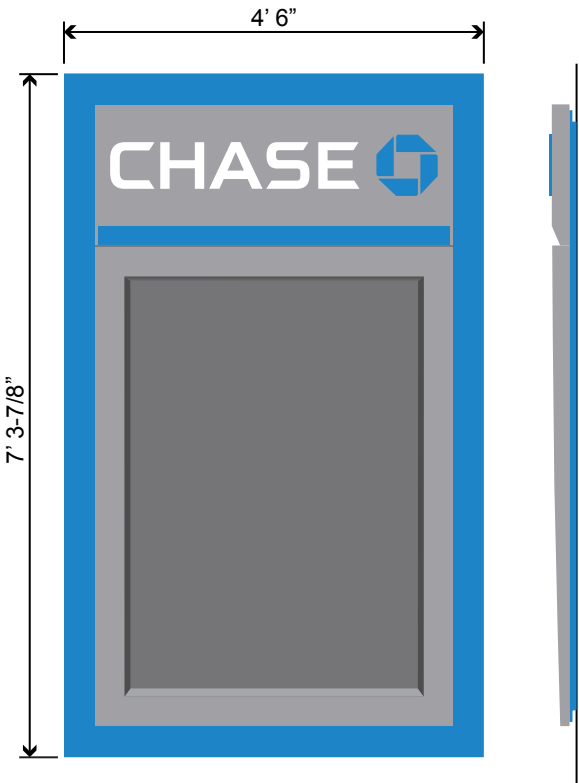
For Reference Only



**TPL-BTR-B-24**  
**24" THIN PROFILE ILLUMINATED INTERIOR BLUE OCTAGON - 4SF**  
SCALE: NTS  
NOTE: Octagon to be Purchased from Bitro.



**eATM-PANEL-SINGLE-SIDE-CAR**  
**BRANDING PANEL FOR SINGLE SIDE CARE eATM 3.0**  
SCALE: NTS



**SUR-TTW-U-4-TP**  
**THIN PROFILE ATM SURROUND - 33SF**  
SCALE: NTS



CHS.NB.961 - Arlington Mass Avenue  
645 Massachusetts Avenue  
Arlington, MA 02476

DESIGNER - JM  
CREATED - 06.17.21  
DRAWING - B95122



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Survey Photos



CHS.NB.961 - Arlington Mass Avenue  
645 Massachusetts Avenue  
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Approach Photos



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DESIGNER - JM  
CREATED - 06.17.21  
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Code Allowances - Completed by Sign Vendor	
General Info	
Zoning Designation	B-5, Central Business
Temporary Signs	
Is the 8' x 4' Coming Soon Ground Sign Allowed? If so, how many?	Downtown location not suitable for F/S signs
Are temporary banners allowed? If so, for how long?	n/a
Are fly guys allowed? If so, for how long?	n/a
Primary Ground Sign	
Will code allow our standard pylon or monument? If yes, which one (ex: P-100, P-75, M-50, M-25, refer to reference guide for all standard options). Is more than 1 primary ground sign allowed?	Downtown location not suitable for F/S signs
If not, what are the pylon / monument restrictions? (include illumination restrictions/overall allowable sf / custom design requirements)	n/a
List the set back requirements.	n/a
Building Sign	
Prototypical FS branch - Will code allow our standard illuminated bldg. sign package (30" letterset on front & sides, 24" letterset on the rear)? Please list size and locations of signs allowed.	1 sign per street, no SF limitations; no flashing/blinking
If not, what are the variables/restrictions (include illumination and sf restrictions, as well as custom sign requirements)?	n/a
In-line / Urban branch - what are the exterior wall sign restrictions (sign type, including max. sf and illumination).	n/a
Interior Window Signs- List all interior window signs restrictions, including storefront set back requirements.	25% window area
ATM Topper- does this count against our overall allowable sf.? Is illumination allowed?	ATM signs subject to review and approval
Are decorative logos allowed (EFIS octagon)? Does it count against overall SF?	n/a
Directional / Regulatory Signs	
Is our standard directional and regulatory sign package allowed?	Downtown location not suitable for F/S signs
If not, what are the variables/restrictions?	n/a
Awnings / ATM Sunscreens	
Are branded awnings allowed?	yes
What if any restrictions are there (Illumination, color/materials, min & max projection)?	8' clearance, no SF limitation, see add'l comments
Are ATM sunscreens allowed? Do they count against overall SF?	ATM signs subject to review and approval
Other Governing Agencies	
Identify other governing agencies that could override code (ARB, HRB, PUD, etc) and list the known restrictions.	ALL SIGNS TO BE REVIEWD AND APPROVED BY the ARLINGTON HISTORICAL COMMISSION
Permitting / Variance Process	
What is the application process and timing for variance approval ? What are the variance application fees?	2-3 months
What is the likelihood of being granted a variance with this municipality?	25%
Architectural Lighting	
Is Architectural lighting allowed? Does it count against overall SF? List provisions.	no flashing/blinking. NO SIGNS TO BE ILLUMINATED FROM 12am - 6am
Additional Comments	
Please list any additional comments	Permitted by the following categories of signs: wall sign, window sign, awning sign

From: Sarah Tuttle <s\_g\_tuttle@yahoo.com>  
To: "jraitt@town.arlington.ma.us" <jraitt@town.arlington.ma.us>  
Date: Sun, 12 Sep 2021 00:17:51 +0000 (UTC)  
Subject: Comment on Proposed Chase Branch in Arlington Center

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To Whom It May Concern on the Redevelopment Board,

I am writing as a resident of Arlington regarding the proposed branch of Chase Bank where Not Your Average Joe's used to be on MA Ave in Arlington Center. I sincerely hope that the location does not become a bank of any sort. To be frank-- in Arlington we have so many banks. I am very, very tired of seeing so many store fronts taken up with financial institutions. The location of the property is one of the best in Arlington-- right across from the library, in the heart of the center, with a huge space. It should be used for something that the community can engage with-- not yet another bank. There is already a large Citizen's Bank on one side of the block, and a large Bank of America property on the other side. Yet another bank in such a prime real estate location would be a waste of an opportunity to invite a business with real value to residents. We need restaurants, shops, and other businesses that invite folks to linger in the center. Businesses that the community wants to patronize. Locations that are fun to browse, or comfortable to gather. We don't need yet another reason to drive by our most accessible, valuable commercial real estate.

Thank you for taking these comments into consideration.

Sincerely,  
Sarah Tuttle

Resident-- Grove St. Arlington

**From:** "Matthew C. Guyton" <mcguyton@mit.edu>  
**Date:** September 14, 2021 at 1:51:43 PM EDT  
**To:** JRaitt@town.arlington.ma.us, ebenson@town.arlington.ma.us, KLau@town.arlington.ma.us, mtintocalis@town.arlington.ma.us, rzsembery@town.arlington.ma.us  
**Cc:** srevilak@town.arlington.ma.us  
**Subject:** Chase Bank - please deny

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Dear Redevelopment Board,

I am writing to ask you to deny the special permit for Chase Bank to replace Not Your Average Joe's.

As a frequent pedestrian in Arlington Center (especially around mealtimes), Not Your Average Joe's was what I consider an "anchor restaurant". You could walk out to the food destination of Arlington Center with the intention of patronizing one of the smaller restaurants, and if those restaurants were too busy, your backup plan could be to eat at Not Your Average Joe's, and vice versa. In this manner, the large restaurant helped drive business for the smaller restaurants. For this reason, I expect that if a restaurant does not soon replace NYAJ's, then Pasha and Thai Moon will soon go out of business. I realize that denying Chase's special permit won't bring NYAJ's back, but it will give up the opportunity for a new restaurant to open in this location before giving it a chance (and before the pandemic ends).

Frequently on our walks around the Center, my wife and I would complain about the prevalence of banks and how they ruin the walkable character of the town. The corner of Mass Ave and Pleasant has a plethora of financial institutions (listed going counterclockwise): Cambridge Bank, Coldwell Banker (not a bank, but sounds like one), Chase Bank requesting to go where NYAJ used to be, and Bank of America - leaving no good reason for pedestrians to walk to that part of the Center. Walking West up Mass Ave, the Citizen's Bank drive-thru exit creates a good place to get hit by a car, which makes me cautious when out with my young son. Walking further west up Mass Ave, the cluster of Brookline Bank, Leader Bank's lending center, and TD Bank creates another dead zone for pedestrians (made even worse by Lender Bank's large empty parking lot and crumbling brick planters that have sat unfixed for months). Keeping the Center a walkable area for pedestrians is important for businesses, because all customers are pedestrians. Banks violate this goal and are a poor use of space in the center of town.

Seeing a physical bank is like seeing a piece of history (and not in the good historical way that we strive for in Arlington). Does anyone still use physical bank locations? I have bought a house, refinanced it many times, and bought a car, all without ever visiting a physical bank location. I can't remember the last time I've physically been to a bank, and the last time I went, I was just there for a lollipop. I don't see how these locations are profitable for the banks, except that they serve as expensive billboards which would otherwise not be allowed in Arlington.

45 of 347

Banks are ghost towns in the evening and make the area less walkable. I believe you should be trying to drive most of the banks out of the Center, not letting more banks in. If you let Chase bank replace NYAJ's, then you have essentially given up on having a vibrant welcoming town center in Arlington. (For the record, I'm not opposed to having Chase bank or any other bank in town - I just don't want to see them located at the main corner of Arlington Center).

-Matt Guyton

22 Irving St



**TOWN OF ARLINGTON**  
DEPARTMENT OF PLANNING and  
COMMUNITY DEVELOPMENT

TOWN HALL, 730 MASSACHUSETTS AVENUE  
ARLINGTON, MASSACHUSETTS 02476  
TELEPHONE 781-316-3090

**MEMORANDUM**

To: Arlington Redevelopment Board  
From: Jennifer Raitt, Director, Department of Planning and Community Development/kl/ac  
Date: September 22, 2021  
RE: Linear Feet of Commercial Banks in Arlington Center

During the Redevelopment Board's August 30, 2021 hearing on Docket #3665 to establish a Chase Bank location within the storefront at 645 Massachusetts Avenue, staff were asked to calculate the total linear feet of banks within Arlington Center.

For economic development purposes, the Town defines the Arlington Center as 375 to 1056 Massachusetts Avenue; 283 to 327 Broadway; Medford, Mill, Mystic Streets; Schouler Court; Summer Street. Within these limits are a total of 264 commercial units<sup>1</sup>.

There are a total of nine banks or ATMs in Arlington Center, or 3.4% of total commercial units. The table below provides the bank name, type, address, parcel frontage, and building width. Parcels on corner lots are indicated with an asterisk; for corner lots, the total parcel and building frontage along both streets are listed first, followed by the frontage specific to Massachusetts Avenue in parentheses.

Bank name	Type	Address	Parcel frontage (ft)	Street-facing building width (ft)
Chase Bank	ATM	323 Broadway	n/a <sup>2</sup>	23
Leader Bank	Bank Branch	449 Mass Ave*	n/a <sup>3</sup>	70 (26)
Cambridge Savings Bank	Bank Branch and Drive-up	626 Mass Ave*	285 (115)	187 (133)
Bank of America	Bank Branch	655 Mass Ave	51	48
Citizens Bank	Bank Branch and Drive-up	699 Mass Ave*	278 (151)	139 (82)
Brookline Bank	Bank Branch and Drive-up	856 Mass Ave*	204 (91)	135 (67)
Leader Bank	Residential Lending	864 Mass Ave*	218 (107)	154 (103)
TD Bank North	Bank Branch and Drive-up	880 Mass Ave*	408 (179)	182 (80)
Watertown Savings Bank	Bank Branch and Drive-up	980 Mass Ave*	268 (110)	155 (69)
<b>TOTAL</b>			<b>1,712 (804)</b>	<b>1,093 (631)</b>
* Corner lot				

Linear calculations have been estimated using the Town's GIS data. Should precise numbers be required, individual site plans would need to be requested from each property.

<sup>1</sup> According to Arlington Town Assessor Data. This figure does not include the 180 individual rental units and commercial condos within commercial properties like 22 Mill, 29 Mill, etc.

<sup>2</sup> Tenant in a structure with multiple units on one lot

<sup>3</sup> Tenant in a structure with multiple units on one lot

**From:** "Jenny Raitt" <JRaitt@town.arlington.ma.us>  
**To:** "Mary Muszynski" <MMuszynski@town.arlington.ma.us>  
**Cc:** "Kelly Lynema" <KLynema@town.arlington.ma.us>  
**Date:** 09/07/2021 11:18 AM  
**Subject:** Fwd: Chase Bank - 645 Massachusetts Avenue, Arlington MA -Special permit

---

Please post to the ARB page.

Jennifer Raitt  
Director, Department of Planning and Community Development  
Town of Arlington

\*Arlington values equity, diversity, and inclusion. We are committed to building a community where everyone is heard, respected, and protected.\*

From: Ian Heanue <iheanue@peconsultingcorp.com>  
To: Jenny Raitt <JRaitt@town.arlington.ma.us>, Mary Muszynski <MMuszynski@town.arlington.ma.us>, Kelly Lynema <KLynema@town.arlington.ma.us>  
Cc: Marc Sides <msides@core-states.com>, Kaitlyn Flynn <kflynn@core-states.com>  
Date: Tue, 7 Sep 2021 14:42:38 +0000  
Subject: Chase Bank - 645 Massachusetts Avenue, Arlington MA -Special permit

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Good Morning,

Please accept this email as an official request to have our special permit application (Docket #3665) for JPMorgan Chase Bank, at 645 Massachusetts Avenue, Arlington MA, continued to the next board meeting schedule on 9/27/2021.

We will work to have all updated documents submitted to your office by 9/22/2021 in order to meet the deadline for the 9/27 meeting.

If you have any questions or require any additional documentation please feel free to reach out.

Thank You,

*Ian Heanue*



**Project Expeditors Consulting Corp.,** WBE, WBENC Certified

Project Expeditors Inc, WBE, WBENC Certified

Faneuil Hall Marketplace

4 South Market Street

Suite S-4035, Third Floor

Boston, MA 02109

Office: 617-227-0159

Cell: 908-907-0484

Fax: 617-227-0501





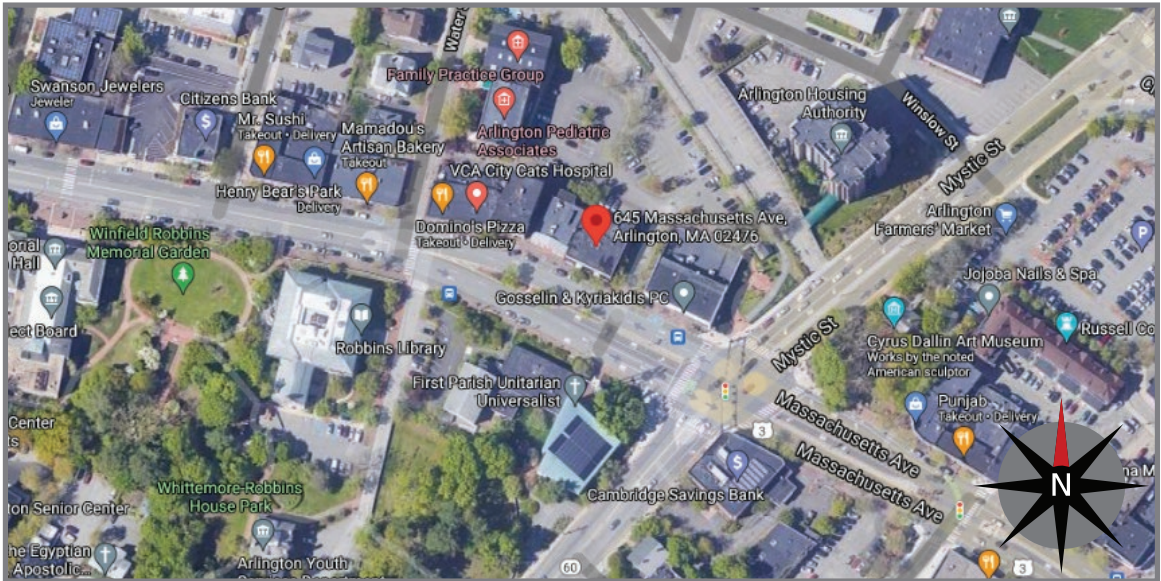
ARLINGTON MASS AVENUE

645 Massachusetts Avenue  
Arlington, MA 02476



**REVISION NOTES:**

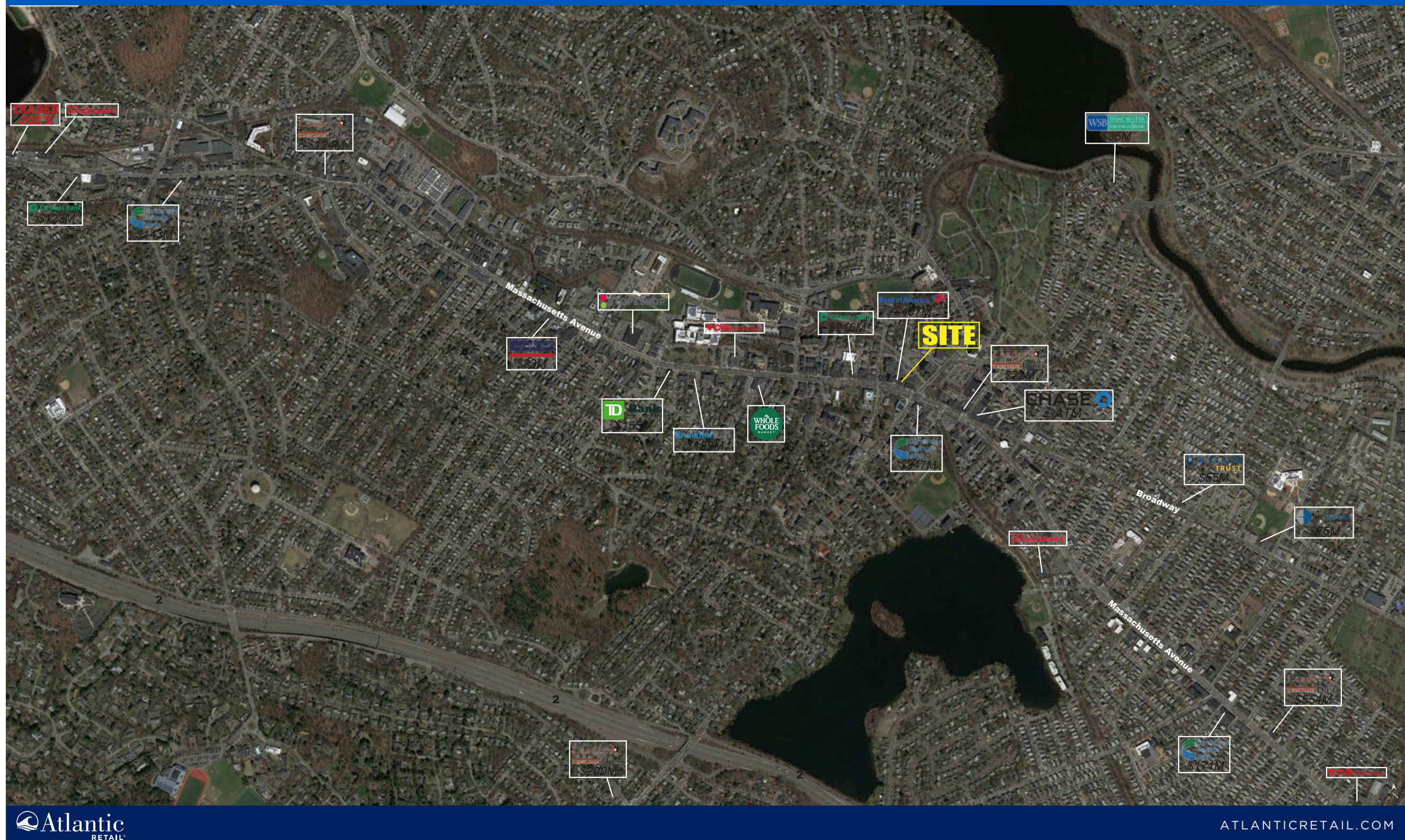
06.23.21	JM	Delete Octagons from All Awnings. Add Option 2, E12 and E13
07.01.21	RJW	Revised as noted.
09.09.21	RJW	Delete E03, add lettersets & elevations.



B95122



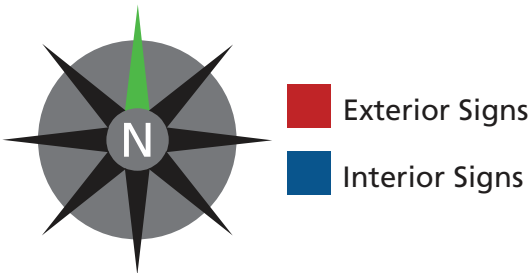
## ARLINGTON, MA





Exterior Scope of Work			
E01	LIF-WBO-18-LED	WHITE W/ BLUE OCTAGON ILLUM CHANNEL LETTERS	20.7SF
E02	NMP-XX	DARK NICKEL METAL PANEL	
E04	NMP-XX	DARK NICKEL METAL PANEL	
E05	LIF-WBO-14-LED	WHITE W/ BLUE OCTAGON ILLUM CHANNEL LETTERS	12.7SF
E06	NMP-XX	DARK NICKEL METAL PANEL	
E07	TC-W-A-RE	WALL MTD REGULATORY SIGN - BANK PATRONS ONLY (Re-ENGINEERED)	2SF
E08	TC-W-A-RE	WALL MTD REGULATORY SIGN - BANK PATRONS ONLY (Re-ENGINEERED)	2SF
E09	TC-W-A-RE	WALL MTD REGULATORY SIGN - BANK PATRONS ONLY (Re-ENGINEERED)	2SF
E12	LIF-WBO-18-LED	WHITE W/ BLUE OCTAGON ILLUM CHANNEL LETTERS	20.7SF

ALL SIGNS TO BE  
REVIEWED/APPROVED  
BY HRC



CHS.NB.961 - Arlington Mass Avenue

645 Massachusetts Avenue  
Arlington, MA 02476

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DESIGNER - JM  
CREATED - 06.17.21  
DRAWING - B95122



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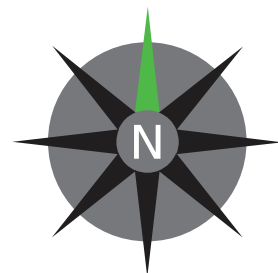
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## Floor Plan

## Interior Scope of Work

I01	ADA-EX	ADA HANDICAPPED EXIT PLAQUE	.25SF
I01.1	CUST-VIN	MATCHING BRONZE VINYL BACKER	
I02	ADA-EX	ADA HANDICAPPED EXIT PLAQUE	.25SF
I02.1	CUST-VIN	MATCHING BRONZE VINYL BACKER	
I03	ADA-RRAG-A-G	ADA ALL GENDER RESTROOM SIGN - ACCESSIBLE	1.4SF
I04	ADA-RRAG-A-G	ADA ALL GENDER RESTROOM SIGN - ACCESSIBLE	1.4SF
I05	ADA-TW	ADA TELLER WALL SIGN	.1SF
I06	ADA-TW-ALS	ADA TELLER WINDOW - ASSISTIVE LISTENING SYSTEM	.1SF
I07	TPL-BTR-B-24	24" THIN PROFILE ILLUMINATED INTERIOR BLUE OCTAGON	4SF
I08	eATM-PANEL-SINGLE-SIDE-CAR	BRANDING PANEL FOR SINGLE SIDE CAR eATM 3.0	
I09	SUR-TTW-U-4-TP	ILLUMINATED THIN PROFILE ATM SURROUND	33SF
E10	ADA-EP	ADA HANDICAPPED ENTRANCE PLAQUE	.25SF
E10.1	CUST-VIN	MATCHING BLUE VINYL BACKER	
E11	ADA-EP-NA	ADA HANDICAPPED ENTRANCE PLAQUE - NOT ACCESSIBLE	.38SF



## Exterior Signs

## Interior Signs



## CHS.NB.961 - Arlington Mass Avenue

645 Massachusetts Avenue  
Arlington, MA 02476

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**DESIGNER - JM**

**CREATED** - 06.17.21

**DRAWING - B95122**

PHILADELPHIA **SIGN**

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E01 - LIF-WBO-18-LED

E02 - NMP-XX

E10 - ADA-EP

E10.1 - CUST-VIN



South Elevation - Massachusetts Ave

SIGN E10 NOT SEEN FROM THIS ANGLE

10' 6-3/8"

CHASE

1' 11-5/8"

3"

4-1/2"

LIF-WBO-18-LED

WHITE W/ BLUE OCTAGON ILLUM CHANNEL LETTERS - 20.7 SF

SCALE: NTS

TBD

3' 0"

NMP-XX

DARK NICKEL METAL PAN - DIMENSIONS TBD

SCALE: NTS

6"

9"

4-1/2"

ADA-EP

HANDICAPPED ENTRANCE PLAQUE

SCALE: NTS

CUST-VIN

MATCHING BLUE VINYL BACKER

SCALE: NTS

SIGN E10 MOUNTS BACK-TO-BACK WITH SIGN I01.



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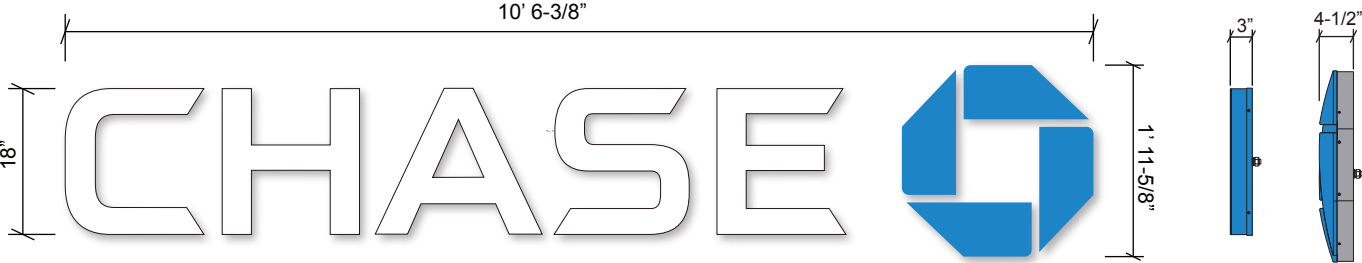
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East Elevation - David Lamson Way



**LIF-WBO-18-LED**  
**WHITE W/ BLUE OCTAGON ILLUM CHANNEL LETTERS - 20.7 SF**  
SCALE: NTS



**NMP-XX**  
**DARK NICKEL METAL PAN - DIMENSIONS TBD**  
SCALE: NTS



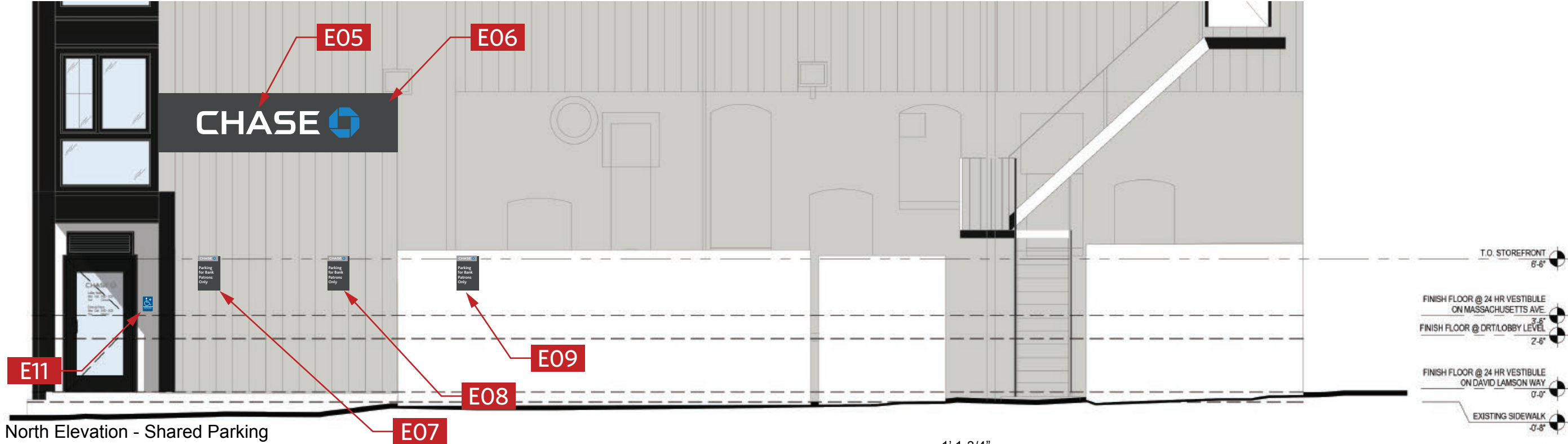
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Arlington, MA 02476

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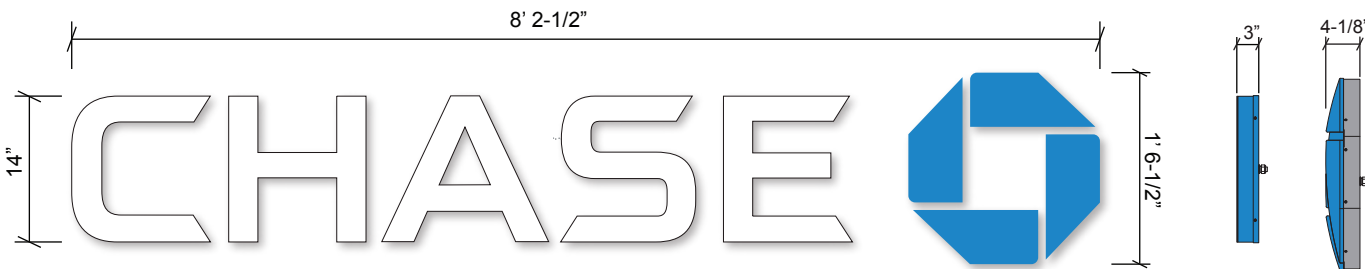
**DESIGNER** - JM  
**CREATED** - 06.17.21  
**DRAWING** - B95122

 **PHILADELPHIASIGN**  
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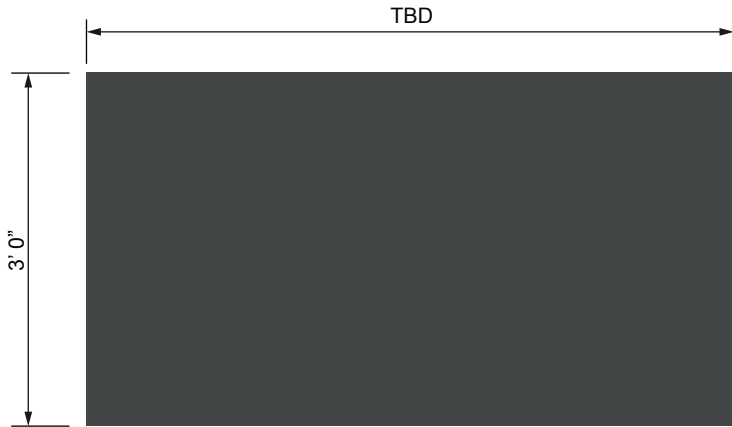
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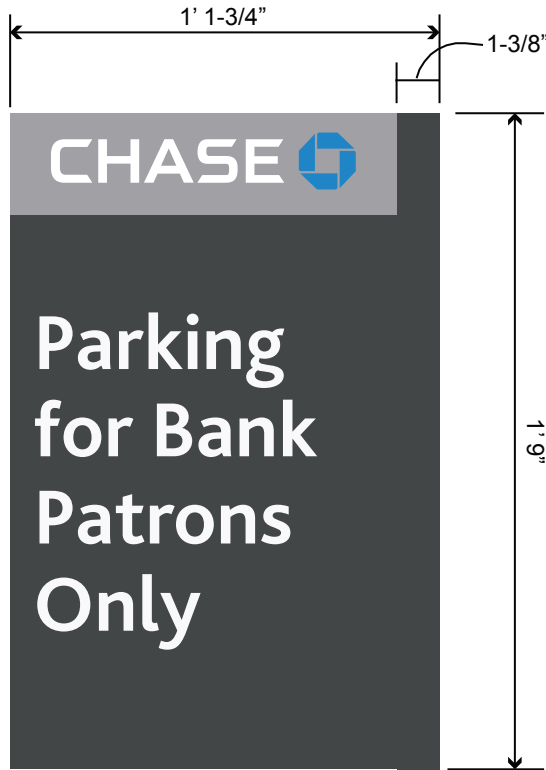
North Elevation - Shared Parking



**LIF-WBO-14-LED**  
**WHITE W/ BLUE OCTAGON ILLUM CHANNEL LETTERS - 12.7 SF**  
SCALE: NTS



**NMP-XX**  
**DARK NICKEL METAL PAN - DIMENSIONS TBD**  
SCALE: NTS



**TC-W-A-RE**  
**WALL MOUNTED REGULATORY SIGN (RE-ENGINEERED) - 2SF**  
SCALE: NTS



**ADA-EP-NA**  
**HANDICAPPED ENTRANCE PLAQUE - NOT ACCESSIBLE**  
SCALE: NTS



CHS.NB.961 - Arlington Mass Avenue  
645 Massachusetts Avenue  
Arlington, MA 02476

DESIGNER - JM  
CREATED - 06.17.21  
DRAWING - B95122

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ARCHITECTURAL RENDERING - CORNER ELEVATION



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645 Massachusetts Avenue  
Arlington, MA 02476

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**DESIGNER** - JM  
**CREATED** - 06.17.21  
**DRAWING** - B95122



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ARCHITECTURAL ELEVATION - MASSACHUSETTS AVENUE



**CHS.NB.961 - Arlington Mass Avenue**  
645 Massachusetts Avenue  
Arlington, MA 02476

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**DESIGNER** - JM  
**CREATED** - 06.17.21  
**DRAWING** - B95122



**PHILADELPHIASIGN**

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ARCHITECTURAL ELEVATION - DAVID LAMSON WAY ELEVATION



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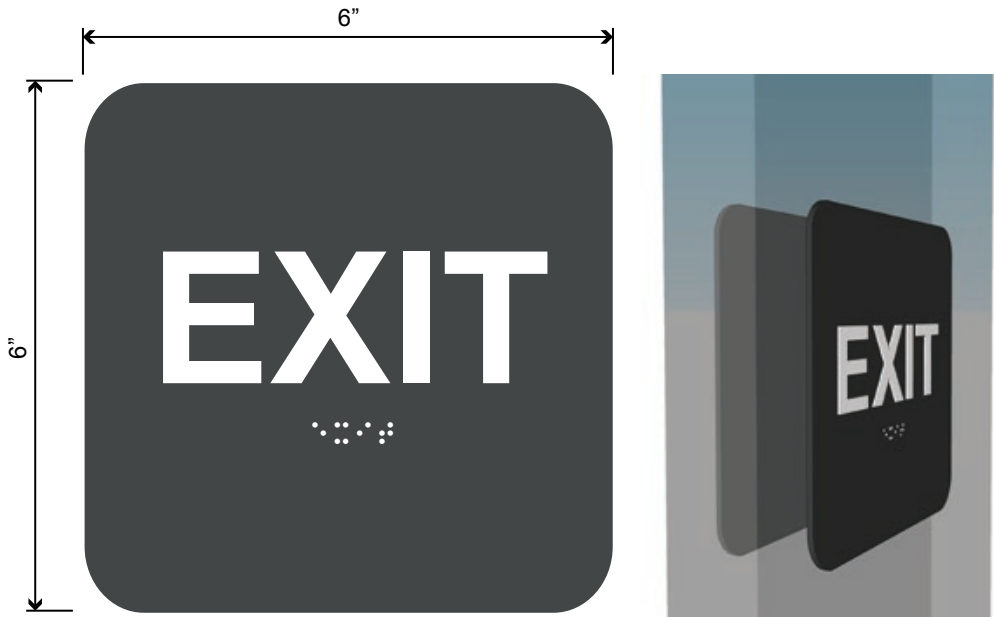
60 of 347

**DESIGNER** - JM  
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**ADA-EX**  
**HANDICAPPED EXIT PLAQUE**  
SCALE: NTS

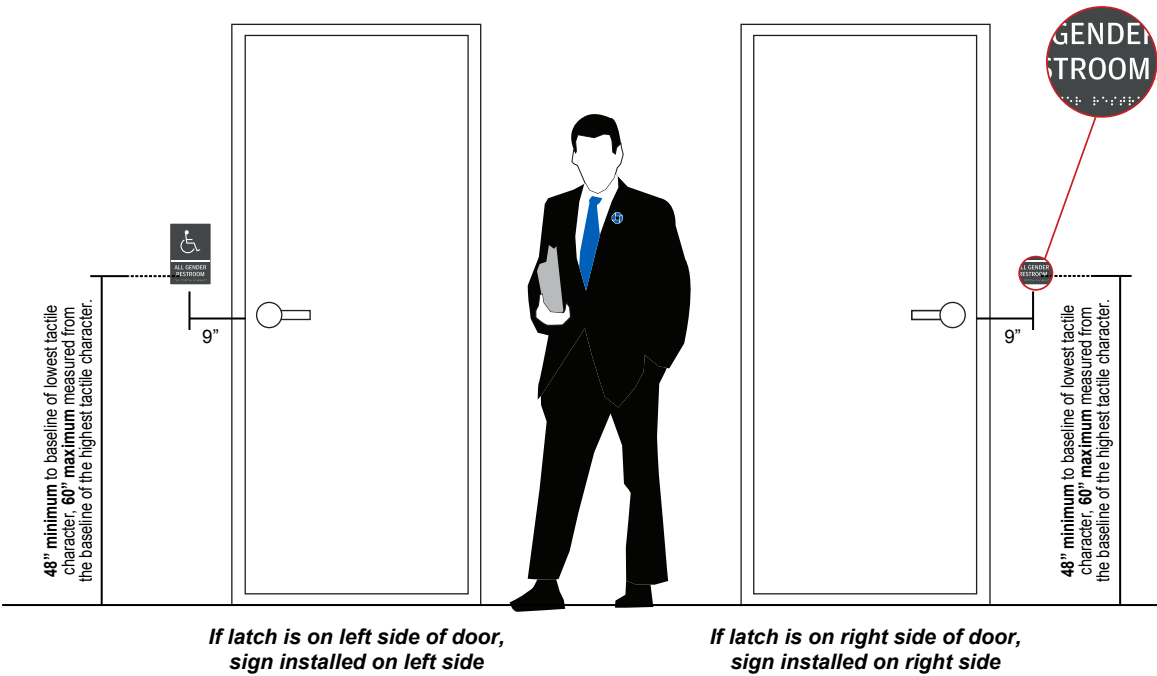
**SIGN I01 MOUNTS  
BACK-TO-BACK WITH  
SIGN E10.**

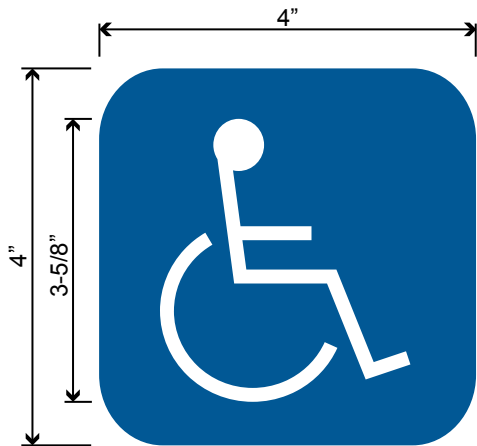
**CUST-VIN**  
**MATCHING BRONZE VINYL BACKER**  
SCALE: NTS



**ADA-RRAG-A-G** All Gender Neutral Restroom Signage  
for Restrooms that **ARE** Accessible

- Tactile sign identifying an accessible restroom entrance.
- Mounted on the wall, next to the door, on the latch side of the door.
- Acrylic tactile signs designed to meet Federal ADA 2010 ADAAG standards.





**ADA-TW**  
**ADA TELLER WALL SIGN**  
SCALE: NTS



**ADA-TW-ALS**  
**ADA TELLER WALL SIGN - ASSISTIVE LISTENING SYSTEM**  
SCALE: NTS

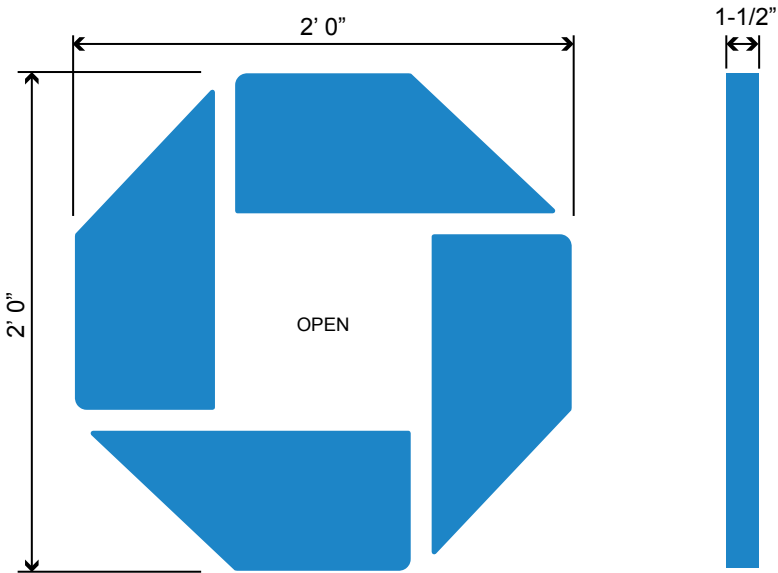


**Placement at Modular Teller Stations  
with Bullet-Resistant Glass**

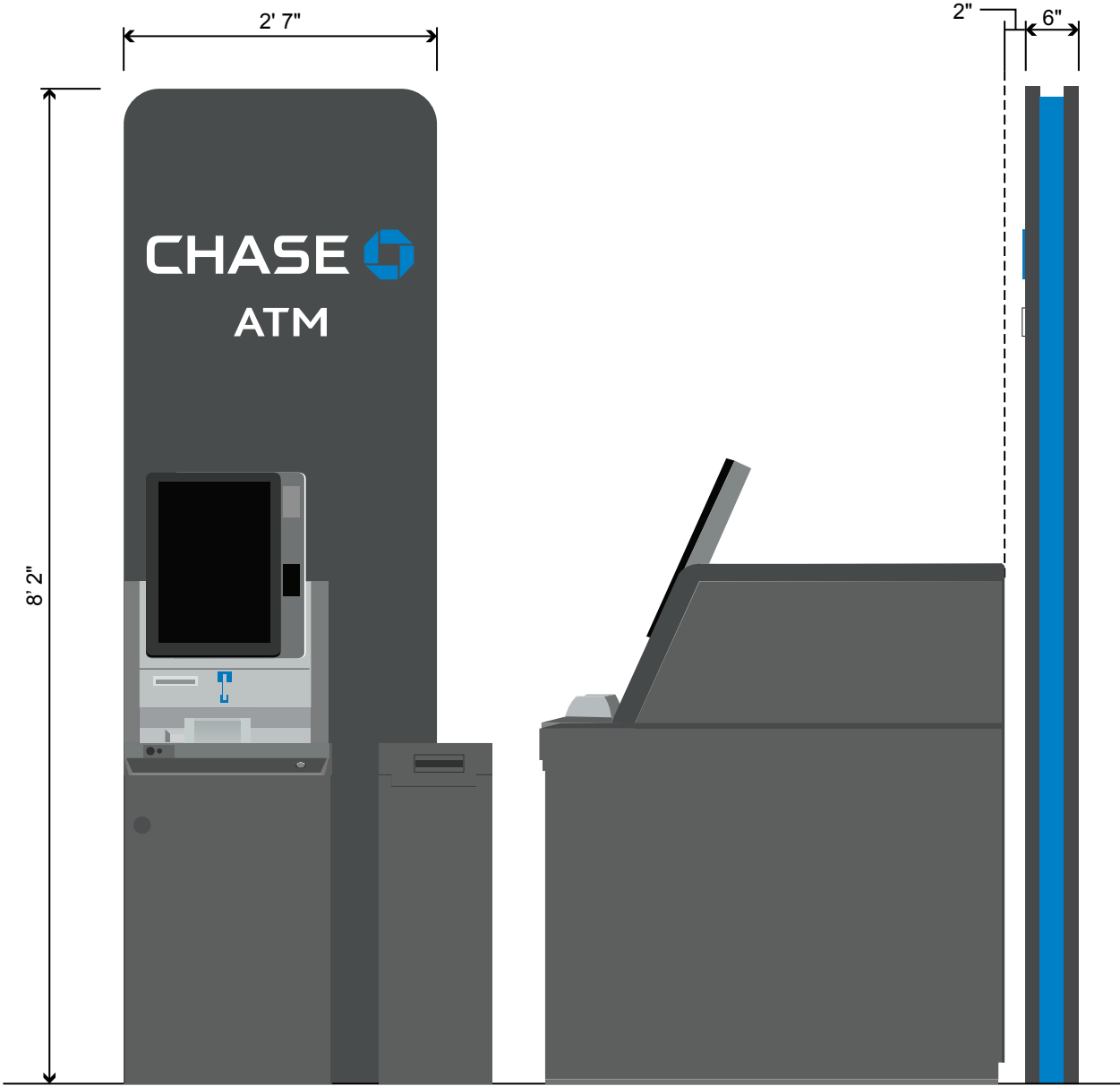




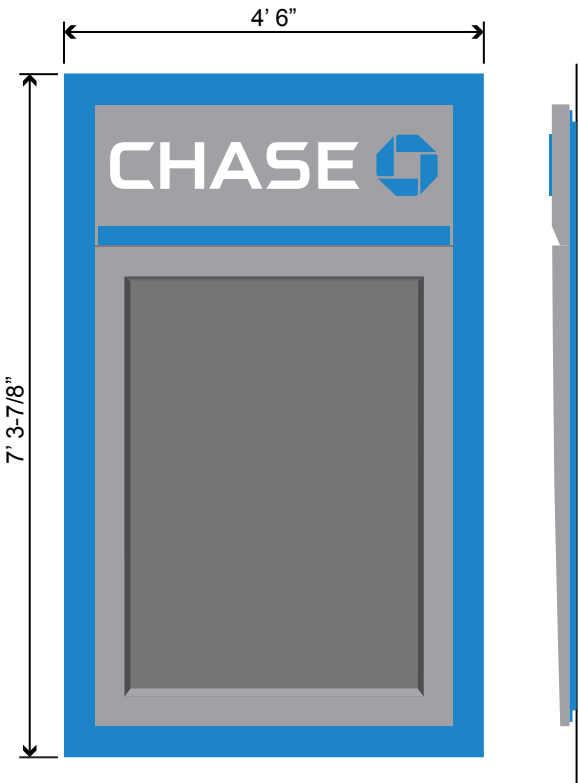
For Reference Only



**TPL-BTR-B-24**  
**24" THIN PROFILE ILLUMINATED INTERIOR BLUE OCTAGON - 4SF**  
SCALE: NTS  
NOTE: Octagon to be Purchased from Bitro.



**eATM-PANEL-SINGLE-SIDE-CAR**  
**BRANDING PANEL FOR SINGLE SIDE CARE eATM 3.0**  
SCALE: NTS



**SUR-TTW-U-4-TP**  
**THIN PROFILE ATM SURROUND - 33SF**  
SCALE: NTS



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Survey Photos



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Approach Photos



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Code Allowances - Completed by Sign Vendor	
General Info	
Zoning Designation	B-5, Central Business
Temporary Signs	
Is the 8' x 4' Coming Soon Ground Sign Allowed? If so, how many?	Downtown location not suitable for F/S signs
Are temporary banners allowed? If so, for how long?	n/a
Are fly guys allowed? If so, for how long?	n/a
Primary Ground Sign	
Will code allow our standard pylon or monument? If yes, which one (ex: P-100, P-75, M-50, M-25, refer to reference guide for all standard options). Is more than 1 primary ground sign allowed?	Downtown location not suitable for F/S signs
If not, what are the pylon / monument restrictions? (include illumination restrictions/overall allowable sf / custom design requirements)	n/a
List the set back requirements.	n/a
Building Sign	
Prototypical FS branch - Will code allow our standard illuminated bldg. sign package (30" letterset on front & sides, 24" letterset on the rear)? Please list size and locations of signs allowed.	1 sign per street, no SF limitations; no flashing/blinking
If not, what are the variables/restrictions (include illumination and sf restrictions, as well as custom sign requirements)?	n/a
In-line / Urban branch - what are the exterior wall sign restrictions (sign type, including max. sf and illumination).	n/a
Interior Window Signs- List all interior window signs restrictions, including storefront set back requirements.	25% window area
ATM Topper- does this count against our overall allowable sf.? Is illumination allowed?	ATM signs subject to review and approval
Are decorative logos allowed (EFIS octagon)? Does it count against overall SF?	n/a
Directional / Regulatory Signs	
Is our standard directional and regulatory sign package allowed?	Downtown location not suitable for F/S signs
If not, what are the variables/restrictions?	n/a
Awnings / ATM Sunscreens	
Are branded awnings allowed?	yes
What if any restrictions are there (Illumination, color/materials, min & max projection)?	8' clearance, no SF limitation, see add'l comments
Are ATM sunscreens allowed? Do they count against overall SF?	ATM signs subject to review and approval
Other Governing Agencies	
Identify other governing agencies that could override code (ARB, HRB, PUD, etc) and list the known restrictions.	ALL SIGNS TO BE REVIEWD AND APPROVED BY the ARLINGTON HISTORICAL COMMISSION
Permitting / Variance Process	
What is the application process and timing for variance approval ?	2-3 months
What are the variance application fees?	
What is the likelihood of being granted a variance with this municipality?	25%
Architectural Lighting	
Is Architectural lighting allowed? Does it count against overall SF? List provisions.	no flashing/blinking. NO SIGNS TO BE ILLUMINATED FROM 12am - 6am
Additional Comments	
Please list any additional comments	Permitted in the following categories of signs: billboard, wall sign, window sign, awning sign





September 18, 2021

Town of Arlington  
Redevelopment Board  
730 Mass Ave. Annex  
Arlington, MA 02476

Re: Application for Special Permit Review, 645 Massachusetts Avenue – Second Review

The whom it may concern;

We would like to thank the board for their time during the August 30, 2020 Arlington Redevelopment Board meeting. The input that was provided during this meeting was taken into consideration in preparation for our second appearance on the topic of the special permits requested for this site.

To respond to some of the comments issued, we offer the following, in conjunction with the attached revised documents:

**Automobile Parking Relief:** It appeared, from the response from the board, that they were in favor of granting this relief, on the grounds of the large municipal lot behind the proposed site and the presence of on-street parking in front. NO RESPONSE REQUIRED

**Bicycle Parking Relief:** Per the board, no relief would be granted on the bicycle parking requirement. THE SUBMISSION MATERIALS HAVE BEEN REVISED TO REFLECT SHORT TERM BICYCLE PARKING IN FRONT OF THE PROPOSED SITE. LONG TERM PARKING WILL BE INTEGRATED INTO THE PROJECT AS WELL TO MEET THE REQUIREMENT.

**Special Permit for the Installation of a Bank:**

Change of Use: Chase bank is seeking a special permit to allow for a bank use greater than 2,000 sf within the B5 District, as required by the zoning by-laws. Additionally, the previous use was a restaurant, requiring a change of use to the proposed bank (business) use. THE BOARD WAS SPLIT ON THE TOPIC. CHASE BANK REQUESTED A CONTINUATION IN ORDER TO ASSEMBLE ADDITIONAL PRESENTATION MATERIALS AND HAVE REPRESENTATIVES FROM CHASE BANK PRESENT TO DISCUSS THE PROJECT.

Façade: The board recommended changing the existing gridded windows to match the proposed new windows. THE RENDERINGS HAVE BEEN REVISED TO INCORPORATE THIS CHANGE

Sidewalk Amenities: The previously proposed plan reflected the removal of the existing benches and planters. The board recommended retaining this component of the exterior. THE RENDERINGS HAVE BEEN REVISED TO INCORPORATE THIS CHANGE



Accessibility: The board questioned why the vestibule at the rear of the building was not being made accessible and requested studying the addition of a ramp or a wheelchair lift. THERE ARE MULTIPLE EXISTING CONSTRAINTS THAT WILL LIMIT THE ABILITY TO MAKE THE REAR DOOR FULLY ACCESSIBLE, INCLUDING IMPACTS TO THE UPPER-LEVEL TENANT ENTRANCE, EXISTING (MUNICIPAL) PARKING LOT, AND SIDEWALK ALONG DAVID LAMSON WAY. IN SPITE OF THESE CONSTRAINTS, CHASE WILL CONTINUE TO ENDEAVOR TO STUDY THESE CONDITIONS AND EVALUATE THE VIABILITY OF AN ACCESSIBLE REAR ENTRANCE. PLEASE NOTE THAT THERE IS RESERVED ACCESSIBLE STREET PARKING ON MASS AVE NEAR THE FRONT ENTRANCE, WHICH WOULD PROVIDE ACCESS TO THE BANK AND ATM VESTIBULE.

Signage: The board was not in favor of the proposed blade sign. They believed the sign was not in keeping with other signage in the Central Business District as asked if we could consider moving the sign on the gray band along the alley side of the building. THE RENDERINGS HAVE BEEN REVISED TO INCORPORATE THIS CHANGE

We look forward to the opportunity to meet with the board again to review this project. Please let us know if anything additional is required.

Sincerely,

James Lalli  
Director of Architecture - Financial  
908.462.9949 | [jlalli@core-states.com](mailto:jlalli@core-states.com)





South Elevation - Massachusetts Ave

SIGN E10 NOT SEEN FROM THIS ANGLE

10' 6-3/8"

18"

CHASE

1' 11-5/8"

3"

4-1/2"

LIF-WBO-18-LED

WHITE W/ BLUE OCTAGON ILLUM CHANNEL LETTERS - 20.7 SF

SCALE: NTS

TBD

3' 0"

NMP-XX

DARK NICKEL METAL PAN - DIMENSIONS TBD

SCALE: NTS

6"

9"

4-1/2"

ADA-EP

HANDICAPPED ENTRANCE PLAQUE

SCALE: NTS

CUST-VIN

MATCHING BLUE VINYL BACKER

SCALE: NTS

SIGN E10 MOUNTS BACK-TO-BACK WITH SIGN I01.



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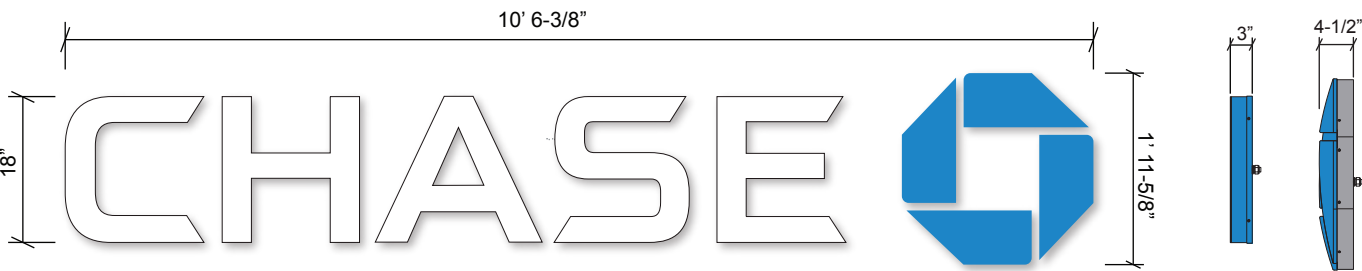
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East Elevation - David Lamson Way



**LIF-WBO-18-LED**  
**WHITE W/ BLUE OCTAGON ILLUM CHANNEL LETTERS - 20.7 SF**  
SCALE: NTS



**NMP-XX**  
**DARK NICKEL METAL PAN - DIMENSIONS TBD**  
SCALE: NTS



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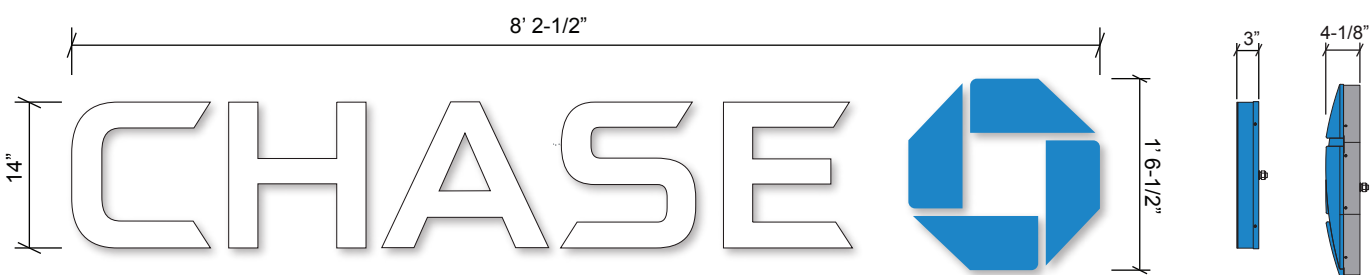
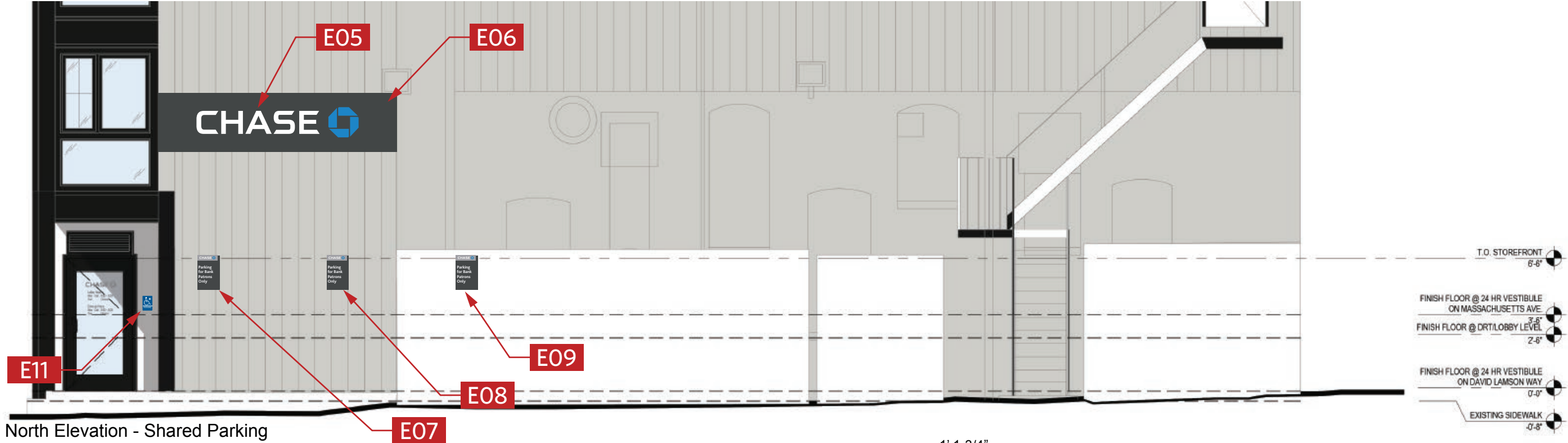
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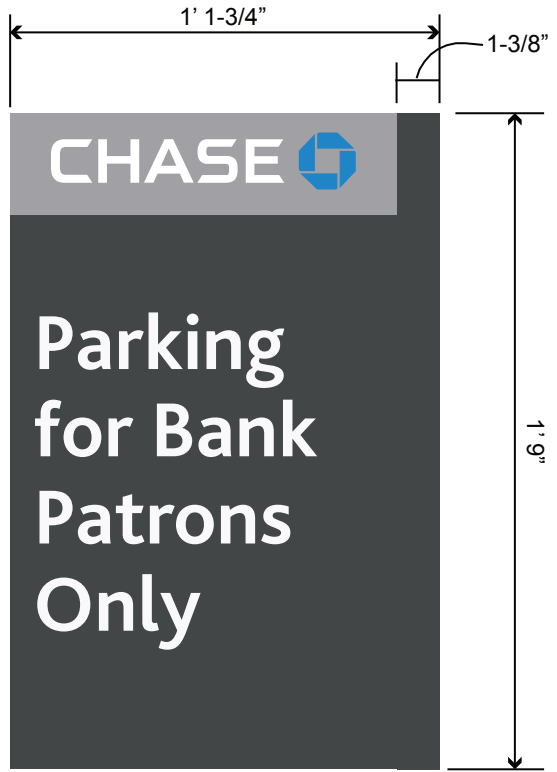




**LIF-WBO-14-LED**  
**WHITE W/ BLUE OCTAGON ILLUM CHANNEL LETTERS - 12.7 SF**  
SCALE: NTS



**NMP-XX**  
**DARK NICKEL METAL PAN - DIMENSIONS TBD**  
SCALE: NTS



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ARCHITECTURAL ELEVATION - MASSACHUSETTS AVENUE



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ARCHITECTURAL ELEVATION - DAVID LAMSON WAY ELEVATION



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TOWN OF ARLINGTON  
REDEVELOPMENT BOARD

Application for Special Permit In Accordance with Environmental Design  
Review Procedures (Section 3.4 of the Zoning Bylaw)

PLANNING & COMMUNITY  
DEVELOPMENT

2021 AUG 11 10 2 18

Docket No. 3665

1. Property Address 645 Massachusetts Avenue, Arlington MA 02476  
Name of Record Owner(s) Key West Realty LLC Phone \_\_\_\_\_  
Address of Owner 63 Trapelo Road, Belmont MA 02478  
Street City, State, Zip
2. Name of Applicant(s) (if different than above) JPMorgan Chase Bank NA  
Address 1111 Polaris Parkway Columbus OH 43240 Phone c/o 617.874.0131  
Status Relative to Property (occupant, purchaser, etc.) Occupant
3. Location of Property 645 Massachusetts Avenue, Arlington MA 02476 05-07-7  
Assessor's Block Plan, Block, Lot No.
4. Deed recorded in the Registry of deeds, Book 50073, Page 393;  
-or- registered in Land Registration Office, Cert. No. \_\_\_\_\_, in Book \_\_\_\_\_, Page \_\_\_\_\_
5. Present Use of Property (include # of dwelling units, if any) Presently a restaurant
6. Proposed Use of Property (include # of dwelling units, if any) Proposed Commercial Bank Branch
7. Permit applied for in accordance with  
the following Zoning Bylaw section(s) 

<u>3.4</u>	<u>Environmental design review</u>
<u>6.1.4</u>	<u>Table of Off-Street Parking Regulations</u>
<u>6.1.12</u>	<u>Bicycle Parking</u>

  
section(s) title(s)
8. Please attach a statement that describes your project and provide any additional information that may aid the ARB in understanding the permits you request. Include any reasons that you feel you should be granted the requested permission.

(In the statement below, strike out the words that do not apply)

The applicant states that \_\_\_\_\_ is the owner -or- occupant -or- purchaser under agreement of the property in Arlington located at 645 Massachusetts Avenue, Arlington MA 02476 which is the subject of this application; and that unfavorable action -or- no unfavorable action has been taken by the Zoning Board of Appeals on a similar application regarding this property within the last two years. The applicant expressly agrees to comply with any and all conditions and qualifications imposed upon this permission, either by the Zoning Bylaw or by the Redevelopment Board, should the permit be granted.

  
Matthew McCool - Vice President  
JPMorgan Chase Bank, N.A.

Signature of Applicant(s)

100 International Dr. Floor 21  
Baltimore, MD, 21202-4673, United States

Address

410-949-2150

Phone



Town of Arlington Redevelopment Board  
Application for Special Permit in accordance with  
Environmental Design Review (Section 3.4)

Required Submittals Checklist

Two full sets of materials and one electronic copy are required. A model may be requested. Review the ARB's Rules and Regulations, which can be found at [arlingtonma.gov/arb](http://arlingtonma.gov/arb), for the full list of required submittals.

- X   Dimensional and Parking Information Form (see attached)
- X   Site plan of proposal
- NA  Model, if required
- X   Drawing of existing conditions
- X   Drawing of proposed structure
- NA  Proposed landscaping. May be incorporated into site plan
- X   Photographs
- X   Impact statement
- X   Application and plans for sign permits
- NA  Stormwater management plan (for stormwater management during construction for projects with new construction)

FOR OFFICE USE ONLY

_____	Special Permit Granted	Date: _____
_____	Received evidence of filing with Registry of Deeds	Date: _____
_____	Notified Building Inspector of Special Permit filing	Date: _____



# TOWN OF ARLINGTON

Dimensional and Parking Information  
for Application to  
The Arlington Redevelopment Board

Docket No. \_\_\_\_\_

Property Location Corner of Mass Ave/David Lamson Way

Zoning District B-5

Owner: Key West Realty LLC

Address: 645 Massachusetts Ave

Present Use/Occupancy: No. of Dwelling Units:

Restaurant/Bar

Uses and their gross square feet:

6400 sf

Proposed Use/Occupancy: No. of Dwelling Units:

Financial Center > 2000sf

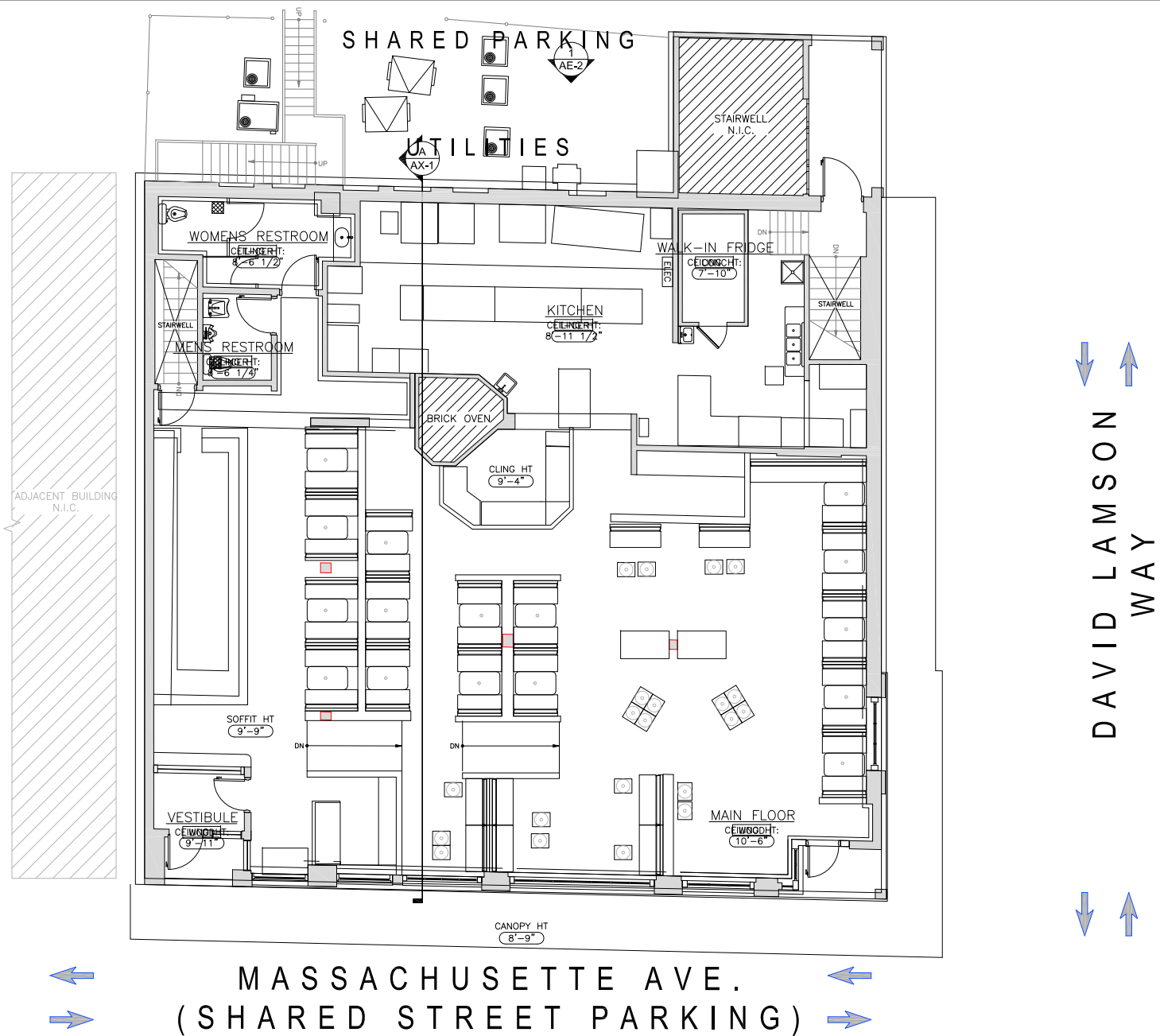
Uses and their gross square feet:

3826 sf (first floor only)

	Present Conditions	Proposed Conditions	Min. or Max. Required by Zoning for Proposed Use
Lot Size	6400sf	3826sf	min. >2,000 sf (with special permit)
Frontage	63'-5"	63'-5"	min.
Floor Area Ratio			max.
Lot Coverage (%), where applicable	N/A	N/A	max.
Lot Area per Dwelling Unit (square feet)	N/A	N/A	min.
Front Yard Depth (feet)	N/A	N/A	min.
Side Yard Width (feet) right side	N/A	N/A	min.
left side	N/A	N/A	min.
Rear Yard Depth (feet)	N/A	N/A	min.
Height			min.
Stories	2	2	stories Existing
Feet	31'-6"	31'-6"	feet Existing
Open Space (% of G.F.A.)			min.
Landscaped (square feet)	N/A	N/A	(s.f.)
Usable (square feet)	N/A	N/A	(s.f.)
Parking Spaces (No.)	0	3	min. 8 (based on 1 per 500sf)
Parking Area Setbacks (feet), where applicable	N/A	N/A	min.
Loading Spaces (No.)	0	0	min.
Type of Construction	IIIA		
Distance to Nearest Building	0-Adjacent	0-Adjacent	min. Existing







EXISTING FLOOR PLAN  
**ARLINGTON MASS AVE**  
 645 Mass Ave - Arlington, MA 02476

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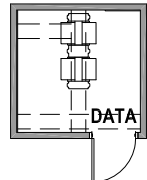


DATE	DESIGNER	AREA	SCALE
09 JUN 21	KMS	AREA sq. ft.	NONE

**DRC Page 6**

## INFORMATION ONLY

REPRESENTED FLOOR PLAN PRECEDES AOR'S FINAL DD / CODE COMPLIANCE VALIDATION. FINAL CONFIGURATION MAY VARY. NOTE TO AOR; DO NOT PROCEED WITH CONSTRUCTION DOCUMENTS IF THIS STAMP IS PRESENT.

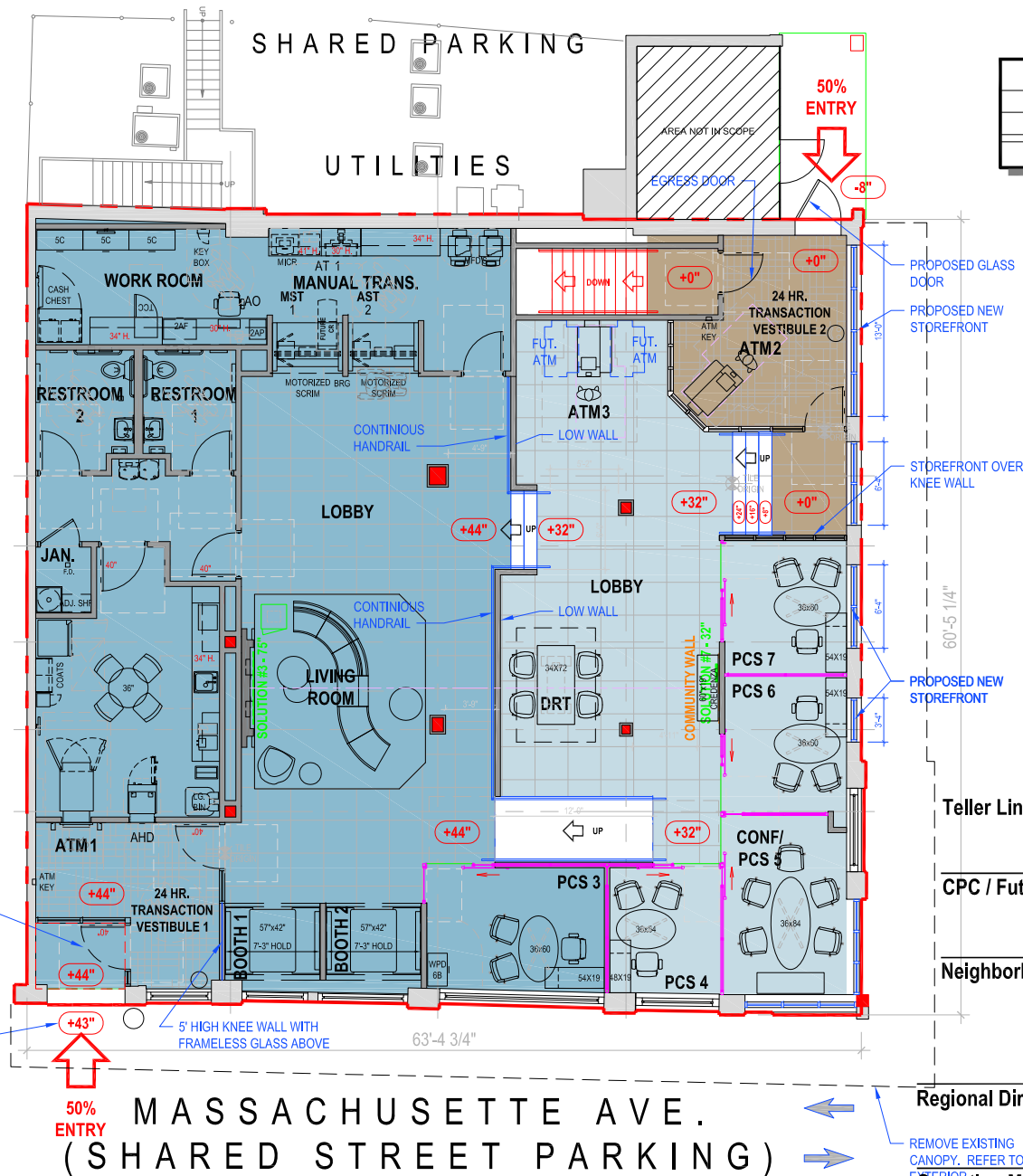


DATA AND ELECTRICAL ROOM IN LOWER LEVEL



ENSURE LEVEL LANDING & ADA ACCESS

VERIFY IN FIELD



## AREA CALCULATIONS (S.F.)

RETAIL BANK - MAIN LEVEL	3826
LOWER LEVEL	3862
<b>TOTAL (S.F.)</b>	<b>7688</b>

## DESIGN STANDARDS

20.3

## HARDINESS ZONE 7A

OVERHEAD DOOR ACCEPTABLE \*  
\* HARDINESS ZONES >= 7A

Teller Line BRG

YES ☐ NO ☐

CPC / Future CPC Office

Neighborhood Name

Regional Director (RD)

DATE

Operating Model Lead (OML)

DATE



PROPOSED FLOOR PLAN  
**ARLINGTON MASS AVE**  
645 Mass Ave - Arlington, MA 02476

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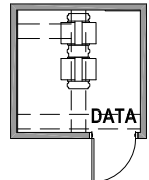
DATE	DESIGNER	AREA	SCALE
09 JUN 21	KMS	SEE ABOVE	NONE

**DRC Page 7**

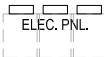


## INFORMATION ONLY

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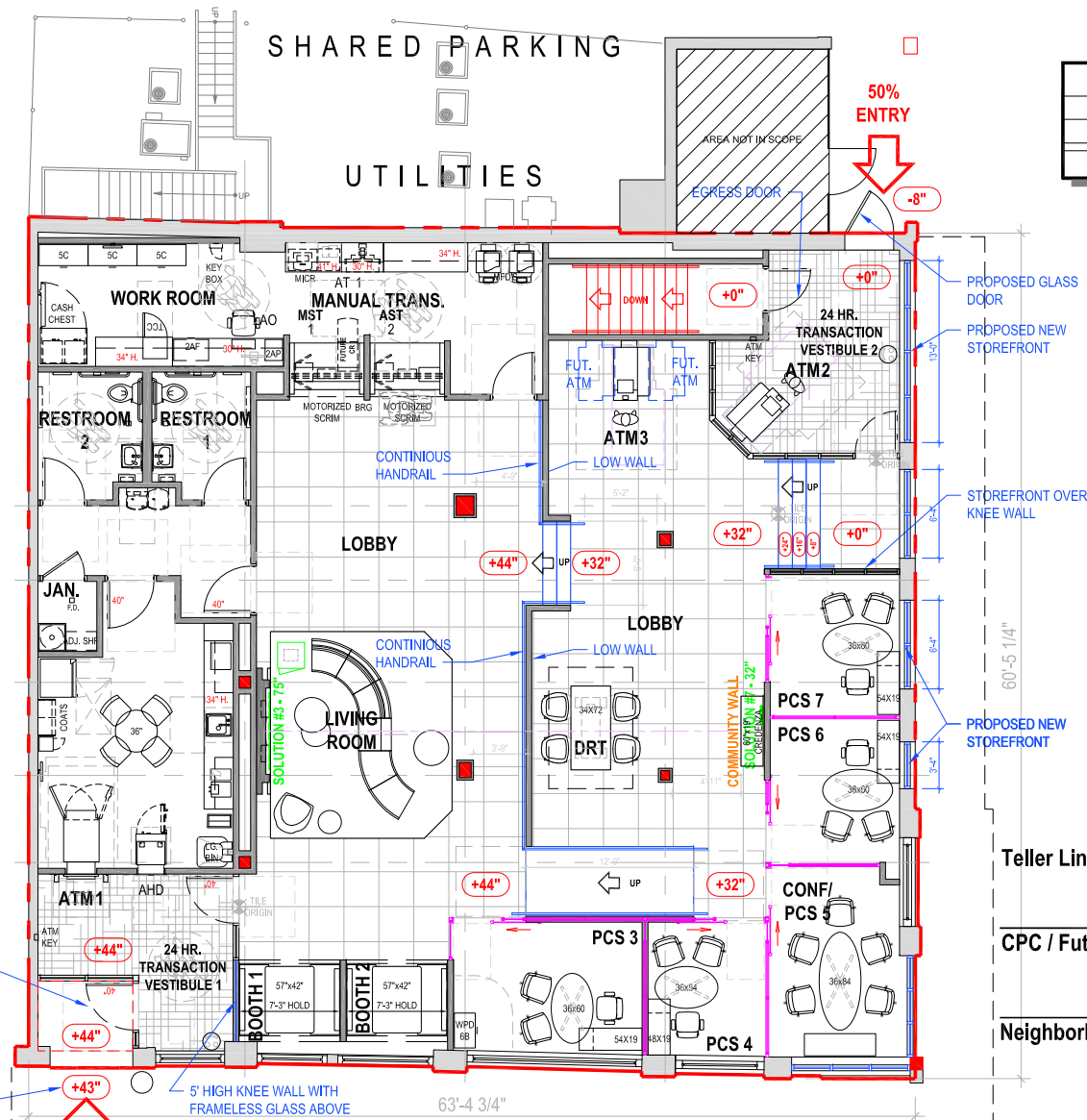


DATA AND ELECTRICAL ROOM IN LOWER LEVEL



ENSURE LEVEL LANDING & ADA ACCESS

VERIFY IN FIELD



## AREA CALCULATIONS (S.F.)

RETAIL BANK - MAIN LEVEL	3826
LOWER LEVEL	3862
<b>TOTAL (S.F.)</b>	<b>7688</b>

## DESIGN STANDARDS

20.3

## HARDINESS ZONE 7A

OVERHEAD DOOR ACCEPTABLE \*  
\* HARDINESS ZONES >= 7A

DAVID LAMSON WAY

Teller Line BRG

YES ☐ NO ☐

CPC / Future CPC Office

Neighborhood Name

Regional Director (RD)

DATE

Operating Model Lead (OML)

DATE



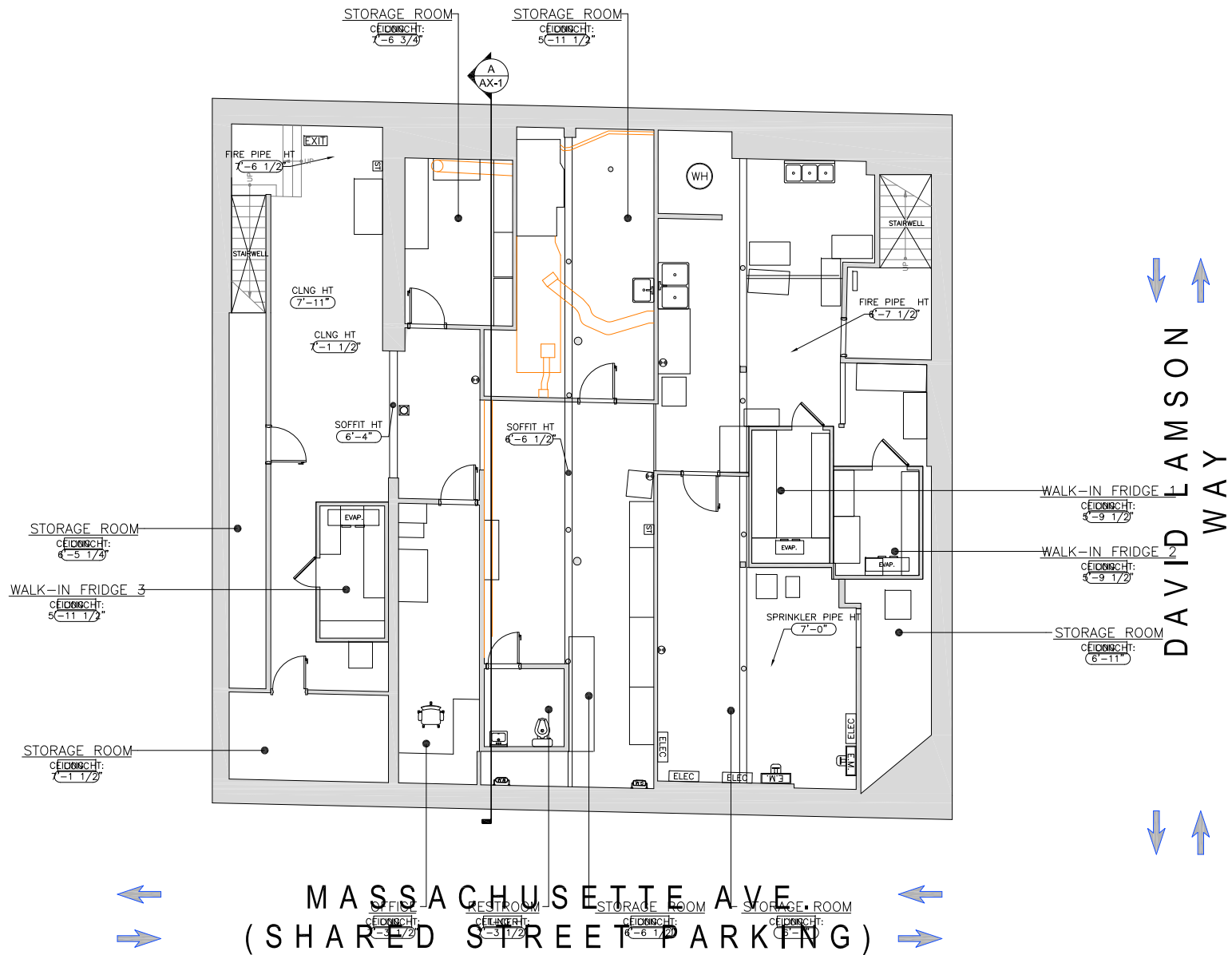
PROPOSED FLOOR PLAN  
**ARLINGTON MASS AVE**  
645 Mass Ave - Arlington, MA 02476

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DATE	DESIGNER	AREA	SCALE
09 JUN 21	KMS	SEE ABOVE	NONE

**DRC Page 8**



EXISTING LOWER LEVEL FLOOR PLAN  
**ARLINGTON MASS AVE**  
 645 Mass Ave - Arlington, MA 02476

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DATE	DESIGNER	AREA	SCALE
09 JUN 21	KMS	AREA sq. ft.	NONE
DRC Page 9			













Rear egress (Not ADA Compliant)

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Tenant will have exclusive rights to 3/5 parking spots (TBD)

DRC Page 12





View from Massachusetts Ave



Views from David Lamson Way





View from  
municipal  
parking



Caldwell Bankers



Bank of America



Rear entrance









July 28, 2021

Town of Arlington  
Redevelopment Board  
730 Mass Ave. Annex  
Arlington, MA 02476

Re: Application for Special Permit Review, 645 Massachusetts Avenue

The whom it may concern;

The following information is regarding a proposed Chase Bank facility at 645 Massachusetts Avenue, Arlington MA. The project will entail the installation of a new financial center in the location formerly occupied by Not Your Average Joe's restaurant and bar. The project will involve work on the first floor of the building and is an interior renovation, not exceeding the building limits.

Please see below, provided to meet the informational requests of the Petition for Special Permit under Environmental Design Review, per section 3.4 of the Arlington Zoning Bylaw for Applicability.

1. Preservation of Landscape: The proposed Chase Bank will be within the existing building limits. As such, the proposed project will not impact any of the existing landscape.
2. Relation of buildings to environment: The proposed project is largely contained in the first floor of the existing two-storey building, so the relationship of the existing building to the adjacent structures will be maintained. Additionally, exterior materials (largely brick) will be maintained, retaining the current building's character.
3. Open Space: The proposed Chase Bank will be within the existing building limits, largely on the first floor of the existing two-storey building. As such, the proposed project will not alter any existing open space.
4. Circulation: The proposed Chase Bank will be within the existing building limits, largely on the first floor of the existing two-storey building. As such, the proposed project will not alter any existing circulation. The bank will have two entrances, one in the front, one in the rear facing the parking lot.
5. Surface Water Drainage: The proposed Chase Bank will be within the existing building limits, largely on the first floor of the existing two-storey building. As such, the proposed project will not impact any existing surface water drainage, nor will it contribute any new load to the drainage systems.
6. Utility Service: Chase Bank intends to reuse the existing utilities that were feed the prior tenant (Not Your Average Joe's). This includes electrical, gas, water, sewer, and fire sprinkler services.



The bank, in all likelihood, will use significantly less utilities and resources than the prior restaurant use.

7. Advertising Features: The bank is proposing a set of 18" internally illuminated channels over the entrance on Massachusetts Avenue. The bank is also proposing a set of 14" internally illuminated letters at the rear entrance, facing the parking lot. The (2) building letter sets comply with section 6.2.8 of the zoning by-laws. Refer to attached for additional information on the proposed signage package.
8. Special Features: The proposed Chase Bank will be within the existing building limits, largely on the first floor of the existing two-storey building. The bank proposes to remove the existing awning from the front and side of the building as part of the project. While the existing equipment area in the rear of the building is to remain, the bank use will require significantly less equipment, and will remove any of those features made obsolete by the conversion such as the exhaust system, exterior storage, and food waste containment.
9. Safety: The proposed Chase Bank will be within the existing building limits, largely on the first floor of the existing two-storey building. The existing life safety systems, including fire alarm and fire sprinkler, will be modified as needed based on the proposed work to maintain all current and required life safety requirements. The project will not impact the exterior sidewalks, posing no impact to public safety beyond the confines of the building.
10. Heritage: The proposed Chase Bank will be within the existing building limits, largely on the first floor of the existing two-storey building. The building will mostly maintain the existing building materials and character, as not to impact the character of the building or the streetscape.
11. Microclimate: The proposed Chase Bank will be within the existing building limits, largely on the first floor of the existing two-storey building. As such, the proposed project will not pose any new impact on the microclimate of the area.
12. Sustainable building and Site Design: Chase Bank incorporates sustainable measures into their projects. While the building is not a LEED building Chase still takes measures in the form of energy efficient HVAC systems, efficient plumbing fixtures, LED light fixtures, and products incorporating low/no VOCs and recycled content.

In addition to the above, Chase bank is seeking relief on the parking requirements in the Zoning By-Laws. Section 6.1.4 of the By-Laws indicate that a business use would be required to provide 1 space per 500sf of gross floor area. Since the proposed Chase Bank space is 3,826sf on the first floor (customer floor), that would require 8 parking spaces. The landlord is providing the bank with 3 dedicated spaces for their use, which falls short of the 8 space requirement. Since the Chase Bank space has on-street parking directly in front along Massachusetts Avenue and a large municipal parking lot directly behind, Chase Bank is seeking relief from the requirement in the By-Laws.

Chase bank is seeking relief on the bicycle parking requirements in the Zoning By-Laws. Section 6.1.12 of the By-Laws indicate that a business use would be required to provide .3 spaces per 1000sf of gross floor area. . Since the proposed Chase Bank space is 3,826sf on the first floor (customer floor), that would require 1.2 spaces (rounds up to 2). Since the Chase Bank space has a bicycle rack in the sidewalk directly

in front along Massachusetts Avenue and 6 racks in the large municipal parking lot directly behind, as well as pay per ride bicycles, Chase Bank is seeking relief from the requirement in the By-Laws.

Please let us know should any further information be required.

Sincerely,

James Lalli  
Director of Architecture - Financial  
908.462.9949 | jlalli@core-states.com



Project Details

Project Name:	Arlington Mass Ave P373597	DND#:	0
Branch Name:		Region:	
Document Name:	20210701_Arlington Mass Ave_Brand Book_DRC PENDING.pdf	Market:	
DRC Date:	07/06/2021	Address:	
OVP#:	38100P373597	City:	
Program:	New Build Program	State:	
	<input type="checkbox"/> ATM - Offline	Zip:	
	<input type="checkbox"/> BBI - Offline		
	<input type="checkbox"/> Large Cap Retrofit		
	<input type="checkbox"/> Merch Only – Offline		
	<input checked="" type="checkbox"/> NB - Regular or Expansion		
	<input type="checkbox"/> Path		
Project Program:	<input type="checkbox"/> Path of Travel - Offline		
	<input type="checkbox"/> Relocations		
	<input type="checkbox"/> Retrofit - BAU - Offline		
	<input type="checkbox"/> Retrofit - Community - Offline		
	<input type="checkbox"/> Retrofit - Companion - Offline		
	<input type="checkbox"/> RSU		
	<input type="checkbox"/> Signage - Offline		
Project Type:	New Build Signage/Elevation		
Designer:	Kanishka Moham Salehi		
Status:	Approved		
Comment		By	On
AV Approved. If AV changes during any phase of the project contact Allison Howard		Allison Miche Howard	7/2/2021
Approver	Approval Status	Responded	
Ashlee Jo Kelly	Approved	07/08/2021 15:38	
Robert John Placek	Approved	07/06/2021 11:14	
Tiffany Anne McLeod	Approved	07/08/2021 08:24	



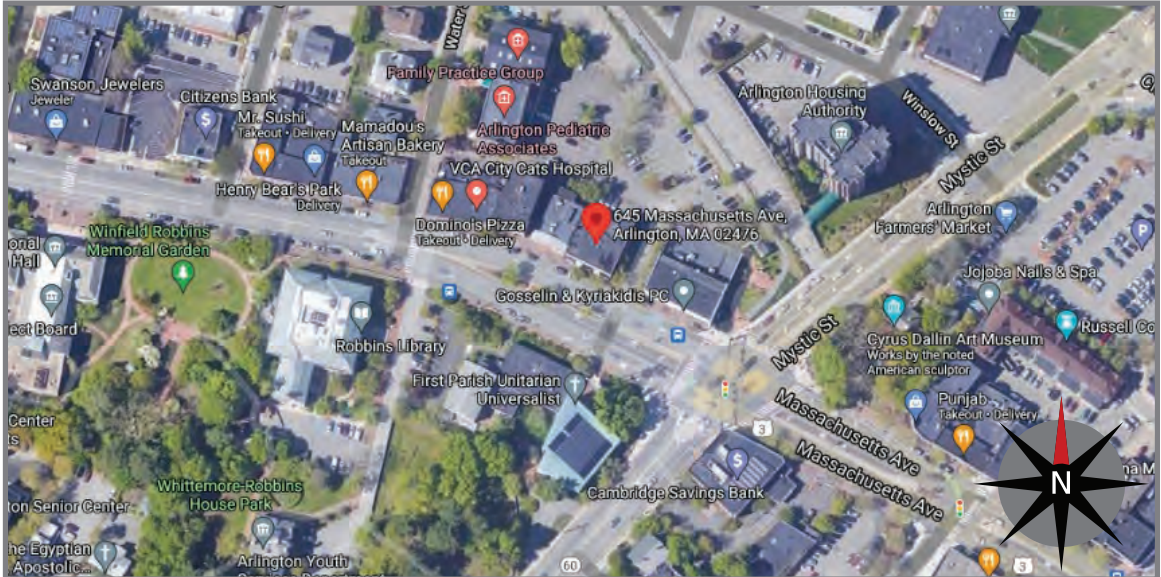
ARLINGTON MASS AVENUE

645 Massachusetts Avenue  
Arlington, MA 02476

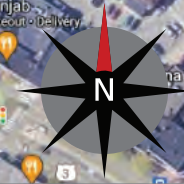


**REVISION NOTES:**

06.23.21	JM	Delete Octagons from All Awnings. Add Option 2, E12 and E13
07.01.21	RJW	Revised as noted.

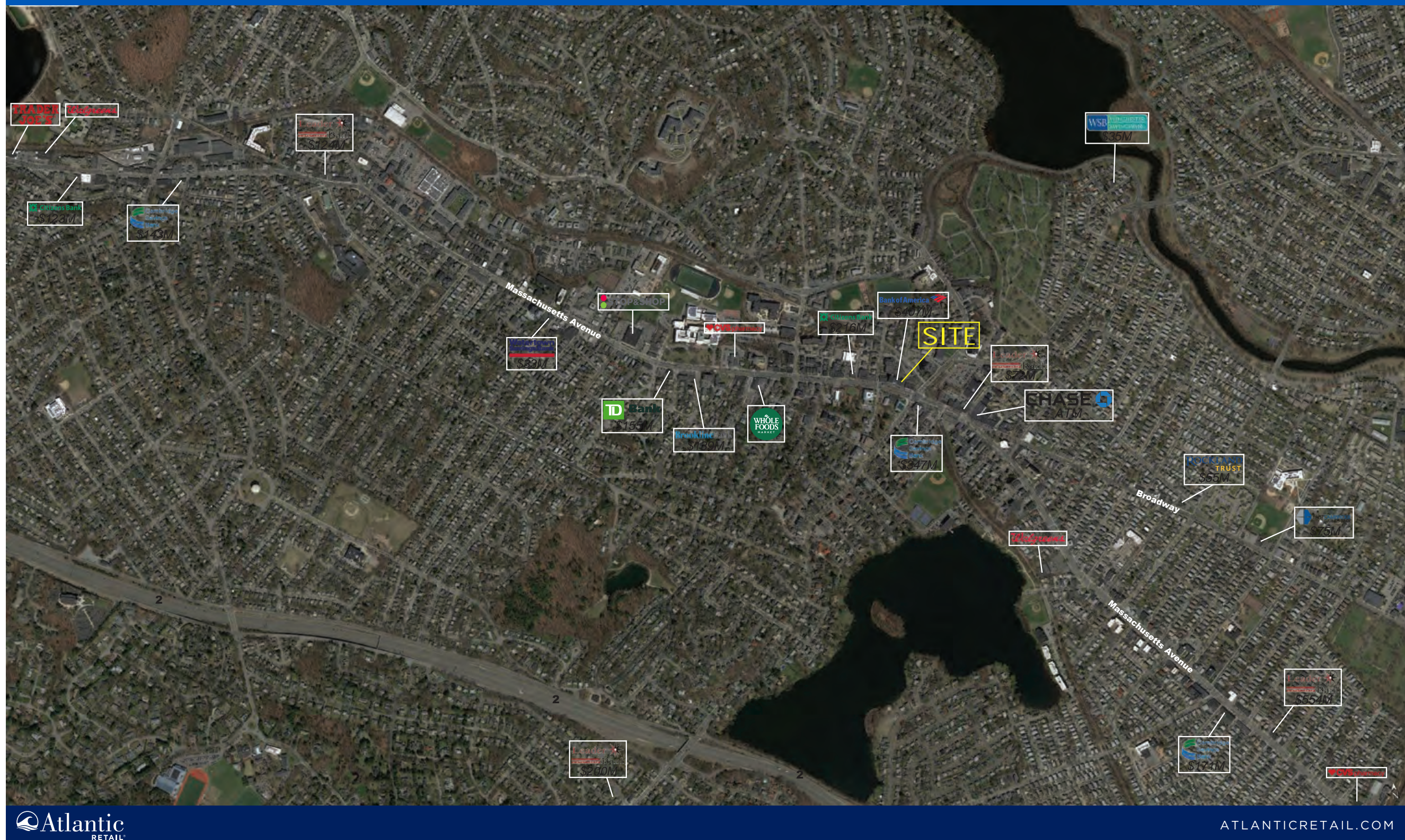


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## ARLINGTON, MA

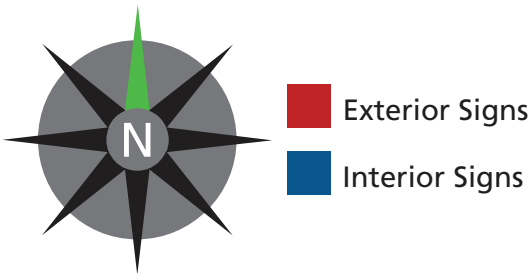




Exterior Scope of Work

E01	LIF-R-BLK-18-LED	BLACK W/ BLUE OCT DAY/NIGHT ILLUM CHNL LTRS - TOP MTD RCWY	20.7SF
E02	NMP-XX	DARK NICKEL METAL PANEL	
E03	F-6	DOUBLE-FACED ILLUMINATED FLAG MOUNTED SIGN	8.2SF
E04	NMP-XX	DARK NICKEL METAL PANEL	
E05	LIF-WBO-14-LED	WHITE W/ BLUE OCTAGON ILLUM CHANNEL LETTERS	12.7SF
E06	NMP-XX	DARK NICKEL METAL PANEL	
E07	TC-W-A-RE	WALL MTD REGULATORY SIGN - BANK PATRONS ONLY (RE-ENGINEERED)	2SF
E08	TC-W-A-RE	WALL MTD REGULATORY SIGN - BANK PATRONS ONLY (RE-ENGINEERED)	2SF
E09	TC-W-A-RE	WALL MTD REGULATORY SIGN - BANK PATRONS ONLY (RE-ENGINEERED)	2SF

ALL SIGNS TO BE  
REVIEWED/APPROVED  
BY HRC

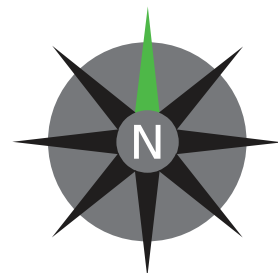




## Floor Plan

## Interior Scope of Work

I01	ADA-EX	ADA HANDICAPPED EXIT PLAQUE	.25SF
I01.1	CUST-VIN	MATCHING BRONZE VINYL BACKER	
I02	ADA-EX	ADA HANDICAPPED EXIT PLAQUE	.25SF
I02.1	CUST-VIN	MATCHING BRONZE VINYL BACKER	
I03	ADA-RRAG-A-G	ADA ALL GENDER RESTROOM SIGN - ACCESSIBLE	1.4SF
I04	ADA-RRAG-A-G	ADA ALL GENDER RESTROOM SIGN - ACCESSIBLE	1.4SF
I05	ADA-TW	ADA TELLER WALL SIGN	.1SF
I06	ADA-TW-ALS	ADA TELLER WINDOW - ASSISTIVE LISTENING SYSTEM	.1SF
I07	TPL-BTR-B-24	24" THIN PROFILE ILLUMINATED INTERIOR BLUE OCTAGON	4SF
I08	eATM-PANEL-SINGLE-SIDE-CAR	BRANDING PANEL FOR SINGLE SIDE CAR eATM 3.0	
I09	SUR-TTW-U-4-TP	ILLUMINATED THIN PROFILE ATM SURROUND	33SF
E10	ADA-EP	ADA HANDICAPPED ENTRANCE PLAQUE	.25SF
E10.1	CUST-VIN	MATCHING BLUE VINYL BACKER	
E11	ADA-EP-NA	ADA HANDICAPPED ENTRANCE PLAQUE - NOT ACCESSIBLE	.38SF



- Exterior Signs
- Interior Signs



## CHS.NB.961 - Arlington Mass Avenue

645 Massachusetts Avenue  
Arlington, MA 02476

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**DESIGNER - JM**

**CREATED** - 06.17.21

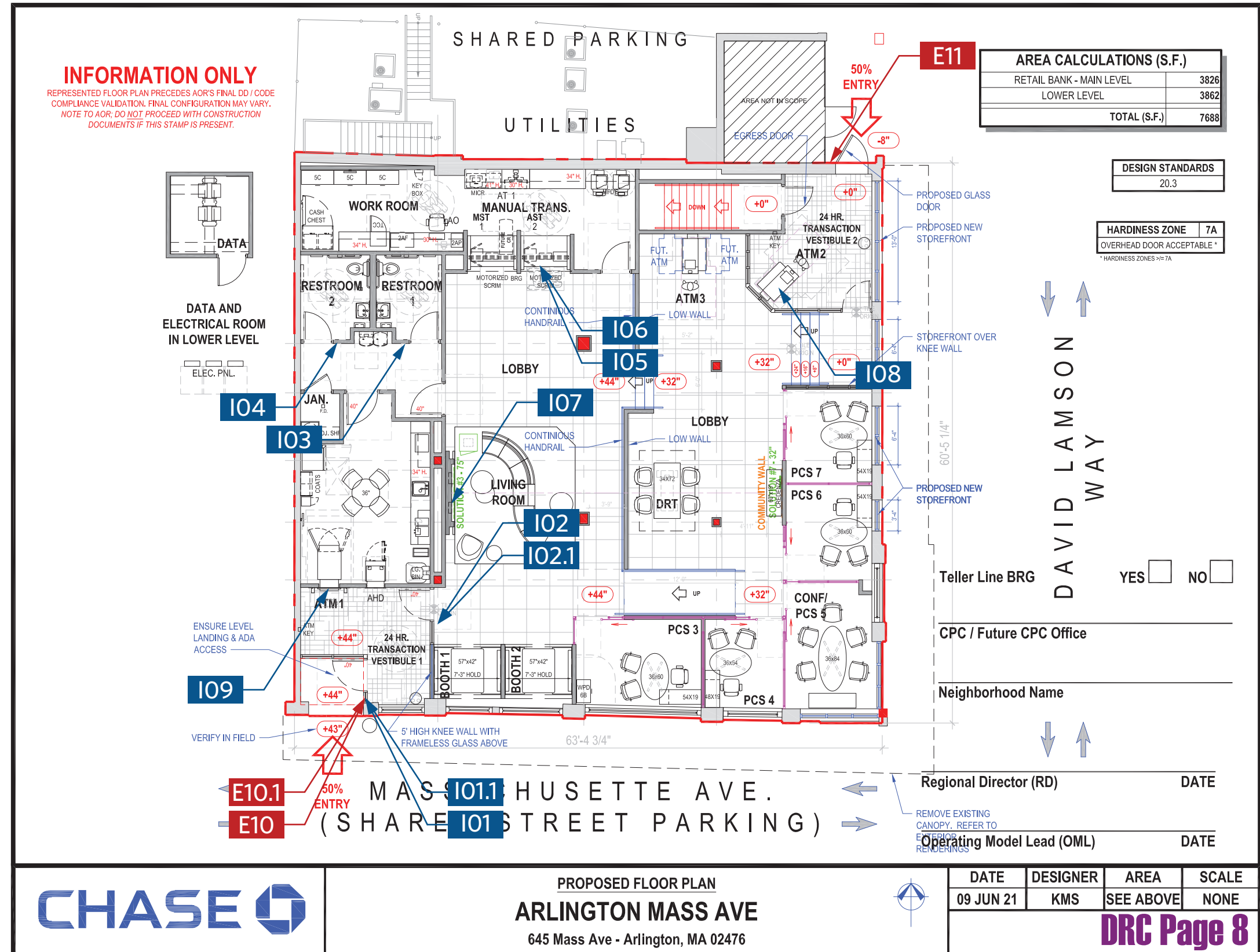
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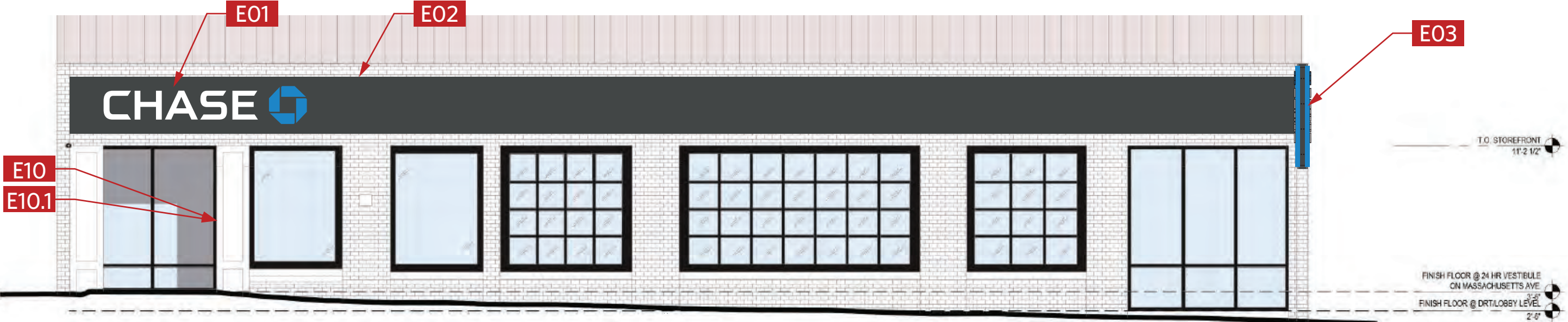


PHILADELPHIASIGN  
BRINGING THE WORLD'S BRANDS TO LIFE

707 WEST SPRING GARDEN ST • PALMYRA, NJ • 08065  
P: 856-829-1460 • F: 856-829-8549 • WEB: <http://www.philadelphiasign.com>

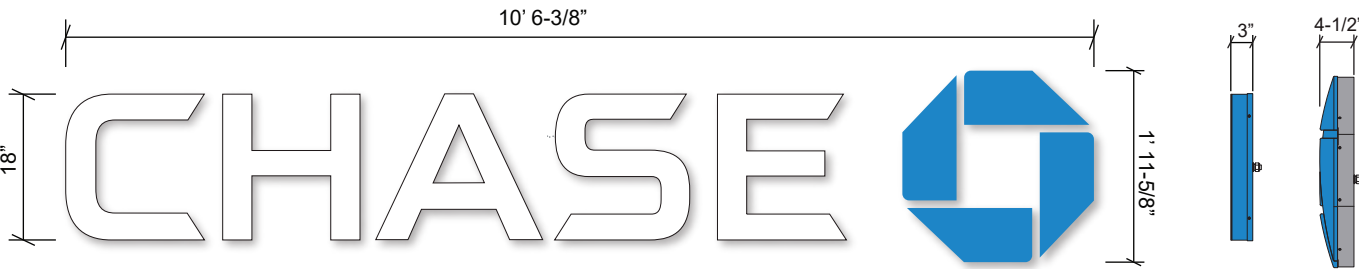
THIS IS AN ORIGINAL UNPUBLISHED DRAWING CREATED  
BY P.S.C.O. IT IS SUBMITTED FOR YOUR PERSONAL USE  
IN CONJUNCTION WITH A PROJECT BEING PLANNED  
FOR YOU BY P.S.C.O. IT IS NOT TO BE SHOWN TO ANYONE  
OUTSIDE YOUR ORGANIZATION NOR IS IT TO BE USED,  
COPIED, REPRODUCED, OR EXHIBITED IN ANY FASHION.





South Elevation - Massachusetts Ave

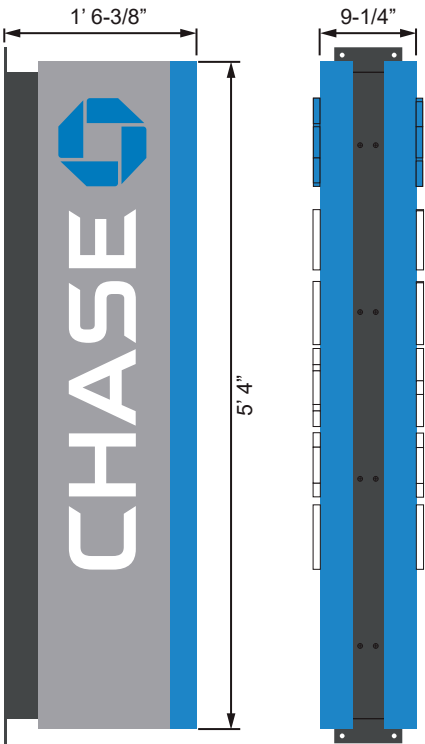
SIGN E10 NOT SEEN FROM THIS ANGLE



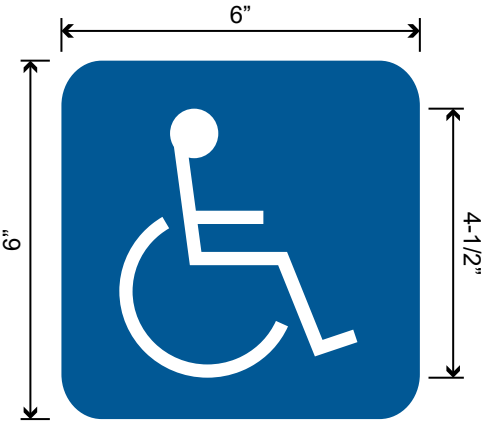
LIF-WBO-18-LED  
WHITE W/ BLUE OCTAGON ILLUM CHANNEL LETTERS - 20.7 SF  
SCALE: NTS



NMP-XX  
DARK NICKEL METAL PAN - DIMENSIONS TBD  
SCALE: NTS



F-6  
ILLUMINATED FLAG SIGN  
SCALE: NTS



ADA-EP  
HANDICAPPED ENTRANCE PLAQUE  
SCALE: NTS  
  
CUST-VIN  
MATCHING BLUE VINYL BACKER  
SCALE: NTS



SIGN E10 MOUNTS BACK-TO-BACK WITH SIGN I01.



CHS.NB.961 - Arlington Mass Avenue  
645 Massachusetts Avenue  
Arlington, MA 02476

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DESIGNER - JM  
CREATED - 06.17.21  
DRAWING - B95122

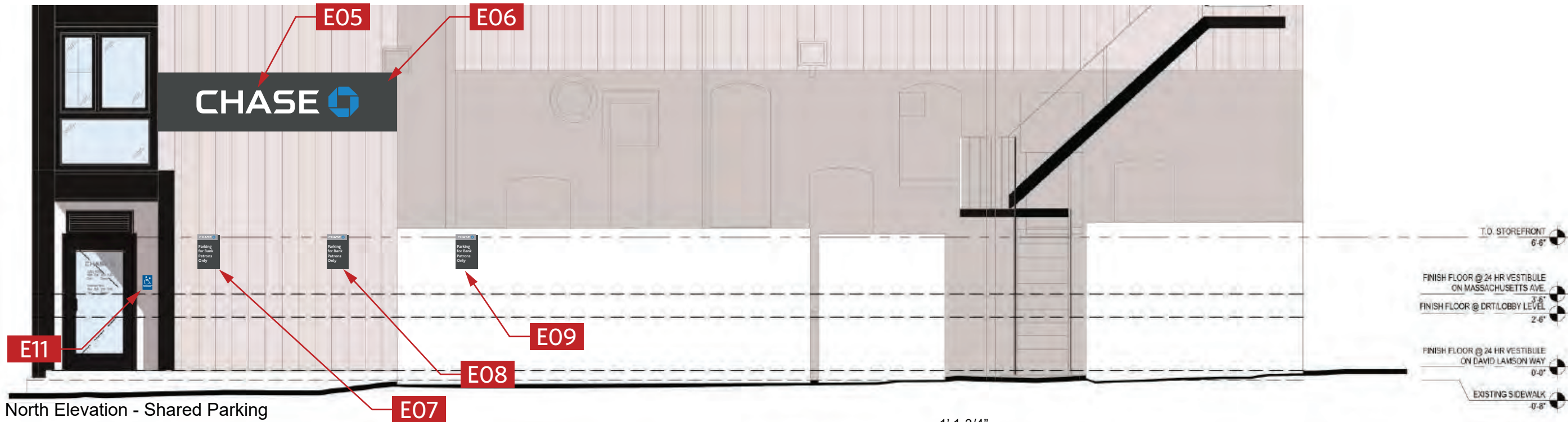
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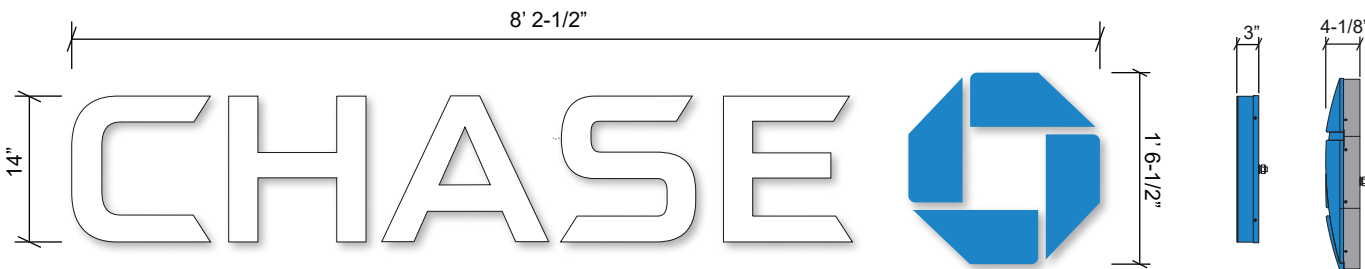




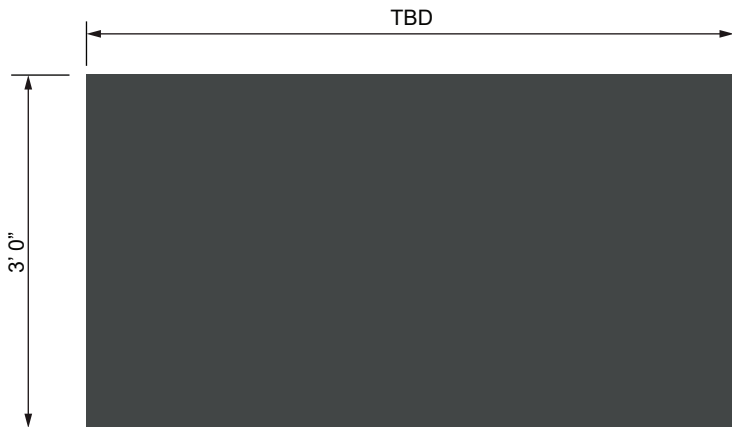
**NMP-XX**  
**DARK NICKEL METAL PAN - DIMENSIONS TBD**  
SCALE: NTS



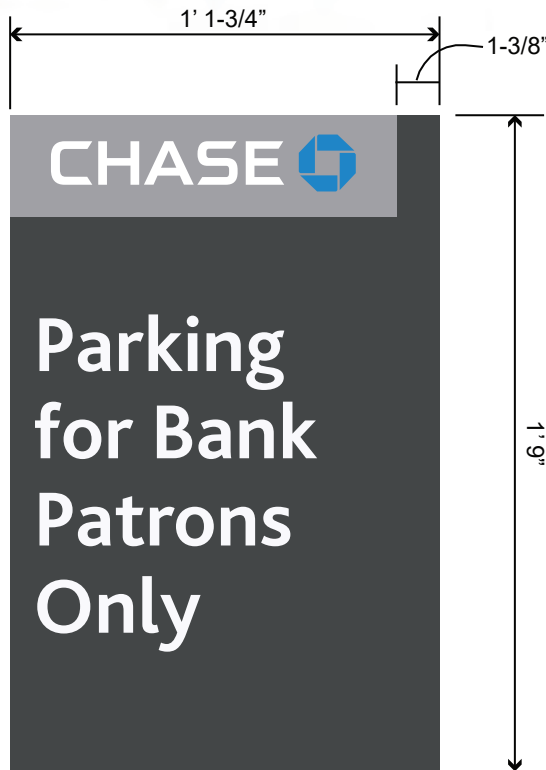
North Elevation - Shared Parking



**LIF-WBO-14-LED**  
**WHITE W/ BLUE OCTAGON ILLUM CHANNEL LETTERS - 12.7 SF**  
SCALE: NTS



**NMP-XX**  
**DARK NICKEL METAL PAN - DIMENSIONS TBD**  
SCALE: NTS



**TC-W-A-RE**  
**WALL MOUNTED REGULATORY SIGN (RE-ENGINEERED) - 2SF**  
SCALE: NTS



**ADA-EP-NA**  
**HANDICAPPED ENTRANCE PLAQUE - NOT ACCESSIBLE**  
SCALE: NTS



CHS.NB.961 - Arlington Mass Avenue  
645 Massachusetts Avenue  
Arlington, MA 02476

DESIGNER - JM  
CREATED - 06.17.21  
DRAWING - B95122

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ARCHITECTURAL RENDERING - CORNER ELEVATION



**CHS.NB.961 - Arlington Mass Avenue**  
645 Massachusetts Avenue  
Arlington, MA 02476

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**DESIGNER** - JM  
**CREATED** - 06.17.21  
**DRAWING** - B95122



**PHILADELPHIASIGN**

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ARCHITECTURAL ELEVATION - MASSACHUSETTS AVENUE



CHS.NB.961 - Arlington Mass Avenue

645 Massachusetts Avenue  
Arlington, MA 02476

105 of 347

DESIGNER - JM

CREATED - 06.17.21

DRAWING - B95122



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ARCHITECTURAL ELEVATION - DAVID LAMSON WAY ELEVATION



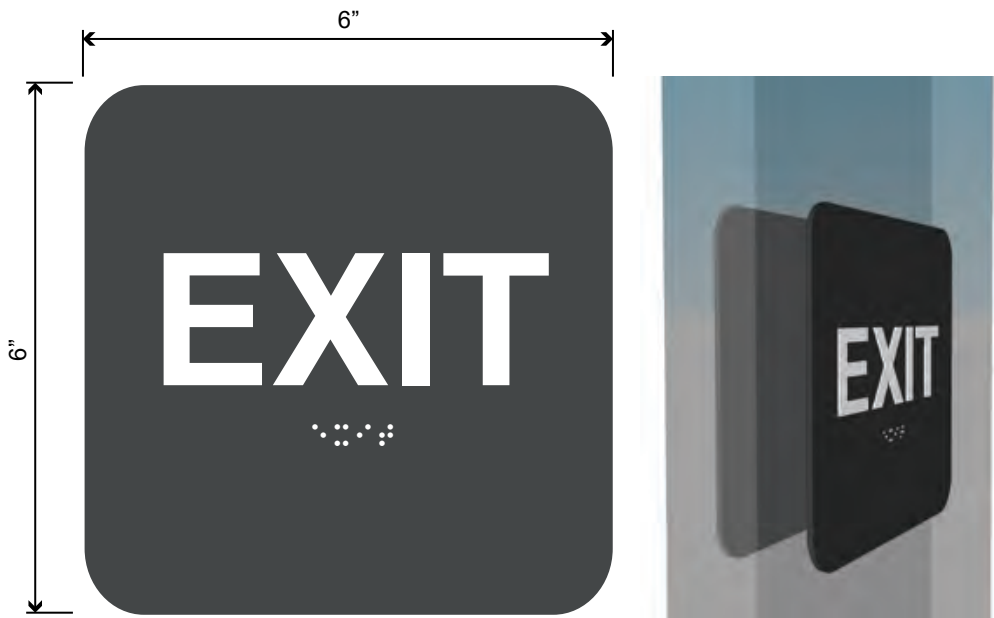
**CHS.NB.961 - Arlington Mass Avenue**  
645 Massachusetts Avenue  
Arlington, MA 02476

106 of 347

**DESIGNER** - JM  
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**ADA-EX**  
**HANDICAPPED EXIT PLAQUE**  
SCALE: NTS

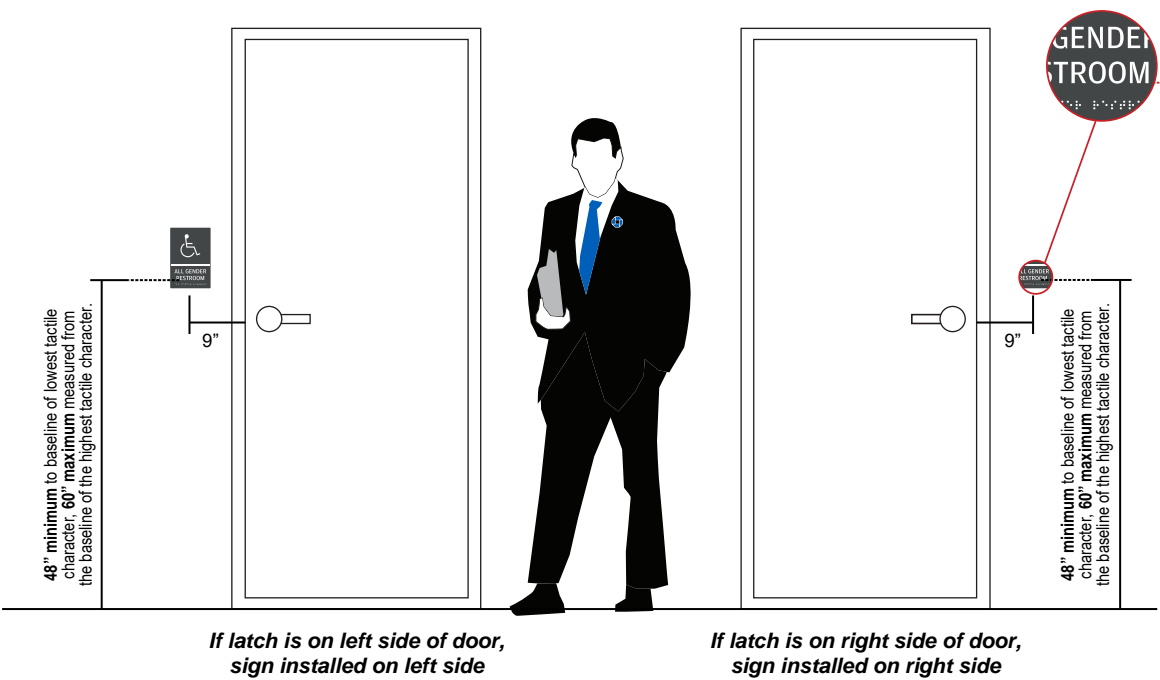
**SIGN I01 MOUNTS  
BACK-TO-BACK WITH  
SIGN E10.**

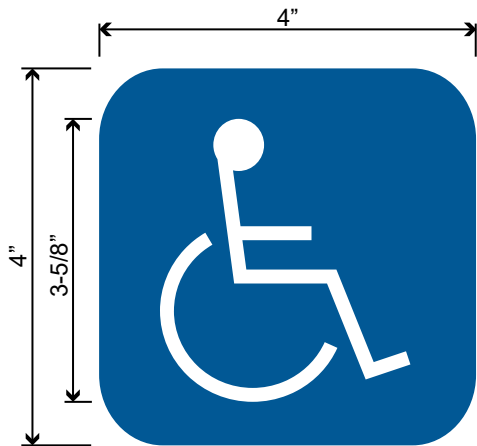
**CUST-VIN**  
**MATCHING BRONZE VINYL BACKER**  
SCALE: NTS



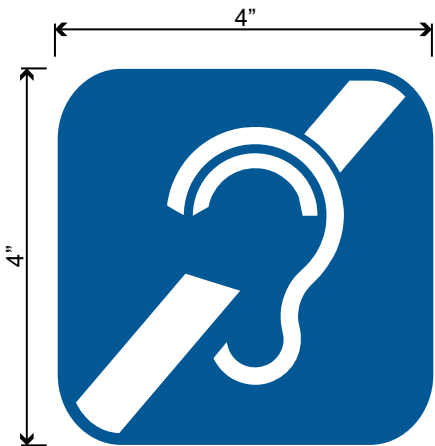
**ADA-RRAG-A-G** All Gender Neutral Restroom Signage  
for Restrooms that **ARE** Accessible

- Tactile sign identifying an accessible restroom entrance.
- Mounted on the wall, next to the door, on the latch side of the door.
- Acrylic tactile signs designed to meet Federal ADA 2010 ADAAG standards.

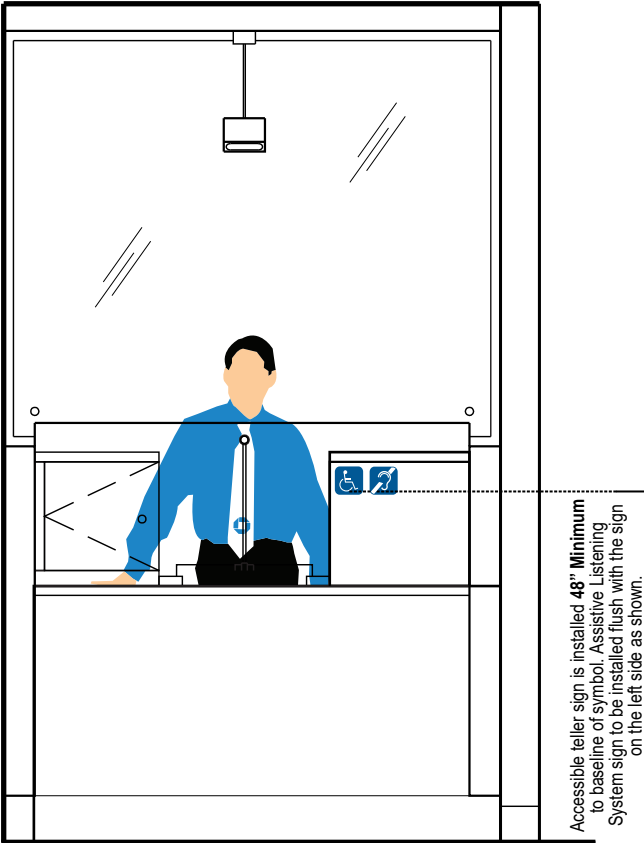




**ADA-TW**  
**ADA TELLER WALL SIGN**  
SCALE: NTS



**ADA-TW-ALS**  
**ADA TELLER WALL SIGN - ASSISTIVE LISTENING SYSTEM**  
SCALE: NTS

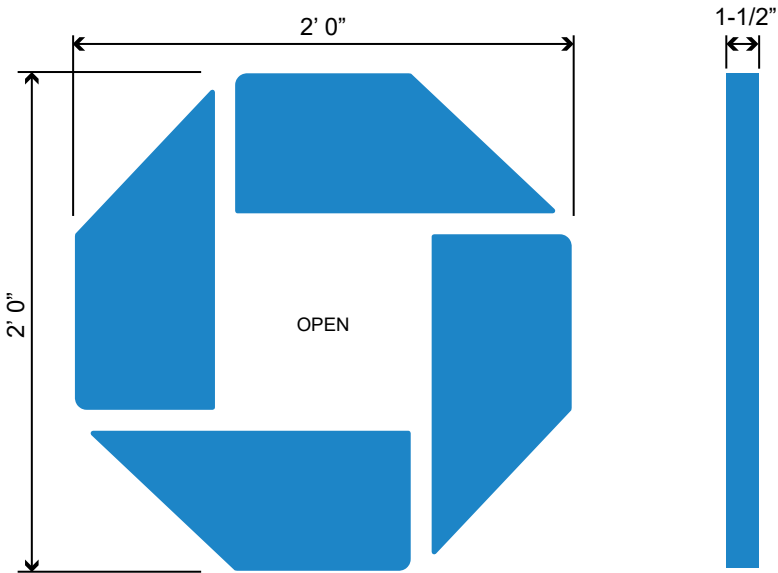


**Placement at Modular Teller Stations**  
**with Bullet-Resistant Glass**

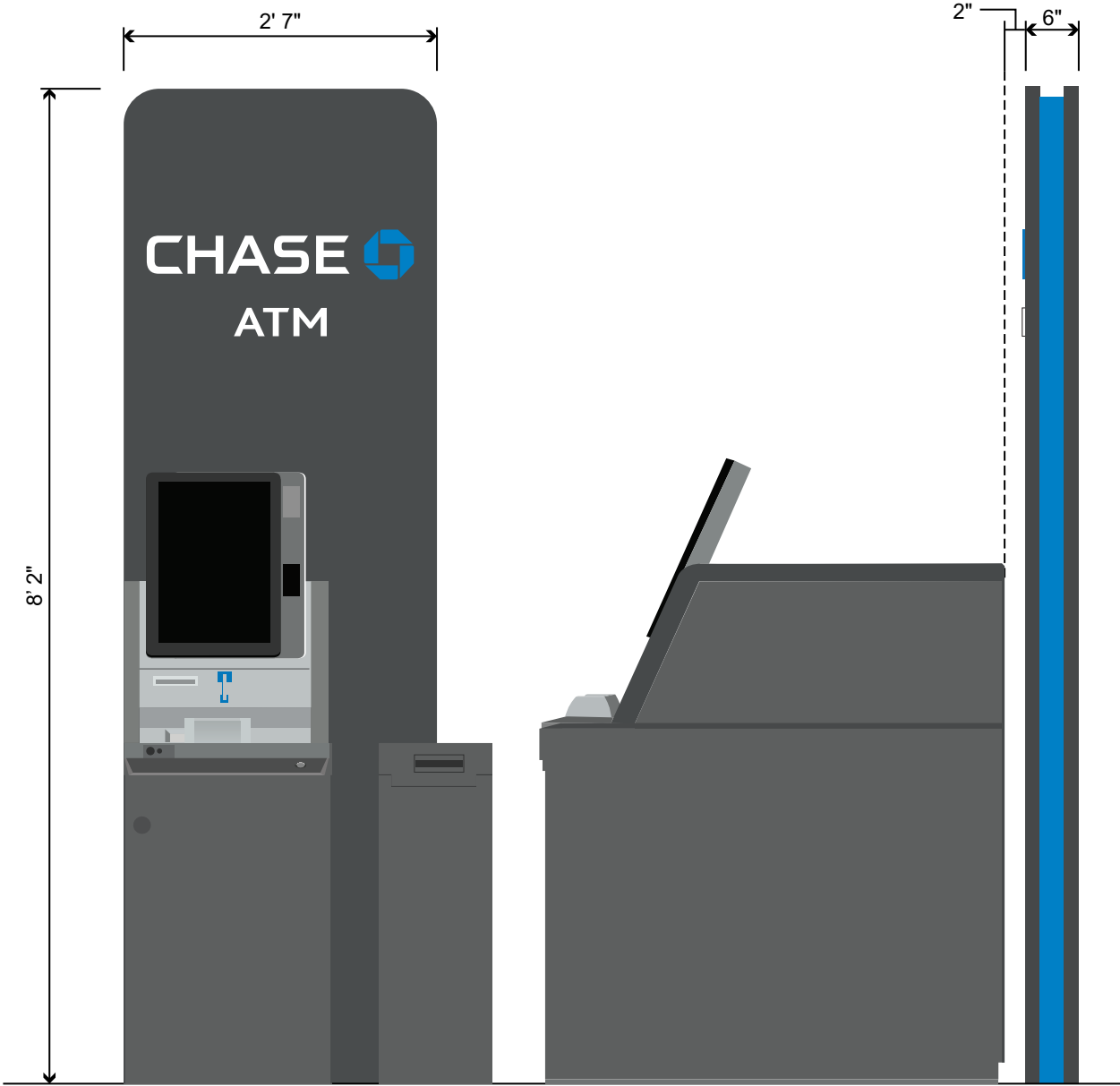




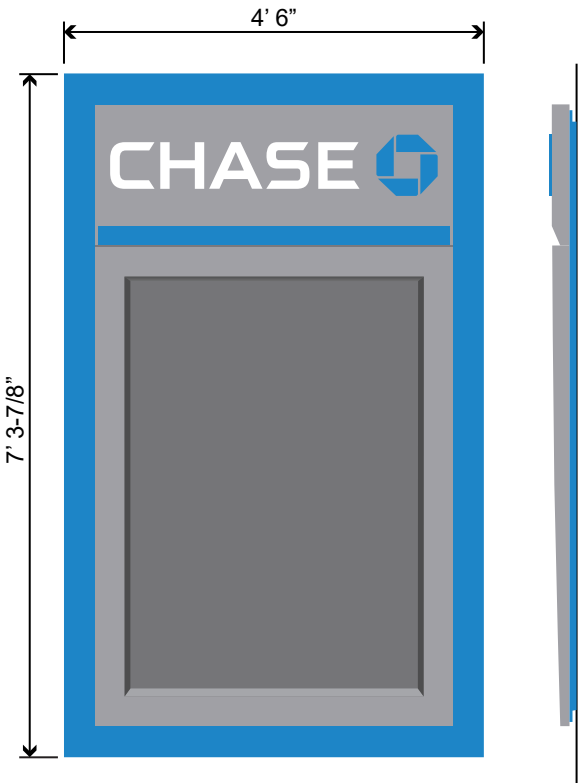
For Reference Only



**TPL-BTR-B-24**  
**24" THIN PROFILE ILLUMINATED INTERIOR BLUE OCTAGON - 4SF**  
SCALE: NTS  
NOTE: Octagon to be Purchased from Bitro.



**eATM-PANEL-SINGLE-SIDE-CAR**  
**BRANDING PANEL FOR SINGLE SIDE CARE eATM 3.0**  
SCALE: NTS



**SUR-TTW-U-4-TP**  
**THIN PROFILE ATM SURROUND - 33SF**  
SCALE: NTS



CHS.NB.961 - Arlington Mass Avenue  
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Arlington, MA 02476

DESIGNER - JM  
CREATED - 06.17.21  
DRAWING - B95122



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Survey Photos



CHS.NB.961 - Arlington Mass Avenue  
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Approach Photos



CHS.NB.961 - Arlington Mass Avenue  
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Arlington, MA 02476

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Code Allowances - Completed by Sign Vendor	
General Info	
Zoning Designation	B-5, Central Business
Temporary Signs	
Is the 8' x 4' Coming Soon Ground Sign Allowed? If so, how many?	Downtown location not suitable for F/S signs
Are temporary banners allowed? If so, for how long?	n/a
Are fly guys allowed? If so, for how long?	n/a
Primary Ground Sign	
Will code allow our standard pylon or monument? If yes, which one (ex: P-100, P-75, M-50, M-25, refer to reference guide for all standard options). Is more than 1 primary ground sign allowed?	Downtown location not suitable for F/S signs
If not, what are the pylon / monument restrictions? (include illumination restrictions/overall allowable sf / custom design requirements)	n/a
List the set back requirements.	n/a
Building Sign	
Prototypical FS branch - Will code allow our standard illuminated bldg. sign package (30" letterset on front & sides, 24" letterset on the rear)? Please list size and locations of signs allowed.	1 sign per street, no SF limitations; no flashing/blinking
If not, what are the variables/restrictions (include illumination and sf restrictions, as well as custom sign requirements)?	n/a
In-line / Urban branch - what are the exterior wall sign restrictions (sign type, including max. sf and illumination).	n/a
Interior Window Signs- List all interior window signs restrictions, including storefront set back requirements.	25% window area
ATM Topper- does this count against our overall allowable sf.? Is illumination allowed?	ATM signs subject to review and approval
Are decorative logos allowed (EFIS octagon)? Does it count against overall SF?	n/a
Directional / Regulatory Signs	
Is our standard directional and regulatory sign package allowed?	Downtown location not suitable for F/S signs
If not, what are the variables/restrictions?	n/a
Awnings / ATM Sunscreens	
Are branded awnings allowed?	yes
What if any restrictions are there (Illumination, color/materials, min & max projection)?	8' clearance, no SF limitation, see add'l comments
Are ATM sunscreens allowed? Do they count against overall SF?	ATM signs subject to review and approval
Other Governing Agencies	
Identify other governing agencies that could override code (ARB, HRB, PUD, etc) and list the known restrictions.	ALL SIGNS TO BE REVIEWD AND APPROVED BY the ARLINGTON HISTORICAL COMMISSION
Permitting / Variance Process	
What is the application process and timing for variance approval ? What are the variance application fees?	2-3 months
What is the likelihood of being granted a variance with this municipality?	25%
Architectural Lighting	
Is Architectural lighting allowed? Does it count against overall SF? List provisions.	no flashing/blinking. NO SIGNS TO BE ILLUMINATED FROM 12am - 6am
Additional Comments	
Please list any additional comments	Permitted by the following categories of signs: wall sign, window sign, awning sign





Bk: 50073 Pg: 393 Doc: DEED  
Page: 1 of 2 09/07/2007 04:01 PM

**QUITCLAIM DEED**

645 Mass. Ave. LLC, a Massachusetts limited liability company with a principal place of business at 11 Pepper Hill Drive, Winchester, MA 01890 for ONE MILLION NINE HUNDRED FIFTY THOUSAND and 00/100 (\$1,950,000.00) DOLLARS consideration paid grants to Key West Realty LLC, a Massachusetts limited liability company with a principal place of business at 63 Tropelo Road, Waltham, MA with quitclaim covenants the land and buildings thereon situated in Arlington, Middlesex County, Massachusetts and now numbered 645 to 651 Massachusetts Avenue and shown on a plan of "Land in Arlington, Mass., Boston & Maine Railroad to Myer Dana, E. D. Chapman, R. E. Engr. June 1922" recorded with Middlesex South District Deeds Book 4534, End, and bounded and described as follows:

SOUTHWESTERLY by said Massachusetts Avenue, as now laid out and existing, 64 feet;  
NORTHWESTERLY by land now or late of Menotomy Trust Company, 100 feet;  
NORTHEASTERLY by land now or formerly of Boston & Maine Railroad, 64 feet; and  
SOUTHEASTERLY by other land now or formerly of Boston & Maine Railroad, 100 feet.

Containing about 6,400 square feet according to said plan.

Be any or all of said measurements or contents more or less or however otherwise said premises may be measured, bounded or described.

Together with the right to sue for the ordinary purposes of a passageway to and from the granted premises, the existing passageway on the Southeasterly side of the granted premises, said passageway being shown on said plan and extending 100 feet Northeasterly from Massachusetts Avenue.

Subject to and with the benefit of easements, rights, restrictions and agreements of record so far as now in force and applicable.

For title, see deed recorded with Middlesex South District Registry of Deeds at Book 49471, Page 543.

MASSACHUSETTS EXCISE TAX  
Southern Middlesex District ROD # 001  
Date: 09/07/2007 04:01 PM  
Ctrl# 099332 27101 Doc# 00168668  
Fee: \$8,892.00 Cons: \$1,950,000.00

645 MASS. AVE. LLC

By: 

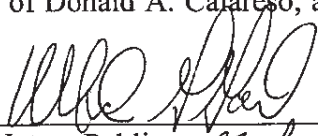
Donald A. Calareso, Manager

CLARK, HUNT AND EMBRY  
55 CAMERIDGE PARKWAY  
CAMBRIDGE, MA 02142


## COMMONWEALTH OF MASSACHUSETTS

Middlesex, ss.

On this 7<sup>th</sup> day of September, 2007, before me, the undersigned notary public, personally appeared 645 Mass. Ave. LLC by Donald A. Calareso, its Manager, proved to me to be the person whose name is signed on the preceding or attached document, through satisfactory evidence of identification, namely, a driver's license issued by the State of Massachusetts bearing the photographic image of the face and signature of Donald A. Calareso, and acknowledged to me that he signed it voluntarily for its stated purpose.

  
Notary Public Michael G. Gault  
My Commission Expires: March 14, 2008

SAWDOX\Bank\_RE\RE\06423\00009\00010931.DOC

  
Eugene C. Brune  
Attest Middlesex S. Registrar





**Town of Arlington, Massachusetts**  
Department of Planning & Community Development  
730 Massachusetts Avenue, Arlington, Massachusetts 02476

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## **Public Hearing Memorandum**

*The purpose of this memorandum is to provide the Arlington Redevelopment Board and public with technical information and a planning analysis to assist with the regulatory decision-making process.*

**To:** Arlington Redevelopment Board

**From:** Jennifer Raitt, Secretary Ex Officio

**Subject:** Environmental Design Review, 645 Massachusetts Ave, Arlington, MA  
Docket #3665

**Date:** August 24, 2021

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### **I. Docket Summary**

This is an application by JP Morgan Chase Bank NA, 1111 Polaris Parkway, Columbus, OH to operate a bank and replace the signage at 645 Massachusetts Avenue property owned by Key West Realty LLC, 63 Trapelo Road, Belmont, MA 02478. The opening of the Special Permit is to allow the Board to review and approve the use as a commercial bank branch with greater than 2,000 square feet in the B5 Central Business District, renovations to the building, and alterations to the façade under Section 3.4 Environmental Design Review (EDR). The prior use was a restaurant, a use that has been at this location since at least the 1970s.

Materials submitted for consideration of this application:

- Application for EDR Special Permit dated August 4, 2021;
- Project Narrative by Core States Group, dated July 28, 2021;
- Existing and Proposed Floor Plans and Photographs, prepared by KMS, dated June 9, 2021;
- Dimensional and Parking Information Sheet;
- Parking Plan;
- Sign Submittal Package, prepared by Philadelphia Sign, dated June 17, 2021; and
- Quitclaim Deed dated September 7, 2007.

II. Application of Special Permit Criteria (Arlington Zoning Bylaw, Section 3.3)

1. Section 3.3.3.A.

**The use requested is listed as a Special Permit in the use regulations for the applicable district or is so designated elsewhere in this Bylaw.**

A commercial bank of 2,000 square feet or more is allowed in the B5 Central Business District with a Special Permit under the jurisdiction of the ARB due to its location on Massachusetts Avenue. The Board can find that this condition is met.

2. Section 3.3.3.B.

**The requested use is essential or desirable to the public convenience or welfare.**

The use and the upgrades to the building and site are in the public's interest. However, banking uses can be found throughout the immediate area in Arlington Center, including four banks/ lending institutions and a number of stand-alone ATMs.

3. Section 3.3.3.C.

**The requested use will not create undue traffic congestion or unduly impair pedestrian safety.**

The proposed location is in the heart of Arlington Center. Many customers are likely to access this location by foot, bicycle, or use on-street parking along Massachusetts Avenue, or park in the public municipal lot behind the structure. The use will not impair pedestrian safety any more than the prior restaurant use. The Board can find that this condition is met.

4. Section 3.3.3.D.

**The requested use will not overload any public water, drainage or sewer system or any other municipal system to such an extent that the requested use or any developed use in the immediate area or in any other area of the Town will be unduly subjected to hazards affecting health, safety, or the general welfare.**

The bank does not have a high demand need for water or sewer. The Board can find that this condition is met.

5. Section 3.3.3.E.

**Any special regulations for the use as may be provided in the Bylaw are fulfilled.**

All such regulations are fulfilled.

6. Section 3.3.3.F.

**The requested use will not impair the integrity or character of the district or adjoining districts, nor be detrimental to the health or welfare.**



Multiple other banks are located within the vicinity and have had no detrimental impact on the integrity or character of the neighborhood, district, or adjoining districts, or on the health and welfare of the community. The Board can find that this condition is met.

**7. Section 3.3.3.G.**

**The requested use will not, by its addition to a neighborhood, cause an excess of the use that could be detrimental to the character of said neighborhood.**

At present, there are four different bank branches and additional ATMs in Arlington Center. The site of this proposed use is directly adjacent to another bank branch location. The prior restaurant use contributed to an active, lively streetscape in the afternoon and evening and provided meals tax income to the Town, neither of which can be said of the proposed use.

**III. Environmental Design Review Standards (Arlington Zoning Bylaw, Section 3.4)**

**1. EDR-1 Preservation of Landscape**

**The landscape shall be preserved in its natural state, insofar as practicable, by minimizing tree and soil removal, and any grade changes shall be in keeping with the general appearance of neighboring developed areas.**

The project site is developed, contains a multi-tenant building, and is entirely impervious. The landscaping around the perimeter of the site will remain in its current state. The Board can find that this condition is met.

**2. EDR-2 Relation of the Building to the Environment**

**Proposed development shall be related harmoniously to the terrain and to the use, scale, and architecture of the existing buildings in the vicinity that have functional or visible relationship to the proposed buildings. The Arlington Redevelopment Board may require a modification in massing so as to reduce the effect of shadows on the abutting property in an R0, R1 or R2 district or on public open space.**

The existing storefront and entry will be renovated, and a second rear entry will be introduced. Additional windows will be introduced to the façade facing David Lamson Way, increasing the transparency of the ground floor. New signage will be introduced. These updates will rehabilitate the existing storefront consistent with the style of the building and neighborhood.

The applicant proposes to eliminate the awning over the Mass Ave and David Lamson Way façades of the building which was added by the prior tenant. The presence of awnings along Mass Ave is desirable, as they provide shelter for pedestrians in

inclement weather, are within the design vernacular of other storefronts along the corridor and mediate the stark façade of the structure.

The applicant should confirm that the “proposed new storefront” callout along David Lamson Way on floor plans on DRC pages 7 and 8 refer to the new commercial windows and not operable service windows.

**3. EDR-3 Open Space**

**All open space (landscaped and usable) shall be so designed as to add to the visual amenities of the vicinity by maximizing its visibility for persons passing by the site or overlooking it from nearby properties. The location and configuration of usable open space shall be so designed as to encourage social interaction, maximize its utility and facilitate maintenance.**

There are no changes proposed to the existing building site and currently no open space on the project site. The Board can find that this condition is met.

**4. EDR-4 Circulation**

**With respect to vehicular and pedestrian and bicycle circulation, including entrances, ramps, walkways, drives, and parking, special attention shall be given to location and number of access points to the public streets (especially in relation to existing traffic controls and mass transit facilities), width of interior drives and access points, general interior circulation, separation of pedestrian and vehicular traffic, access to community facilities, and arrangement of vehicle parking and bicycle parking areas, including bicycle parking spaces required by Section 6.1.12 that are safe and convenient and, insofar as practicable, do not detract from the use and enjoyment of proposed buildings and structures and the neighboring properties.**

The number of parking spaces will increase from zero to three. The new use requires eight spaces, therefore the applicant requests relief from the Zoning Bylaw parking requirement. The site is adjacent to available on-street parking along Massachusetts Avenue and the public parking in the Railroad lot. The ARB has jurisdiction to reduce the number of parking spaces required by the Zoning Bylaw.

The applicant does not propose to add handicap parking, nor is the proposed rear entry to the building ADA accessible. Under this proposal, accessible access to the building would require parking along Mass Ave and travelling in the right of way until reaching a curb cut, or parking in the Railroad lot and circumnavigating the entire east and south sides of the building before reaching the accessible entry and designate one of the three parking spaces as HC accessible.

The circulation on the site will change with the addition of the second entry to the building off the Railroad lot. The application materials indicate an assumption that 50% of customers will use this entrance. The applicant is strongly encouraged to make the rear entry ADA accessible for the reasons described above.



Related to bicycle parking, the “office, business, or professional use” requires two long term and two short term bicycle parking spaces. The applicant has requested an exemption from the bike parking bylaw, citing bike racks along Massachusetts Avenue and in the Railroad lot as reasonable provision of parking. No long-term bicycle parking is indicated in the application.

The applicant should provide both indoor long-term bicycle parking for employees and short-term parking for visitors. The proximity of the business to the Minuteman Bikeway increases the demand for bicycle parking. For short term parking, the addition of a bike rack to the Massachusetts Avenue or the rear side of the building is recommended, which would support bicycle access to Arlington Center overall. Long term bicycle parking should be provided in an enclosed or covered area or inside the building. Further, indoor bicycle storage shall be included on the plan.

**5. EDR-5 Surface Water Drainage**

**Special attention shall be given to proper site surface drainage so that removal of surface waters will not adversely affect neighboring properties or the public storm drainage system. Available Best Management Practices for the site should be employed, and include site planning to minimize impervious surface and reduce clearing and re-grading. Best Management Practices may include erosion control and stormwater treatment by means of swales, filters, plantings, roof gardens, native vegetation, and leaching catch basins. Stormwater should be treated at least minimally on the development site; that which cannot be handled on site shall be removed from all roofs, canopies, paved and pooling areas and carried away in an underground drainage system. Surface water in all paved areas shall be collected in intervals so that it will not obstruct the flow of vehicular or pedestrian traffic and will not create puddles in the paved areas.**

**In accordance with Section 3.3.4., the Board may require from any applicant, after consultation with the Director of Public Works, security satisfactory to the Board to insure the maintenance of all stormwater facilities such as catch basins, leaching catch basins, detention basins, swales, etc. within the site. The Board may use funds provided by such security to conduct maintenance that the applicant fails to do.**

**The Board may adjust in its sole discretion the amount and type of financial security such that it is satisfied that the amount is sufficient to provide for any future maintenance needs.**

There will be no changes to the exterior of the building or surface water run-off as a result of this proposal. The Board can find that this condition is met.

**6. EDR-6 Utilities Service**

**Electric, telephone, cable TV, and other such lines of equipment shall be underground. The proposed method of sanitary sewage disposal and solid waste disposal from all buildings shall be indicated.**

There will be no changes to the utility service as a result of this proposal, which will affect only the interior of the building. The Board can find that this condition is met.

**7. EDR-7 Advertising Features**

**The size, location, design, color, texture, lighting and materials of all permanent signs and outdoor advertising structures or features shall not detract from the use and enjoyment of proposed buildings and structures and the surrounding properties.**

The project site is in the Business Sign District. The proposal exceeds the total number of signs allowable by right. The applicant submitted a sign package with a number of primary and incidental signs proposed:

- One wall sign with illuminated channel lettering located over the main entrance measuring 20.7 square feet;
- One wall sign with illuminated channel lettering located over the rear entrance measuring 12.7 square feet;
- One illuminated projecting sign measuring 8.17 square feet on each side;
- Three (3) wall mounted regulatory signs in parking area measuring 2 square feet each;
- One ADA entrance plaque measuring 0.25 square feet; and
- One ADA directional sign measuring 0.45 square feet.

The existing awning will be removed and the sign band across the Mass Ave and David Lamson Way façades will be covered with a dark nickel metal panel three feet in height.

While the narrative description addresses the two proposed wall signs, it does not describe the projecting sign, incidental signage, or other façade elements such as the new windows on David Lamson way and the dark nickel metal panels on all three facades.

Per Section 6.2.2(C), the ARB may grant a Special Permit to allow more than the number of signs allowed, “provided the architecture of the building, the location of the building relative to the street, or the nature of the use being made of the building is such that an additional sign or signs of a larger size should be allowed in the public interest.”

In terms of sign area, the two wall signs and the projecting sign comply with Section 6.2.5 of the Zoning Bylaw if the dark metal panels (E02, E03, and E06 in the sign package), for which dimensions are not provided, are not counted toward the total square footage of allowed signage. Dimensional information for the elements on all



three façades, including the mounting height of the projecting sign, are required to determine compliance with Sections 6.2.5(D)(8) and 6.2.5(D)(10).

Overall, the aggregate square footage of incidental signage exceeds what is allowed per Section 6.2.1(E)(3) by 0.076 square feet. The applicant has not proposed any window signage, however any intended signage including hours of operation and logos on entryways needs to be included in the sign package. Any additional window signage or incidental signage would add to the excess of the six feet of allowed incidental signage.

**8. EDR-8 Special Features**

**Exposed storage areas, exposed machinery installations, service areas, truck loading areas, utility buildings and structures, and similar accessory areas and structures shall be subject to such setbacks, screen plantings or other screening methods as shall reasonably be required to prevent their being incongruous with the existing or contemplated environment and the surrounding properties.**

There will be no adverse impacts on light, air and water resources, or on noise and temperature levels. The Board can find that this condition is met.

**9. EDR-9 Safety**

**With respect to personal safety, all open and enclosed spaces shall be designed to facilitate building evacuation and maximize accessibility by fire, police and other emergency personnel and equipment. Insofar as practicable, all exterior spaces and interior public and semi-public spaces shall be so designed to minimize the fear and probability of personal harm or injury by increasing the potential surveillance by neighboring residents and passersby of any accident or attempted criminal act.**

The renovation of the space at 645 Massachusetts Avenue will conform to code requirements for safety and accessibility by emergency personnel and equipment. The Board can find that this condition is met.

**10. EDR-10 Heritage**

**With respect to Arlington's heritage, removal or disruption of historic, traditional or significant uses, structures or architectural elements shall be minimized insofar as practical whether these exist on the site or on adjacent properties.**

The building containing 645 Massachusetts Avenue is listed on the *Inventory of Historically or Architecturally Significant Properties in the Town of Arlington* and is under the jurisdiction of the Arlington Historical Commission. The Historical Commission will review the proposal and this permit shall be conditioned on their approval.

**11. EDR-11 Microclimate**

**With respect to the localized climatic characteristics of a given area, any development which proposes new structures, new hard surface, ground coverage or**

**the installation of machinery which emits heat, vapor or fumes shall endeavor to minimize insofar as practicable, any adverse impacts on light, air and water resources or on noise and temperature levels of the immediate environment.**

There are no proposed changes (new structures, hard surface, ground coverage, or machinery) that will impact the microclimate. The Board can find that this condition is met.

**12. EDR-12 Sustainable Building and Site Design**

**Projects are encouraged to incorporate best practices related to sustainable sites, water efficiency, energy and atmosphere, materials and resources, and indoor environmental quality. Applicants must submit a current Green Building Council Leadership in Energy and Environmental Design (LEED) checklist, appropriate to the type of development, annotated with narrative description that indicates how the LEED performance objectives will be incorporated into the project.**

The construction will be primarily indoors, and will prioritize energy efficient HVAC systems, plumbing fixtures, LED fixtures, and products incorporating low/no VOCs and recycled content. The Board can find that this condition is met.

**IV. Conditions**

1. The final sign, exterior material, and lighting plans shall be administratively approved by the Department of Planning and Community Development. Any substantial or material deviation during construction from the approved plans and specifications is subject to the written approval of the Arlington Redevelopment Board
2. Any substantial or material deviation during construction from the approved plans and specifications is subject to the approval of the Arlington Redevelopment Board.
3. The Board maintains continuing jurisdiction over this permit and may, after a duly advertised public hearing, attach other conditions or modify these conditions as it deems appropriate in order to protect the public interest and welfare.
4. Snow removal from all parts of the site, as well as from any abutting public sidewalks, shall be the responsibility of the owner and shall be accomplished in accordance with Town Bylaws.
5. Trash shall be picked up only on Monday through Friday between the hours of 7:00 am and 6:00 pm. All exterior trash and storage areas on the property, if any, shall be properly screened and maintained in accordance with the Town Bylaws.



6. Upon the issuance of the building permit the Applicant shall file with the Inspectional Services Department and the Police Department the names and telephone numbers of contact personnel who may be reached 24 hours each day during the construction period.



**Town of Arlington, Massachusetts**  
Department of Planning & Community Development  
730 Massachusetts Avenue, Arlington, Massachusetts 02476

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## **Public Hearing Memorandum**

*The purpose of this memorandum is to provide the Arlington Redevelopment Board and public with technical information and a planning analysis to assist with the regulatory decision-making process.*

**To:** Arlington Redevelopment Board

**From:** Jennifer Raitt, Secretary Ex Officio

**Subject:** Environmental Design Review, 455-457 Massachusetts Avenue, Arlington, MA, Docket #3673

**Date:** September 27, 2021

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### **I. Docket Summary**

This is an application by 2-14 Medford Street, LLC, 455 Massachusetts Avenue, Suite 1, Arlington, MA, to open Special Permit Docket #3673 for the construction of a mixed-use building containing existing retail, restaurants, and adding 13 residential units, including two affordable rental units, at 455-457 Massachusetts Avenue and 2-14 Medford Street in the B3 Village Business District. The opening of the hearing is to allow the Board to review and approve the development under Section 3.4, Environmental Design Review Special Permit of the Arlington Zoning Bylaw.

The Applicant proposes to convert a portion of existing ground floor retail and second floor office spaces to residential units and to add a second floor to a one-story brick commercial structure, consolidating two buildings into one mixed use building. The proposed building is a two-story mixed-use structure. The 13 residential units are comprised of one studio and 12 one-bedroom apartments. Two affordable units, or 15% of the total units, are provided. Ten of the 11 existing commercial spaces would remain, resulting in a total commercial area of 7,802 square feet. Parking is provided onsite in a garage with 14 parking spaces.

Materials submitted for consideration of this application:

- Application for EDR Special Permit, including an Environmental Impact Statement;



- Site Development Plan Set, prepared by Allen & Major Associates, Inc. dated August 20, 2021;
- Architectural Drawing Set, including floor plans, elevations, and renderings, prepared by Market Square Architects, dated July 27, 2021;
- Description of project construction process by project estimated, no date;
- Mixed-Use Redevelopment Drainage Summary prepared by Allen & Major Associates, dated August 20, 2021; and
- LEED Checklist.

## II. Application of Special Permit Criteria (Arlington Zoning Bylaw, Section 3.3)

### 1. Section 3.3.3.A.

**The use requested is listed as a Special Permit in the use regulations for the applicable district or is so designated elsewhere in this Bylaw.**

Mixed-use is allowed by Special Permit in the B3 Village Business District. The Zoning Bylaw, in Section 5.5.1.D, indicates that the district's predominant uses include retail, service, and office establishments catering to both convenience and comparison-good shoppers and oriented to pedestrian traffic. Mixed-use buildings are allowed and encouraged, including in the principal business area at Medford Street and Massachusetts Avenue. Mixed-use is a combination of two or more distinct land uses, such as those proposed by this applicant, and the definition encourages such uses to be in a single, multi-story structure, such as that proposed by the applicant.

The Arlington Center area, which radiates outward from the intersection of Massachusetts Avenue and Mystic and Pleasant Streets, is a major shopping district in the town. It is comprised of a blend of zoning districts from R1 through R7 and B1 through the town's only B5 districts. Arlington Center includes several B3 districts; the district in which this parcel sits stretches from the southeast edge of Whittemore Park (Old Mystic Street) and Massachusetts Avenue to just beyond Franklin Street. To the southwest across Massachusetts Avenue is the B5 district, there are predominately single-story and two-story commercial buildings. To the northwest within the B3 district are additional two-story commercial storefronts along Massachusetts Avenue, there is a two-and-a-half story apartment building facing the Russell Common Parking Lot; to the southeast across Medford Street is another single-story and two-story commercial block, which includes the Regent Theater. To the northeast is the three-story Arlington Catholic High School and Saint Agnes Parish, and the Russell Common Parking Lot, all within the R1 district; directly east is the R2 zoning district with a predominantly two-family residential streetscape of Arlington Center is dominant.

The Board can find that this condition is met.

**2. Section 3.3.3.B.**

**The requested use is essential or desirable to the public convenience or welfare.**

The requested use is essential and desirable. The second key finding in the Master Plan notes that “Massachusetts Avenue has the capacity for growth. It can support mixed-use development commensurate with its function as Arlington’s primary commercial corridor. Massachusetts Avenue is accessible to neighborhoods throughout the town; it has frequent bus service, bicycle routes, and good walkability. Increased density through greater building heights and massing would benefit the corridor from an urban design perspective and benefit the town from a fiscal perspective”(p.8).

This proposal will bring one new studio and 12 one-bedroom residential apartment units, of which two will be affordable to households earning at or below 70% of the area median income. The Town has clearly established affordable housing priorities described in its Housing Production Plan (adopted by the Select Board and Redevelopment Board and approved by the State in 2016). New housing opportunities, including market-rate and affordable homes, are needed in the community; this project helps address that demand.

The ground floor commercial spaces along Medford Street and Massachusetts Avenue will largely remain, although several will be reduced in area as part of the renovation, and one will be converted to a lobby. Ten commercial spaces (noted as commercial tenants on the architectural plans) would remain following the redevelopment of the site, as would venting for the existing restaurants. The office spaces on the second floor would be eliminated. Overall, there will be net loss of approximately 6,900 square feet of commercial space. The loss is due to the need to provide an entry and circulation for the second-floor housing units, the elimination of office spaces on the second floor, and the need to also provide parking on the site.

The Board can find that this condition is met.

**3. Section 3.3.3.C.**

**The requested use will not create undue traffic congestion or unduly impair pedestrian safety.**

The proposed project includes 16 parking spaces for cars, comprised of two surface parking spaces, and 13 garage parking spaces, and one ADA accessible parking space. Regarding bicycle parking, the project includes eight short-term outdoor parking spaces and 26 long-term indoor spaces for building tenants and visitors. It is not expected that the proposed project will unduly impair pedestrian safety. The Board can find that this condition is met.

**4. Section 3.3.3.D.**

**The requested use will not overload any public water, drainage or sewer system or any other municipal system to such an extent that the requested use or any**



**developed use in the immediate area or in any other area of the Town will be unduly subjected to hazards affecting health, safety, or the general welfare.**

A Drainage Summary letter indicates that standards have been met with the proposed stormwater design, which includes the installation of pervious paver systems and an underground filtration system. In addition, a landscaped buffer will be introduced the site. Overall the proposal will result in a reduction of impervious area and quantity of stormwater flowing from the site. The proposed project will improve, not overload, public utilities. The Board can find that this condition is met.

**5. Section 3.3.3.E.**

**Any special regulations for the use as may be provided in the Bylaw are fulfilled.**

As a condition of any decision for the proposed mixed-use building, the Applicant will need to fulfill the requirements of Section 8.2 which outline the affordable housing requirements. A building with 13 units requires two affordable units that are representative of the mix of units in the building available to eligible households making up to 70% of the area median income. There are no other special regulations for the use that must be fulfilled. The Board can find that this condition is met.

**6. Section 3.3.3.F.**

**The requested use will not impair the integrity or character of the district or adjoining districts, nor be detrimental to the health or welfare.**

The proposed building largely maintains the existing first floor façade, with the additional improvement of restoration of decorative cement work at 455 Massachusetts Avenue, which is currently obscured by an awning. The façade of the commercial storefronts at 2-14 Medford Street is proposed to be retained, preserving the historic aesthetic of the structure consistent with other buildings in the business district.

The redevelopment of the second story and conversion into housing units will not impair the integrity or character of the district or the adjoining districts and it will not be detrimental to health or welfare. The proposed structure is generally consistent with the Design Standards for the Town of Arlington.

The upper floor façade is comprised of white fiber cement panels along Massachusetts Avenue, and light green fiber cement panels along Medford Street. The building also includes differentiation of the upper story and variation in the façade with a shared roof deck at the intersection of Massachusetts Avenue and Medford Street, which is encouraged in the Arlington Design Standards. Along Medford Street, the design of the second story is compatible the Regent Theater and its adjacent properties directly across the street. Additionally, the applicant has agreed to make the Park Terrace façade available for public art or mural installations to improve the relationship with the streetscape. The Applicant has also proposed installation of a more permanent

outdoor seating area with a canopy for more comfortable and year-round outdoor seating for the community.

While both 455-457 Massachusetts Avenue and 4-14 Medford Street are in the Arlington Center Historic District, only 4-14 Medford Street is a contributing historic structure. Both structures are under the jurisdiction of the Arlington Historical Commission, who will need to review the building design.

Long-term indoor bicycle parking is accessed through either the lobby or behind the building, short-term outside bicycle parking is available along Park Terrace; structured and surface vehicular parking is located on the ground floor and accessed via Park Terrace.

The Applicant is proposing a floor area ratio (FAR) of 1.5. The existing building's FAR is 0.98. The Zoning Bylaw allows a maximum FAR of 1.5 for a mixed-use building on a lot less than 20,000 square feet in this zoning district. The maximum FAR of 1.5 is at odds with the maximum story and height allowed in the Zoning Bylaw of up to 5 stories and 60 feet respectively.

The proposed mixed-use building is in keeping with adjacent land uses, both Massachusetts Avenue and Medford Street include residential uses, including The Legacy across Massachusetts Avenue to the east and multifamily buildings along Medford Street. While it is desired to maintain or increase the amount of commercial space, new residential units will not impair the integrity or character of the district, or the adjoining districts and it will not be detrimental to health or welfare.

**7. Section 3.3.3.G.**

**The requested use will not, by its addition to a neighborhood, cause an excess of the use that could be detrimental to the character of said neighborhood.**

The use will not be in excess or detrimental to the character of the neighborhood. The Board can find this condition is met.

**III. Environmental Design Review Standards (Arlington Zoning Bylaw, Section 3.4)**

**1. EDR-1 Preservation of Landscape**

**The landscape shall be preserved in its natural state, insofar as practicable, by minimizing tree and soil removal, and any grade changes shall be in keeping with the general appearance of neighboring developed areas.**

The existing property is entirely impervious and there is no natural landscape to preserve with the building fully saturating the building lot. As part of the project, two areas of approximately 2,457 square feet of impervious material will be replaced with a landscaped buffer of perennials, flowering trees, and arborvitae and pervious walkways along the side property line and a landscaped walkway to the lobby. The new



landscaping will introduce a buffer from the rear parking lot of the property at 473-475 Massachusetts Ave. The Board can find that this condition is met.

**2. EDR-2 Relation of the Building to the Environment**

**Proposed development shall be related harmoniously to the terrain and to the use, scale, and architecture of the existing buildings in the vicinity that have functional or visible relationship to the proposed buildings. The Arlington Redevelopment Board may require a modification in massing so as to reduce the effect of shadows on the abutting property in an R0, R1 or R2 district or on public open space.**

There are a range of architectural styles and zoning districts in the vicinity. Building heights in the vicinity range from single-story to 11-stories. The ground floor storefronts will largely be preserved, however the transparency of the lobby for the apartment units will be reduced as the existing glass storefront will be reconstructed with three smaller windows. The applicant proposes to remove the awning and restore the concrete detailing above the façade at 455 Massachusetts Avenue storefront; the existing benches along Massachusetts Avenue will be maintained, and flower boxes and awnings will be added along the façade. The second floor façade maintains a commercial mixed-use appearance and roofline consistent with adjacent structures in the district.

The applicant seeks relief from required setbacks. The structure is on a corner lot and subject to Section 5.3.8.A, which states that the setback should be the same as an adjacent lot. On the Medford Street, the adjoining property is the Arlington Catholic High School in the R1 Zoning District, which has a significantly larger setback than the existing structure at 2-14 Medford St. The Board can adjust this requirement per Section 5.3.16. The Board can find that this condition is met.

**3. EDR-3 Open Space**

**All open space (landscaped and usable) shall be so designed as to add to the visual amenities of the vicinity by maximizing its visibility for persons passing by the site or overlooking it from nearby properties. The location and configuration of usable open space shall be so designed as to encourage social interaction, maximize its utility and facilitate maintenance.**

As noted above, the proposed project will add areas of landscaping to an existing impervious site. The proposal includes approximately 2,457 square feet of landscaped open space along the rear of the building, which also provides a buffer with the adjacent building at 473-475 Massachusetts Avenue. This provides about half of the 10% landscaped open space requirement. The usable open space is located on the roof deck and is approximately 2,140 square feet. This is only 9% of the usable open space requirement and does not meet all the requirements for usable open space.

Lastly, the applicant seeks relief from the required 15-foot buffer in Section 5.3.21, as a landscaped buffer is precluded by the applicant's need to provide parking on-site. The Board may determine that a waiver is required as the adjacent R1 site is occupied by the Arlington Catholic High School, which is not a "buildable residential lot."

**4. EDR-4 Circulation**

**With respect to vehicular and pedestrian and bicycle circulation, including entrances, ramps, walkways, drives, and parking, special attention shall be given to location and number of access points to the public streets (especially in relation to existing traffic controls and mass transit facilities), width of interior drives and access points, general interior circulation, separation of pedestrian and vehicular traffic, access to community facilities, and arrangement of vehicle parking and bicycle parking areas, including bicycle parking spaces required by Section 6.1.12 that are safe and convenient and, insofar as practicable, do not detract from the use and enjoyment of proposed buildings and structures and the neighboring properties.**

The proposed project includes a total of 16 vehicle and 34 bicycle parking spaces (see Site Development Plan Set, Sheet C-102). The ground-level garage will provide 14 spaces for vehicles, including one van-accessible HP vehicle space. Two surface parking spaces are located adjacent to the garage. Parking access is provided via a drive aisle from Park Terrace. The proposed number of vehicle parking spaces is an increase over the existing conditions; at present the site provides 14 parking spaces, eight of which are tandem spaces blocked by the other six spaces and none of which are HP spaces.

The parking requirement is for mixed-use which calculates the parking required for each individual use; the parking required for the residential use totals 15 parking spaces, and while the commercial, business, and retail space would typically require 27 parking spaces, the first 3,000 square feet of non-residential space in mixed-use buildings is exempt from the parking requirements per Section 6.1.10.C. The applicant is requesting a reduction in the number of parking spaces provided to 16 spaces per Section 6.1.5 of the Zoning Bylaw. As such, the Applicant should submit a Transportation Demand Management Plan.

Pedestrian circulation behind the building would be improved, as two previous walkways connecting the rear of each storefront and the lobby to Park Terrace, as well as the trash and recycling area, are proposed. The sidewalk on Park Terrace should be reconstructed. Any potential improvements in the public right-of-way will require additional review and approval by the Engineering Division. Additionally, benches along Massachusetts Avenue, often used by individuals waiting at the adjacent bus stop, will remain. Planters along the Massachusetts Avenue façade and landscaping at the rear of the building will improve the human scale elements of the ground floor space.

Regarding bicycle parking, the applicant proposes both short-term and long-term bicycle parking exceeding the required minimum. Eight short-term spaces are provided off Park Terrace, one more than the minimum required. Indoor long-term bicycle parking is provided behind the lobby and accessible through the Massachusetts Avenue



entry or via a walkway behind the storefronts at 2-14 Medford Street; 26 spaces are proposed, five more than the minimum required. Bike rack specifications are needed to determine compliance with Section 6.1.12.E.

<b>Vehicle Parking Requirements*</b>			
<b><u>Number of Bedrooms/ Apartment Use</u></b>	<b><u>Number of Units</u></b>	<b><u>Zoning Requirement</u></b>	<b><u>Total Parking Required</u></b>
1-bedroom	12	1.15	14
Studio	1	1	1
<b><u>Commercial</u></b>	<b><u>Square feet</u></b>	<b><u>Zoning Requirement</u></b>	<b><u>Total Parking Required</u></b>
Service	5,377	1 per 300sf	18
Business	1,255	1 per 500sf	2.5
Retail	1,951	1 per 300sf	6.5
<b>Total Required Vehicle Parking</b>			<b>42</b>
<b>Total Proposed Vehicle Parking after Section 6.1.5 Reduction</b>			<b>32</b>
* First 3,000sf of non-residential space in mixed-use buildings is exempt.			
<b>Bicycle Parking Requirements</b>			
<b><u>Use</u></b>	<b><u>Short-Term Parking</u></b>	<b><u>Long-Term Parking</u></b>	
Residential	1.3 spaces	19.5 spaces	
Retail	5 spaces	1.1 spaces	
<b>Total Required Bicycle Parking</b>	<b>7</b>	<b>21</b>	
<b>Total Proposed Bicycle Parking</b>	<b>8</b>	<b>26</b>	

The proposed project is highly accessible by transit, bike, and walking, and since there is a reduction in commercial space, it is likely that the aggregate number of trips to this location will be reduced. The provided parking is intended for residential tenants and not for patrons of the commercial space, with commercial tenants and visitors relying on on-street parking or the Russell Common Parking Lot. As such, the actual impact of the current trips is dispersed across Massachusetts Avenue, Medford Street, Broadway Plaza, and the Russell Common Parking Lot.

The applicant is seeking relief from the drive aisle dimensions described in Section 6.1.11.C(3). The parking garage provides a 20-foot drive aisle, less than the required 24-foot aisle necessary for two-way traffic. A formalized concrete drive apron is proposed to improve access to parking. Due to the existing street network and one-way condition on Park Terrace, parking would be accessed through the Russell Common Parking Lot, and vehicles would exit the site from Park Terrace onto Medford Street. It may be appropriate for the ARB to request a trip distribution analysis to assess how people will access this site. The ARB may also wish to request additional details on how service vehicles and delivery trucks would access the site.

**5. EDR-5 Surface Water Drainage**

Special attention shall be given to proper site surface drainage so that removal of surface waters will not adversely affect neighboring properties or the public storm drainage system. Available Best Management Practices for the site should be employed, and include site planning to minimize impervious surface and reduce clearing and re-grading. Best Management Practices may include erosion control and stormwater treatment by means of swales, filters, plantings, roof gardens, native vegetation, and leaching catch basins. Stormwater should be treated at least minimally on the development site; that which cannot be handled on site shall be removed from all roofs, canopies, paved and pooling areas and carried away in an underground drainage system. Surface water in all paved areas shall be collected in intervals so that it will not obstruct the flow of vehicular or pedestrian traffic and will not create puddles in the paved areas. In accordance with Section 3.3.4., the Board may require from any Applicant, after consultation with the Director of Public Works, security satisfactory to the Board to ensure the maintenance of all stormwater facilities such as catch basins, leaching catch basins, detention basins, swales, etc. within the site. The Board may use funds provided by such security to conduct maintenance that the Applicant fails to do. The Board may adjust in its sole discretion the amount and type of financial security such that it is satisfied that the amount is sufficient to provide for any future maintenance needs.

The application materials, drainage summary letter, and site development plan show that surface water drainage will be improved through the installation of pervious pavers and an underground stormwater infiltration system that will receive reduce stormwater runoff from the site. In addition, approximately 1,008 square feet of presently impervious pavement will be replaced with landscaped areas. This is an improvement over the existing conditions. The proposed design complies with the Town's current stormwater bylaw. Final design materials must be submitted for review and approval by the Town Engineer.

**6. EDR-6 Utilities Service**

Electric, telephone, cable TV, and other such lines of equipment shall be underground. The proposed method of sanitary sewage disposal and solid waste disposal from all buildings shall be indicated.

All utility service will be provided through connections to existing utility lines adjacent to the site. The Board can find that this condition is met.

**7. EDR-7 Advertising Features**

The size, location, design, color, texture, lighting and materials of all permanent signs and outdoor advertising structures or features shall not detract from the use and enjoyment of proposed buildings and structures and the surrounding properties.

The application materials show a representative sign on one of the commercial storefronts. Any future signage would be subject to review by the Department of



Planning and Community Development, and possibly the Redevelopment Board, prior to the issuance of a sign permit. Additionally, lighting and any other potential outdoor features relative to the building should be provided.

**8. EDR-8 Special Features**

**Exposed storage areas, exposed machinery installations, service areas, truck loading areas, utility buildings and structures, and similar accessory areas and structures shall be subject to such setbacks, screen plantings or other screening methods as shall reasonably be required to prevent their being incongruous with the existing or contemplated environment and the surrounding properties.**

The roofing plan provided indicates that roof structures are appropriately set back and that a parapet and additional screening will provide screening of said structures. The site plan shows an enclosed dumpster and recycling area located adjacent to the surface parking. The existing businesses currently utilize Park Terrace for truck loading and unloading and for service deliveries. The Board can find that this condition is met.

**9. EDR-9 Safety**

**With respect to personal safety, all open and enclosed spaces shall be designed to facilitate building evacuation and maximize accessibility by fire, police and other emergency personnel and equipment. Insofar as practicable, all exterior spaces and interior public and semi-public spaces shall be so designed to minimize the fear and probability of personal harm or injury by increasing the potential surveillance by neighboring residents and passersby of any accident or attempted criminal act.**

The proposed building has been designed to meet all relevant health and safety codes. A lighting plan was not provided as part of the plan set. These details are needed to assess safety criteria and compliance.

**10. EDR-10 Heritage**

**With respect to Arlington's heritage, removal or disruption of historic, traditional or significant uses, structures or architectural elements shall be minimized insofar as practical whether these exist on the site or on adjacent properties.**

While both structures are part of the Arlington Center Historic District area, only 4-14 Medford Street is a contributing historic structure as identified on the *Inventory of Historically or Architecturally Significant Properties in the Town of Arlington*. Both structures are under the jurisdiction of the Arlington Historical Commission, which will need to review the building design.

**11. EDR-11 Microclimate**

**With respect to the localized climatic characteristics of a given area, any development which proposes new structures, new hard surface, ground coverage or the installation of machinery which emits heat, vapor or fumes shall endeavor to**

**minimize insofar as practicable, any adverse impacts on light, air and water resources or on noise and temperature levels of the immediate environment.**

Based upon materials provided in the application, there will be no adverse impacts on air and water resources or on temperature levels of the immediate environment. While the applicant states that they intend to minimize any adverse impact, additional plan details are needed regarding lighting and emissions from machinery located on the roof to determine any impacts on the immediate environment.

#### **12. EDR-12 Sustainable Building and Site Design**

**Projects are encouraged to incorporate best practices related to sustainable sites, water efficiency, energy and atmosphere, materials and resources, and indoor environmental quality. Applicants must submit a current Green Building Council Leadership in Energy and Environmental Design (LEED) checklist, appropriate to the type of development, annotated with narrative description that indicates how the LEED performance objectives will be incorporated into the project.**

The proposed building generates a LEED score that demonstrates the building could qualify for LEED platinum certification. The Board can find that this condition is met.

### **IV. Findings**

The following findings are for the Board's consideration:

1. The ARB finds that the project is consistent with Environmental Design Review per Section 3.4 of the Zoning Bylaw.
2. The ARB finds that the setbacks on Lake Street and Chandler Street are appropriate per Section 5.3.16.
3. The ARB finds that the two-story building will not adversely affect the adjacent R1 zoning districts per Section 5.3.19.
4. The ARB finds that the vehicle and bicycle parking improvements justify the parking reduction per Section 6.1.5.

### **V. Conditions**

#### **A. General**

1. The final design, sign, exterior material, landscaping, and lighting plans shall be subject to the approval of the Arlington Redevelopment Board or administratively approved by the Department of Planning and Community Development. Any substantial or material deviation during construction from the approved plans and specifications is subject to the written approval of the Arlington Redevelopment Board.



2. Any substantial or material deviation during construction from the approved plans and specifications is subject to the written approval of the Arlington Redevelopment Board.
3. The Board maintains continuing jurisdiction over this permit and may, after a duly advertised public hearing, attach other conditions or modify these conditions as it deems appropriate in order to protect the public interest and welfare.
4. Snow removal from all parts of the site, as well as from any abutting public sidewalks, shall be the responsibility of the owner and shall be accomplished in accordance with Town Bylaws.
5. Trash shall be picked up only on Monday through Friday between the hours of 7:00 am and 6:00 pm. All exterior trash and storage areas on the property, if any, shall be properly screened and maintained in accordance with Article 30 of Town Bylaws.
6. The Applicant shall provide a statement from the Town Engineer that all proposed utility services have adequate capacity to serve the development. The applicant shall provide evidence that a final plan for drainage and surface water removal has been reviewed and approved by the Town Engineer.
7. Upon installation of landscaping materials and other site improvements, the Applicant shall remain responsible for such materials and improvement and shall replace and repair as necessary to remain in compliance with the approved site plan.
8. All utilities serving or traversing the site (including electric, telephone, cable, and other such lines and equipment) shall be underground.
9. Upon the issuance of the building permit the Applicant shall file with the Building Inspector and the Department of Community Safety the names and telephone numbers of contact personnel who may be reached 24 hours each day during the construction period.
10. Building signage will be filed with and reviewed and approved by the Department of Planning and Community Development and Inspectional Services.

## **B. Special Conditions**

1. The owner will work with the Department of Planning and Community Development to comply with all requirements of Section 8.2, Affordable Housing Requirements.

2. The affordable units must be equitably dispersed throughout the building and shall be comparable to market-rate units in terms of location, quality and character, room size, number of rooms, number of bedrooms, and external appearance.
3. An Affordable Housing Deed Restriction shall be executed with the Town prior to issuance of an Occupancy Permit for the two affordable units.
4. No condominium conversion of said affordable rental units shall be permitted without the express permission of this Board. In the case of a proposed condominium conversion, Applicant shall work with the Department of Planning and Community Development to ensure that the units continue to meet the requirements of Section 8.2.





## TOWN OF ARLINGTON

DEPARTMENT OF PLANNING and  
COMMUNITY DEVELOPMENT

TOWN HALL, 730 MASSACHUSETTS AVENUE  
ARLINGTON, MASSACHUSETTS 02476  
TELEPHONE 781-316-3090

### M E M O R A N D U M

To: Jennifer Raitt, Director, Department of Planning and Community Development  
From: Ali Carter, Economic Development Coordinator  
Date: September 23, 2021  
Re: History of Commercial Vacancies at 455 Mass Ave

At Special Town Meeting in the fall of 2016, Town Meeting members voted to institute a bylaw that required owners of commercial and industrial properties in Arlington to register any vacant units in their buildings after 21 days of vacancy. In 2018, the bylaw was revised by Town Meeting to extend the allowed vacancy period to 90 days. Property owners with units vacant for 90 days or more are required to register the vacant unit with the Town and to pay a \$400 registration fee. Failure to register within 30 days of notification results in a fine of \$100 per day. Waivers may be granted at the joint discretion of the Director of Planning and Community Development (DPCD) and the Director of Inspectional Services if property owners can make a valid claim of financial hardship or if they are willing to activate their vacant storefront with public art or a temporary "pop-up" use.

To enforce this bylaw, the DPCD has tracked commercial and industrial vacancies in Arlington carefully since December 2016 when the bylaw was formally approved by the Attorney General's office. When the COVID-19 pandemic began, the enforcement of the bylaw was temporarily suspended by the Select Board under the State of Emergency issued by the Commonwealth of Massachusetts on March 10, 2020. Enforcement of this bylaw was reinstated 90 days after the end of the State of Emergency, which was on September 13, 2021.

There are currently three commercial vacancies in the property at 455 Massachusetts Avenue in Arlington Center.

1. **457 Massachusetts Avenue** was last occupied by Papa Gino's. It has been vacant since November of 2018, and registration was required in February of 2019. The property received a one-year waiver of the vacant storefront bylaw registration fee for display of public art in the storefront windows. Shortly after this waiver expired and further action was to be taken, the pandemic hit, and the enforcement of the bylaw was suspended. On September 15, 2021, DPCD sent a letter to 2-14 Medford Street LLC, the owner of the property, notifying them to register this and one other unit within the building for the vacant storefront registry.
2. **455 Massachusetts Avenue** was last occupied by Abilyn's Frozen Bakery. It has been vacant since October 2019. The property owner, 455 Mass Ave, was required to register the property by January 2020. The suspension of the bylaw enforcement in March 2020 due to the COVID-19 State of Emergency extended the allowed vacancy period, and the owner would not have been required to re-register the vacant unit in September 2021. However, in the proposal currently before the ARB, this space is intended to be repurposed into a lobby for the second story

residential units. Therefore, the owner has not been asked to add this unit to the vacant storefront registry at this time.

3. **8 Medford Street** was last occupied by the Artful Heart Gallery. It has been vacant since May 2021. On September 15, 2021, DPCD sent a letter to 2-14 Medford Street LLC, the owner of the property, notifying them to register this and one other unit within the building for the vacant storefront registry.



# TOWN OF ARLINGTON

TOWN CLERK

Date August 30, 20 21

Rec'd from 2-14 Medford Street, LLC

Address 455 Massachusetts Avenue, Suite 1  
Arlington, MA 02474

By jaw

		AMOUNT	
	Certificate		
	Marriage Int.		
	Fin. Statement		
	Pole Location		
	Garage Renewal		
	Misc. Books		
	Sporting Licenses		
	Dup. Dog Tags		
	Dog Licenses		
	Citation #		
<u>1</u>	<u>Misc. Licenses / Permits EDR</u>	<u>4,510.</u>	<u>00</u>

Leader Bank check #1222

29956

RV PRINT SOLUTIONS 603-580-4687 33268277

LB 1/17

Docket 3673



TOWN OF ARLINGTON  
REDEVELOPMENT BOARD

Application for Special Permit In Accordance with Environmental Design  
Review Procedures (Section 3.4 of the Zoning Bylaw)

PLANNING & COMMUNITY  
DEVELOPMENT

2021 AUG 27 AM 11:06

Docket No. 3673

1. Property Address 455-457 Massachusetts Avenue & 2-14 Medford Street  
Name of Record Owner(s) 2-14 Medford Street, LLC Phone 781-654-6306  
Address of Owner 455 Massachusetts Avenue, Ste 1, Arlington, MA 02474  
Street City, State, Zip

2. Name of Applicant(s) (if different than above) Same as above  
Address \_\_\_\_\_ Phone \_\_\_\_\_  
Status Relative to Property (occupant, purchaser, etc.) \_\_\_\_\_

3. Location of Property Map 45, Lots 4-5.a  
Assessor's Block Plan, Block, Lot No.

4. Deed recorded in the Registry of deeds, Book \_\_\_\_\_, Page \_\_\_\_\_;  
-or- registered in Land Registration Office, Cert. No. 7802C, in Book 1523, Page 99

5. Present Use of Property (include # of dwelling units, if any) Retail, Service, Restaurant

6. Proposed Use of Property (include # of dwelling units, if any) Mixed-Use  
Apartment Units & Retail,  
Service, Restaurant

7. Permit applied for in accordance with \_\_\_\_\_ See Exhibit "A" attached  
the following Zoning Bylaw section(s) \_\_\_\_\_  
\_\_\_\_\_ section(s) title(s)  
\_\_\_\_\_

8. Please attach a statement that describes your project and provide any additional information that may aid the ARB in understanding the permits you request. Include any reasons that you feel you should be granted the requested permission.  
See attached

(In the statement below, strike out the words that do not apply)  
The applicant states that 2-14 Medford Street, LLC is the owner -or- occupant -or- purchaser under agreement of the property in Arlington located at 455-457 Massachusetts Avenue & 2-14 Medford Street which is the subject of this application; and that unfavorable action -or- no unfavorable action has been taken by the Zoning Board of Appeals on a similar application regarding this property within the last two years. The applicant expressly agrees to comply with any and all conditions and qualifications imposed upon this permission, either by the Zoning Bylaw or by the Redevelopment Board, should the permit be granted.

Signature of Applicant(s)

1171 Massachusetts Avenue, Arlington, MA 02476 781-646-4911

Address

Phone



**Exhibit A to Application for Special Permit  
in Accordance with Environmental Design Review**

455-457 Massachusetts Avenue

Arlington, MA

7. Permit applied for in accordance with the following Zoning Bylaw section(s):
- 3.4 Environmental Design Review
  - 5.5.2 Dimensional and Density Regulations
  - SP (Mixed Use  $\leq 20,000$  SF)
  - 6.12.5 Parking Reduction in a Business Zone
  - 6.1.11 Parking Standards
  - 5.3.21 Screening and Buffer Requirements
  - 5.3.16 Setback Requirements
  - 5.3.19 Reduced Height Buffer Area
  - 5.3.8 Corner Lot Requirements
  - 5.2.4 Multiple Principal Uses



Town of Arlington Redevelopment Board  
Application for Special Permit in accordance with  
Environmental Design Review (Section 3.4)

Required Submittals Checklist

Two full sets of materials and one electronic copy are required. A model may be requested. Review the ARB's Rules and Regulations, which can be found at [arlingtonma.gov/arb](http://arlingtonma.gov/arb), for the full list of required submittals.

- ☒ Dimensional and Parking Information Form (see attached)
- ☒ Site plan of proposal
- ☐ Model, if required
- ☒ Drawing of existing conditions
- ☒ Drawing of proposed structure
- ☒ Proposed landscaping. May be incorporated into site plan
- ☒ Photographs
- ☒ Impact statement
- ☐ Application and plans for sign permits
- ☒ Stormwater management plan (for stormwater management during construction for projects with new construction)

FOR OFFICE USE ONLY

_____ Special Permit Granted	Date: _____
_____ Received evidence of filing with Registry of Deeds	Date: _____
_____ Notified Building Inspector of Special Permit filing	Date: _____



## TOWN OF ARLINGTON REDEVELOPMENT BOARD

### Petition for Special Permit under Environmental Design Review (see Section 3.4 of the Arlington Zoning Bylaw for Applicability)

For projects subject to Environmental Design Review, (see Section 3.4), please submit a statement that completely describes your proposal, and addresses each of the following standards.

1. **Preservation of Landscape.** The landscape shall be preserved in its natural state, insofar as practicable, by minimizing tree and soil removal, and any grade changes shall be in keeping with the general appearance of neighboring developed areas.
2. **Relation of Buildings to Environment.** Proposed development shall be related harmoniously to the terrain and to the use, scale, and architecture of existing buildings in the vicinity that have functional or visual relationship to the proposed buildings. The Arlington Redevelopment Board may require a modification in massing to reduce the effect of shadows on abutting property in an R0, R1 or R2 district or on public open space.
3. **Open Space.** All open space (landscaped and usable) shall be so designed as to add to the visual amenities of the vicinity by maximizing its visibility for persons passing the site or overlooking it from nearby properties. The location and configuration of usable open space shall be so designed as to encourage social interaction, maximize its utility, and facilitate maintenance.
4. **Circulation.** With respect to vehicular, pedestrian and bicycle circulation, including entrances, ramps, walkways, drives, and parking, special attention shall be given to location and number of access points to the public streets (especially in relation to existing traffic controls and mass transit facilities), width of interior drives and access points, general interior circulation, separation of pedestrian and vehicular traffic, access to community facilities, and arrangement of vehicle parking and bicycle parking areas, including bicycle parking spaces required by Section 8.13 that are safe and convenient and, insofar as practicable, do not detract from the use and enjoyment of proposed buildings and structures and the neighboring properties.
5. **Surface Water Drainage.** Special attention shall be given to proper site surface drainage so that removal of surface waters will not adversely affect neighboring properties or the public storm drainage system. Available Best Management Practices for the site should be employed, and include site planning to minimize impervious surface and reduce clearing and re-grading. Best Management Practices may include erosion control and storm water treatment by means of swales, filters, plantings, roof gardens, native vegetation, and leaching catch basins. Storm water should be treated at least minimally on the development site; that which cannot be handled on site shall be removed from all roofs, canopies, paved and pooling areas and carried away in an underground drainage system. Surface water in all paved areas shall be collected at intervals so that it will not obstruct the flow of vehicular or pedestrian traffic, and will not create puddles in the paved areas.

In accordance with Section 3.3.4, the Board may require from any applicant, after consultation with the Director of Public Works, security satisfactory to the Board to insure the maintenance of all storm water facilities such as catch basins, leaching catch basins, detention basins, swales, etc. within the site. The Board may use funds provided by such security to conduct maintenance that the applicant fails to do. The Board may adjust in its sole discretion the amount and type of financial security such that it is satisfied that the amount is sufficient to provide for the future maintenance needs.

6. **Utility Service.** Electric, telephone, cable TV and other such lines and equipment shall be underground. The proposed method of sanitary sewage disposal and solid waste disposal from all buildings shall be indicated.
7. **Advertising Features.** The size, location, design, color, texture, lighting and materials of all permanent signs and outdoor advertising structures or features shall not detract from the use and enjoyment of proposed buildings and structures and the surrounding properties. Advertising features are subject to the provisions of Section 6.2 of the Zoning Bylaw.

8. **Special Features.** Exposed storage areas, exposed machinery installations, service areas, truck loading areas, utility buildings and structures, and similar accessory areas and structures shall be subject to such setbacks, screen plantings or other screening methods as shall reasonably be required to prevent their being incongruous with the existing or contemplated environment and the surrounding properties.
9. **Safety.** With respect to personal safety, all open and enclosed spaces shall be designed to facilitate building evacuation and maximize accessibility by fire, police, and other emergency personnel and equipment. Insofar as practicable, all exterior spaces and interior public and semi-public spaces shall be so designed as to minimize the fear and probability of personal harm or injury by increasing the potential surveillance by neighboring residents and passersby of any accident or attempted criminal act.
10. **Heritage.** With respect to Arlington's heritage, removal or disruption of historic, traditional or significant uses, structures, or architectural elements shall be minimized insofar as practicable, whether these exist on the site or on adjacent properties.
11. **Microclimate.** With respect to the localized climatic characteristics of a given area, any development which proposes new structures, new hard-surface ground coverage, or the installation of machinery which emits heat, vapor, or fumes, shall endeavor to minimize, insofar as practicable, any adverse impact on light, air, and water resources, or on noise and temperature levels of the immediate environment.
12. **Sustainable Building and Site Design.** Projects are encouraged to incorporate best practices related to sustainable sites, water efficiency, energy and atmosphere, materials and resources, and indoor environmental quality. Applicants must submit a current Green Building Council Leadership in Energy and Environmental Design (LEED) checklist, appropriate to the type of development, annotated with narrative description that indicates how the LEED performance objectives will be incorporated into the project. [LEED checklists can be found at <http://www.usgbc.org/DisplayPage.aspx?CMSPageID=220b> ]

In addition, projects subject to Environmental Design Review must address and meet the following Special Permit Criteria (see Section 3.3.3 of the Zoning Bylaw):

1. The use requested is listed as a special permit in the use regulations for the applicable district or is so designated elsewhere in this Bylaw.
2. The requested use is essential or desirable to the public convenience or welfare.
3. The requested use will not create undue traffic congestion or unduly impair pedestrian safety.
4. The requested use will not overload any public water, drainage or sewer system or any other municipal system to such an extent that the requested use or any developed use in the immediate area or in any other area of the Town will be unduly subjected to hazards affecting health, safety or the general welfare.
5. Any special regulations for the use as may be provided in this Bylaw are fulfilled.
6. The requested use will not impair the integrity or character of the district or adjoining districts, nor be detrimental to the health, morals, or welfare.
7. The requested use will not, by its addition to a neighborhood, cause an excess of the particular use that could be detrimental to the character of said neighborhood.



# TOWN OF ARLINGTON

Dimensional and Parking Information  
for Application to  
The Arlington Redevelopment Board

Docket No. \_\_\_\_\_

Property Location 455-457 Massachusetts Ave

Zoning District B3

Owner: 2-14 Medford Street, LLC

Address: 455 Massachusetts Ave, Arlington, MA

Present Use/Occupancy: No. of Dwelling Units:

Retail, Service, Restaurant

Uses and their gross square feet:

2-Story 18,588 GSF

Proposed Use/Occupancy: No. of Dwelling Units:

Uses and their gross square feet:

Mixed-Use, 13 Apartment Units & Retail, Service, Restaurant 2-Story Mixed-Use, 28,373 GSF

	Present Conditions	Proposed Conditions	Min. or Max. Required by Zoning for Proposed Use
Lot Size	18,929 SF	18,929 SF	min. ----
Frontage	124.8 FT	124.8 FT	min. 50 FT
Floor Area Ratio	0.98	1.5	max. 1.5
Lot Coverage (%), where applicable	N/A	N/A	max. ----
Lot Area per Dwelling Unit (square feet)	N/A	N/A	min. ----
Front Yard Depth (feet)	0 FT	0 FT	min. 0 FT
Side Yard Width (feet) right side	----	----	min. ----
left side	0 FT	0 FT	min. 0 FT
Rear Yard Depth (feet)	0.6 FT	0.6 FT	min. (H+L)/6
Height	----	----	min. ----
Stories	2-STORY	2-STORY	stories 5-STORY
Feet	<35 FT	<35 FT	feet 60 FT
Open Space (% of G.F.A.)	----	----	min. ----
Landscaped (square feet)	0 %	24.1 %	(s.f.) 10% 1,021 SF
Usable (square feet)	0 %	0 %	(s.f.) 20% 2,042 SF
Parking Spaces (No.)	14*	16	min. 31.9
Parking Area Setbacks (feet), where applicable	N/A	N/A	min. N/A
Loading Spaces (No.)	N/A	N/A	min. N/A
Type of Construction	NEW CONSTRUCTION		
Distance to Nearest Building	0 FT	0 FT	min.

2,457 SF/10,211SF  
(Res. Floor Area)

10,211 SF (Res. Floor Area) X 0.10 = 1,021 SF

10,211 SF (Res. Floor Area) X 0.20 = 2,042 SF

\*8 SPACES FROM THE EXSITING  
14 SPACES ARE TANDOM SPACES,  
BLOCKED IN BY THE OTHER 6.

Updated August 28, 2018  
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## **TOWN OF ARLINGTON REDEVELOPMENT BOARD**

Petition for Special Permit under Environmental Design Review (see Section 3.4 of the Arlington Zoning Bylaw for Applicability)

For projects subject to Environmental Design Review, (see Section 3.4), please submit a statement that completely describes your proposal, and addresses each of the following standards.

1. Preservation of Landscape. The landscape shall be preserved in its natural state, insofar as practicable, by minimizing tree and soil removal, and any grade changes shall be in keeping with the general appearance of neighboring developed areas.

**There is no existing landscaping on-site and the proposed design will introduce an arborvitae row, shrubs, and Serviceberry tree along Park Terrace, See Allen & Major Associates, Inc planting schedule – trees, shrubs, groundcovers, and perennials dated August 20, 2021, Landscape Plan No. L-101 as part of the Applicant's submission. The existing street trees along Mass Ave will be protected and maintained. Also, the proposed design has been graded to closely match the existing building.**

2. Relation of Buildings to Environment. Proposed development shall be related harmoniously to the terrain and to the use, scale, and architecture of existing buildings in the vicinity that have functional or visual relationship to the proposed buildings. The Arlington Redevelopment Board may require a modification in massing to reduce the effect of shadows on abutting property in an RO, R1 or R2 district or on public open space.

**The proposed construction has been developed in a manner consistent with the existing building structures at the site and to the use, scale, and architecture of existing building in the vicinity of the property that have functional or visual relationship to the proposed construction. No part of the proposed structure is higher than the existing structure. Shadows from the proposed structure will not have an adverse impact on any Residential (R) use.**

3. Open Space. All open space (landscaped and usable) shall be so designed as to add to the visual amenities of the vicinity by maximizing its visibility for persons passing the site or overlooking it from nearby properties. The location and configuration of usable open space shall be so designed as to encourage social interaction, maximize its utility, and facilitate maintenance.

**The existing site has no open space. The Applicant proposes to add 2,457 square feet of landscaped open space. This area includes planted areas and pervious**



**paver systems. This area has been calculated to 24.1% of the residential floor area.**

4. Circulation. With respect to vehicular, pedestrian and bicycle circulation, including entrances, ramps, walkways, drives, and parking, special attention shall be given to location and number of access points to the public streets (especially in relation to existing traffic controls and mass transit facilities), width of interior drives and access points, general interior circulation, separation of pedestrian and vehicular traffic, access to community facilities, and arrangement of vehicle parking and bicycle parking areas, including bicycle parking spaces required by Section 8.13 that are safe and convenient and, insofar as practicable, do not detract from the use and enjoyment of proposed buildings and structures and the neighboring properties.

**The existing parking situation is not functional. 8 spaces from the existing 14 spaces are tandem spaces blocked in by other 6 spaces. The proposed layout provides a functional 16 spaces.**

5. Surface Water Drainage. Special attention shall be given to proper site surface drainage so that removal of surface waters will not adversely affect neighboring properties or the public storm drainage system. Available Best Management Practices for the site should be employed and include site planning to minimize impervious surface and reduce clearing and re-grading. Best Management Practices may include erosion control and storm water treatment by means of swales, filters, plantings, roof gardens, native vegetation, and leaching catch basins. Storm water should be treated at least minimally on the development site; that which cannot be handled on site shall be removed from all roofs, canopies, paved and pooling areas and carried away in an underground drainage system. Surface water in all paved areas shall be collected at intervals so that it will not obstruct the flow of vehicular or pedestrian traffic and will not create puddles in the paved areas.

**Surface water drainage at the site will be improved with the installation of pervious paver systems and an underground infiltration system. These systems will receive clean roof runoff for storm water recharge and greatly reduce the quantity of stormwater runoff from the parcel.**

**The proposed work will also result in approximately 1,008 square feet of impervious material being replaced with landscaped areas. See Allen & Major Associates, Inc. letter to the Director of Planning & Community Development dated August 20, 2021.**

In accordance with Section 3.3.4, the Board may require from any applicant, after consultation with the Director of Public Works, security satisfactory to the Board to insure the maintenance of all storm water facilities such as catch basins, leaching catch basins, detention basins, swales, etc. within the site. The Board may use funds provided by such security to conduct maintenance that the applicant fails to do. The Board may

adjust in its sole discretion the amount and type of financial security such that it is satisfied that the amount is sufficient to provide for the future maintenance needs.

6. Utility Service. Electric, telephone, cable TV and other such lines and equipment shall be underground. The proposed method of sanitary sewage disposal and solid waste disposal from all buildings shall be indicated.

**Utility service would be through a tie into existing utility lines at the site.**

7. Advertising Features. The size, location, design, color, texture, lighting and materials of all permanent signs and outdoor advertising structures or features shall not detract from the use and enjoyment of proposed buildings and structure and the surrounding properties. Advertising features are subject to the provisions of Section 6.2 of the Zoning Bylaw.

**Any proposed relief with respect to advertising signage will be initially through the Planning Department for the purpose of determining whether any signage issues can be handled administratively or whether those issues need to go before the ARB.**

8. Special Features. Exposed storage areas, exposed machinery installations, service areas, truck loading areas, utility buildings and structures, and similar accessory areas and structures shall be subject to such setbacks, screen plantings or other screening methods as shall reasonably be required to prevent their being incongruous with the existing or contemplated environment and the surrounding properties.

9. Safety. With respect to personal safety, all open and enclosed spaces shall be designed to facilitate building evacuation and maximize accessibility by fire, police, and other emergency personnel and equipment. Insofar as practicable, all exterior spaces and interior public and semi-public spaces shall be so designed as to minimize the fear and probability of personal harm or injury by increasing the potential surveillance by neighboring residents and passersby of any accident or attempted criminal act.

**With respect to personal safety all open and enclosed spaces have been designed to facilitate building evacuation and maximize accessibility by fire, police, and other emergency personnel and equipment. All spaces have been designed to comply with applicable codes and ordinances.**

10. Heritage. With respect to Arlington's heritage, removal, or disruption of historic, traditional, or significant uses, structures, or architectural elements shall be minimized insofar as practicable, whether these exist on the site or on adjacent properties.

**The Applicant understands that it will need to appear before the Arlington Historical Commission with respect to its plans as both structures i.e., the Massachusetts Avenue structure and the 4-14 Medford Street structure are part of**



**the Arlington Center Historic District, although only the 4-14 Medford Street structure is a contributing historic structure.**

11. Microclimate. With respect to the localized climatic characteristics of a given area, any development which proposes new structures, new hard-surface ground coverage, or the installation of machinery which emits heat, vapor, or fumes, shall endeavor to minimize, insofar as practicable, any adverse impact on light, air, and water resources, or on noise and temperature levels of the immediate environment.

**Adequate provision has been made for any emission of heat vapor fumes from the property so as to endeavor to minimize, insofar as practicable, any adverse impact on light, air and water resources, or noise and temperature levels of the immediate environment.**

12. Sustainable Building and Site Design. Projects are encouraged to incorporate best practices related to sustainable sites, water efficiency, energy and atmosphere, materials and resources, and indoor environmental quality. Applicants must submit a current Green Building Council Leadership in Energy and Environmental Design (LEED) checklist, appropriate to the type of development, annotated with narrative description that indicates how the LEED performance objectives will be incorporated into the project. [LEED checklists can be found at <http://www.usgbc.org/DisplayPage.aspx?CMSPageID=220b>]

**See LEED submission for the project dated August 11, 2021, as part of the Applicant's submission.**

**In addition, projects subject to Environmental Design Review must address and meet the following Special Permit Criteria (see Section 3.3.3 of the Zoning Bylaw):**

1. The use requested is listed as a special permit in the use regulations for the applicable district or is so designated elsewhere in this Bylaw.

**The use requested is listed as a Special Permit in the use regulations for the applicable district. See Section 5.5.2 (a) Districts and Uses contained within the Zoning Bylaw.**

2. The requested use is essential or desirable to the public convenience or welfare.

**The requested use is essential or desirable to the public convenience or welfare because a portion of the property has been vacant for a substantial period of time within Arlington Center and the proposal seeks to add a restaurant use as well as residential use to the site on mixed-use basis which comports with the provisions of the Master Plan.**

3. The requested use will not create undue traffic congestion or unduly impair pedestrian safety.

**The requested use will not create undue traffic congestion or unduly impair pedestrian safety as indicated within the substance of the Applicant's plans.**

4. The requested use will not overload any public water, drainage or sewer system or any other municipal system to such an extent that the requested use or any developed use in the immediate area or in any other area of the Town will be unduly subjected to hazards affecting health, safety, or the general welfare.

**The requested use will not overload any public water, drainage or sewer system or any municipal system.**

5. Any special regulations for the use as may be provided in this Bylaw are fulfilled.

**Any special regulations for the use as may be provided for in the Bylaw are fulfilled.**

6. The requested use will not impair the integrity or character of the district or adjoining districts, nor be detrimental to the health, morals, or welfare.

**The requested use will not impair the integrity or character of the district or adjoining districts, nor be detrimental to the health, morals, or welfare, but rather it will enhance the integrity and character of the district and adjoining districts because of the addition of a restaurant use as well as more residential use which again comports with the substance Master Plan and the design of the changes to**



**the existing building do not result in massing issues but rather is compatible with the physical characteristics of other buildings in the neighborhood of the property.**

7. The requested use will not, by its addition to a neighborhood, cause an excess of the particular use that could be detrimental to the character of said neighborhood.

**The requested use will not, by its addition to a neighborhood, cause an excess of that use which could be detrimental to the character of said neighborhood because the requested use will enhance the character of the neighborhood because of the nature and type of the construction which will fit in nicely with adjoining structures in the district.**

455-457 Massachusetts Avenue  
Arlington, MA

**Environmental Impact Statement**

The Petitioner is requesting a Special Permit for a mixed-use development project at 455-457 Massachusetts Avenue and 4-14 Medford Street.

The site is located on the corner of Medford Street and Massachusetts Avenue in a B3 zone and there is an existing curb cut to the parcel located off of Park Terrace to the rear of the parcel.

The lot size is 18,929 square feet and the property has frontage of 124.8 feet on both Massachusetts Avenue and Medford Street while zoning requires 50 feet.

The changes to the building will result in a mixed-use building containing thirteen (13) apartment units with a retail, service and restaurant component with the gross square feet of the mixed-use building containing 28,373 square feet.

The floor area ratio is presently 0.98 and the proposed FAR is 1.5 and the requirement for zoning is 1.5.

There is a zero-foot front yard setback and there is zero right and left side setback.

The rear yard depth is 0.6 feet, and the proposed rear yard depth will also be 0.6 feet.

The current height of the building is a two-story building, and the proposed height would also be a two-story building while zoning in the B3 zone would allow a five-story height.

The height in feet of the existing building is 35 feet and the proposed height would also be 35 feet while zoning would allow 60 feet.

There is presently no open space at the site and the Petitioner's plans propose to add 2,457 square feet of landscaped open space including planted areas and pervious paver systems which would result in the landscaped open space having a percentage of 24.1% of the residential floor area.

There is no existing usable open space and there is no ability to create usable open space and it would be the Petitioner's position that the existing building is non-conforming with respect to the usable open space requirement contained in the zoning



bylaw and the proposed changes to the building do not increase the non-conformity to the extent that the non-conformity with respect to usable open space is extinguished.

There are presently fourteen (14) parking spaces at the site and sixteen (16) are proposed while zoning requires 30.19 parking spaces. Petitioner will be seeking zoning relief with respect to Section 6.1.5 (c) of the zoning bylaw with regard to a reduction in the parking requirement in accordance with the transportation demand management provisions of the zoning bylaw.

Waivers are being requested with respect Section 6.1.11 – Parking Standards - Petitioner is requesting that the drive aisle width requirement be reduced from 24 feet to 20 feet as well as a reduction in the amount of parking spaces required and the manner of which parking will occur on the site.

A waiver is also being requested in connection with 5.3.21 – the screening and buffering requirements contained in the bylaw as the Petitioner's proposed plans do not allow for the required buffer along the rear of the lot abutting the R1 zone.

A waiver is also being requested in connection with Section 5.3.16 – Setback Requirements – which section gives the ARB the authority to grant a special permit to adjust the required setbacks contained in the bylaw to account for specific conditions unique to the proposal.

It is Petitioner's position that the existing configuration of the lot and the plans to not change the character of the site significantly would represent conditions unique to the proposal allowing the ARB to grant relief with respect to this section of the bylaw.

Relief is also sought from the provisions of Section 5.3.19, i.e., reduced height buffer. It is the Petitioner's position that if this waiver is required, it may already be complying with its provisions as its plans will have no adverse effect on abutting uses (school, church) in the R1 district.

Relief is also sought from Section 3.3.8, the corner lot requirement contained in the bylaw.

Relief would also be sought, if necessary, from the provisions of Section 5.2.4, the multiple principal use section of the zoning bylaw as there will be two uses with respect to the property, i.e., a commercial use as well as a residential use.

Petitioner is providing covered bicycle parking and storage and is prepared to provide bicycle sharing on site and would be open to other means acceptable to the ARB with respect to satisfying the provisions of the transportation demand management provisions of the zoning bylaw.

It is the Petitioner's position that its proposal if approved by the ARB will result in a welcome addition to the heart of Arlington Center, large portions of which have fallen into disuse over many years as the design will add thirteen (13) residential apartments which could be a step toward enlivening retail activity in Arlington Center, an element which has been sorely missed for many years.

It is also the Petitioner's position that its planned design will have no adverse environmental impact upon the neighborhood in which the property is located but rather will result in an improvement to the neighborhood by adding a mix of residential and commercial uses at the site comporting with objectives set forth within the substance of Master Plan.



MARKET  
SQUARE

56 of 347  
ARCHITECTS

104 Congress St., STE 204  
Perth Amboy, NJ 08861  
TEL: 609.511.1137





Title: EXISTING SECOND FLOOR PLAN  
 AE.02

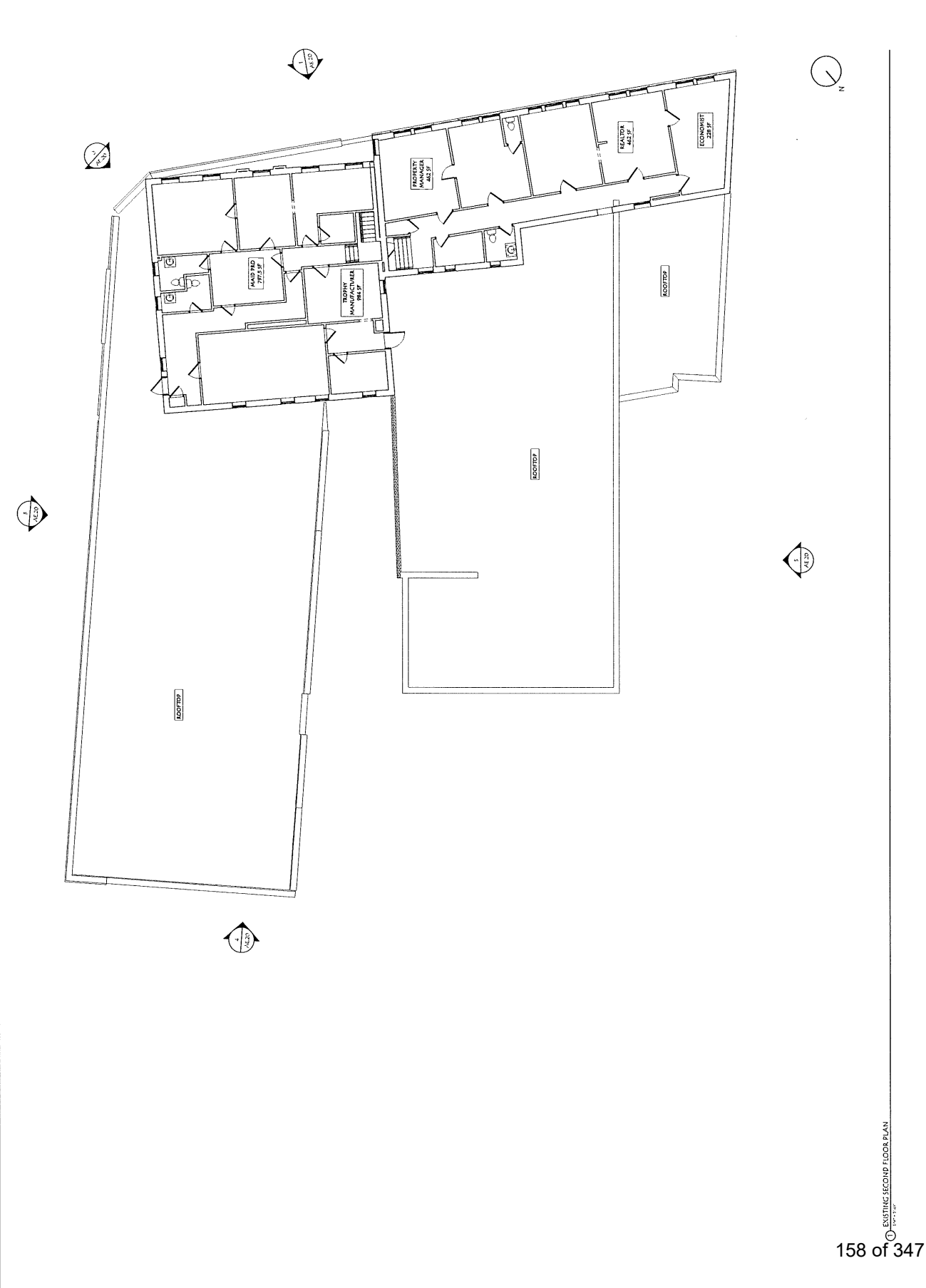
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 Project No: 202035

Revisions	Date
1/8" = 1'-0"	07/27/20
VLA, WCB	202035
Checker	
Project No	
Date	

455 MASSACHUSETTS AVENUE  
 455 MASSACHUSETTS AVENUE  
 ARLINGTON, MA

**NOT FOR CONSTRUCTION**

**SCOTT ARCHITECTS**  
 100 Commonwealth Ave., 18th Fl.  
 Boston, MA 02118  
 Tel: 617.452.1000



OVERALL  
FOUNDATION  
FLOOR PLAN  
A1.00

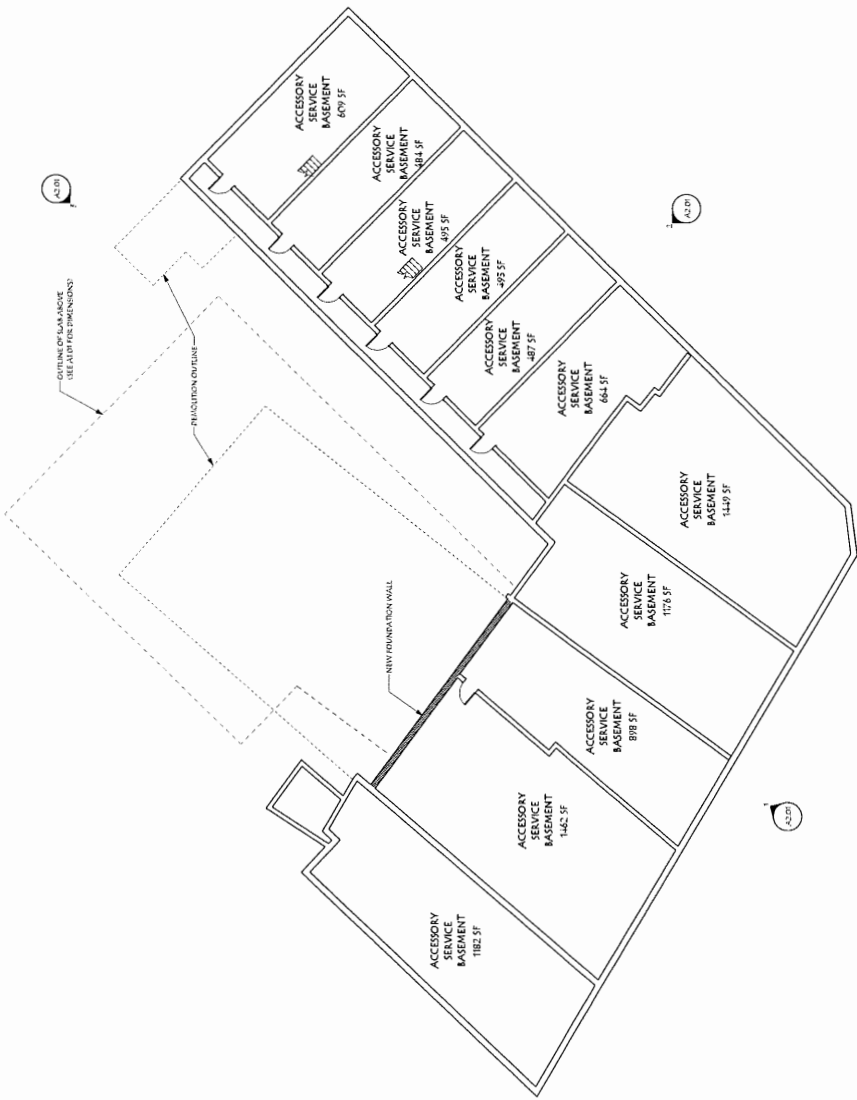
Date: 08/20/21  
Project No: 2000031  
Checked By: ALW  
Drawn By: PPS  
As Indicated  
Revisions: 1

NOT FOR  
CONSTRUCTION

ARLINGTON MIXED USE  
455 MASS AVE  
ARLINGTON, MA



KEY  
EXISTING TO REMAIN  
NEW CONSTRUCTION



1 BASEMENT  
SCALE 3/32" = 1'-0"



OVERALL FIRST FLOOR PLAN

A1.01

Title: Scale: Date: 08/20/21

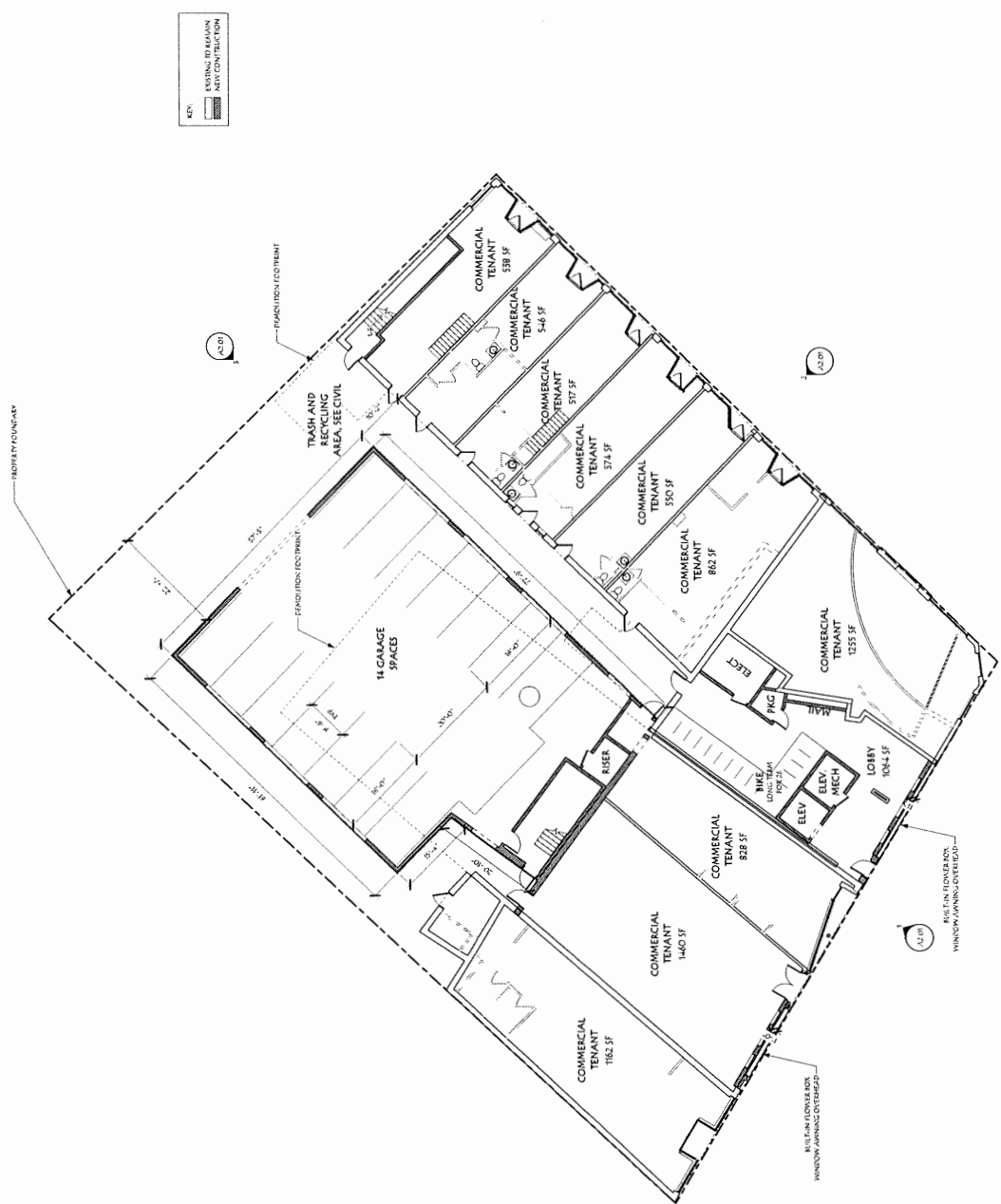
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Drawn By: ALW

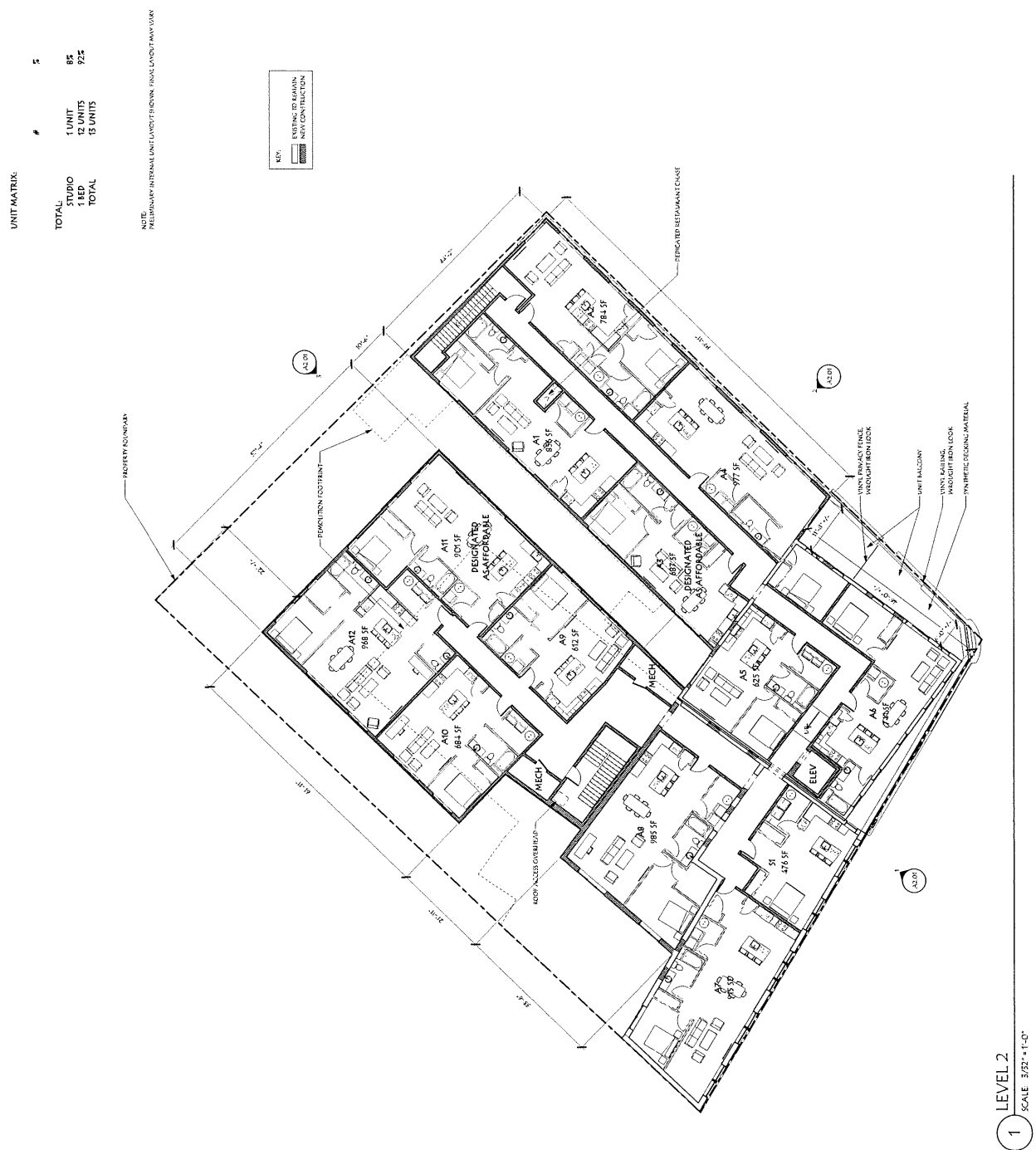
Noted By: ALW

NOT FOR CONSTRUCTION

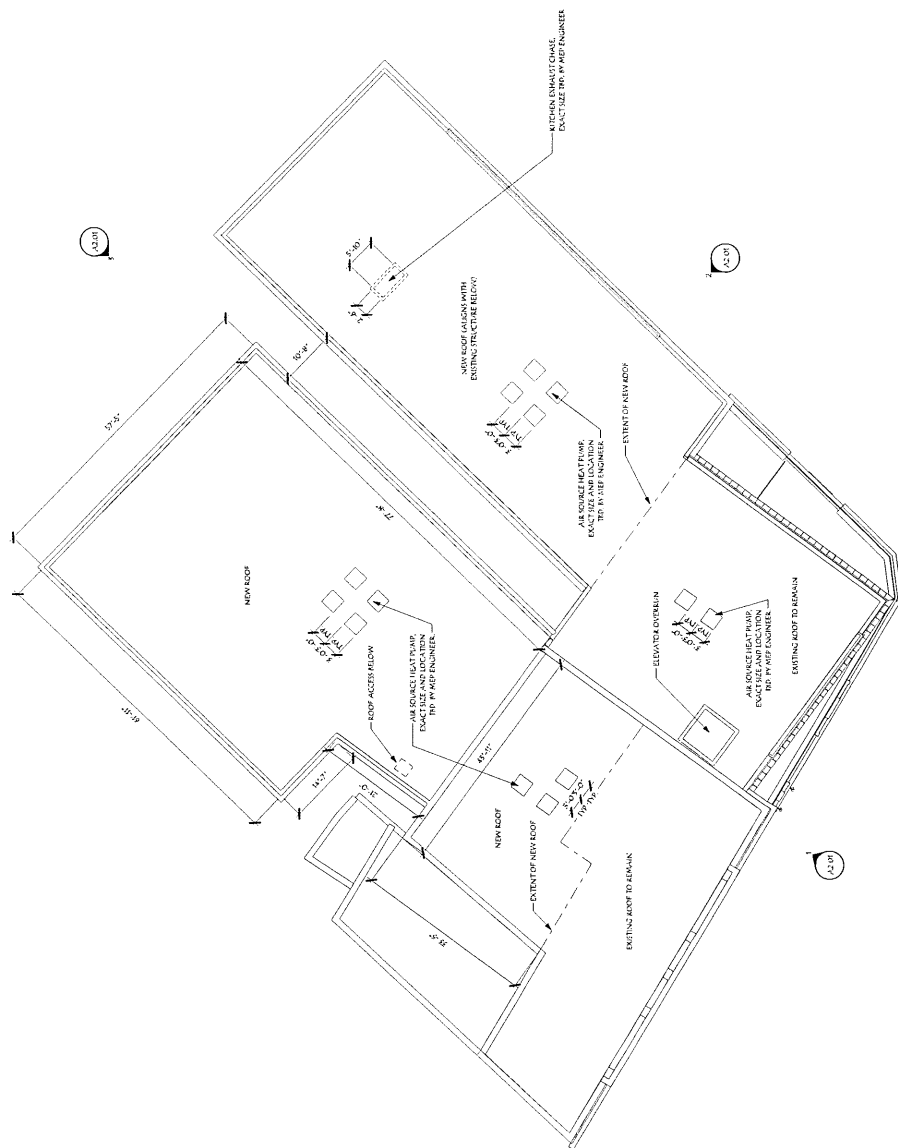
ARLINGTON MIXED USE  
455 MASS AVE  
ARLINGTON, MA



1 LEVEL 1  
SCALE 3/32" = 1'-0"

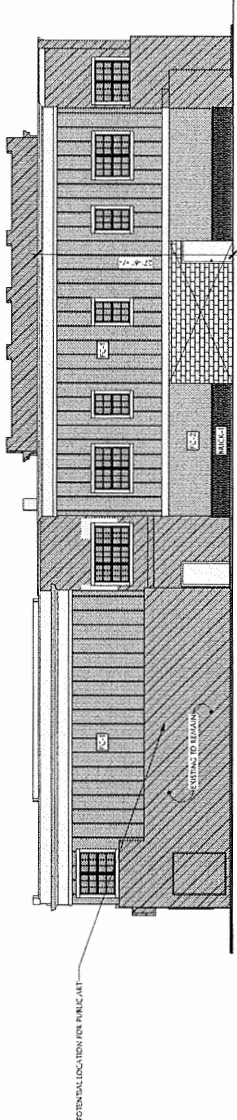




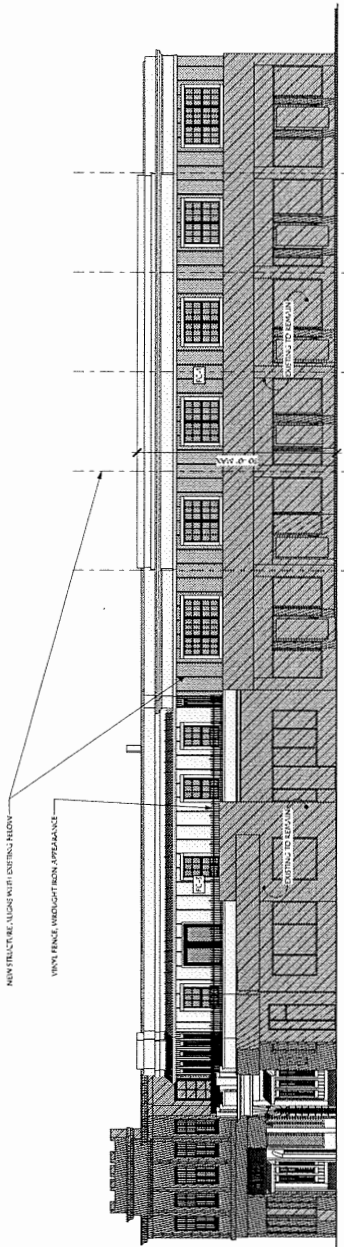


1 ROOF SCALE: 3/52

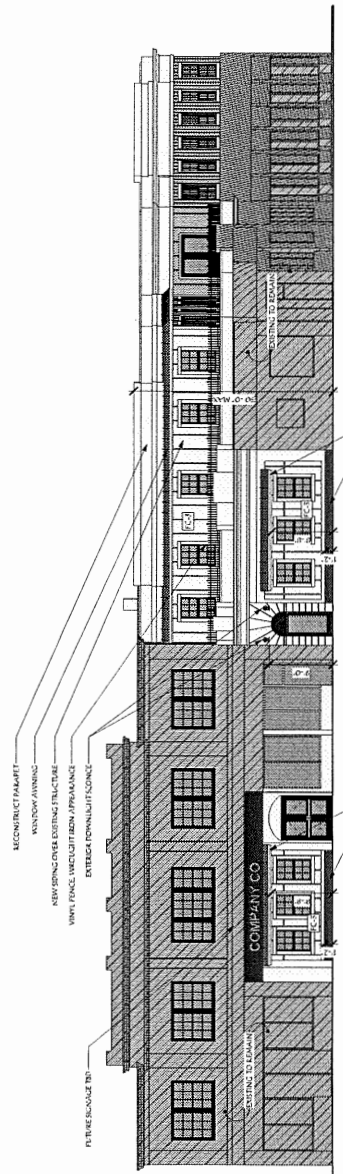
FINISH KEY:  
 FC-1 BOARD AND BATTEN FIRE CEMENT (LT GREEN)  
 FC-2 BOARD AND BATTEN FIRE CEMENT (LT BLUE)  
 FC-3 BOARD AND BATTEN FIRE CEMENT (WHITE)  
 BRICK-1 BRICK FINISH




3 EXTERIOR ELEVATION - REAR  
 SCALE: 1/8" = 1'-0"



2 EXTERIOR ELEVATION - MEDFORD ST  
 SCALE: 1/8" = 1'-0"



1 EXTERIOR ELEVATION - MASS AVE  
 SCALE: 1/8" = 1'-0"

 AR SQUARED ARCHITECTS 455 MASS AVE ARLINGTON, MA 02464 TEL: 617.551.0022	ARLINGTON MIXED USE 455 MASS AVE ARLINGTON, MA	NOT FOR CONSTRUCTION	Title: EXTERIOR ELEVATIONS A2.01	Date: 08/20/21 Project No: 2020034 Checked By: ALW Drawn By: PPS Scale: 1/8" = 1'-0" Revisions:	2021 Mutual Square Architects #120700133 21 01 01 AM
			Date: 08/20/21 Project No: 2020034 Checked By: ALW Drawn By: PPS Scale: 1/8" = 1'-0" Revisions:		

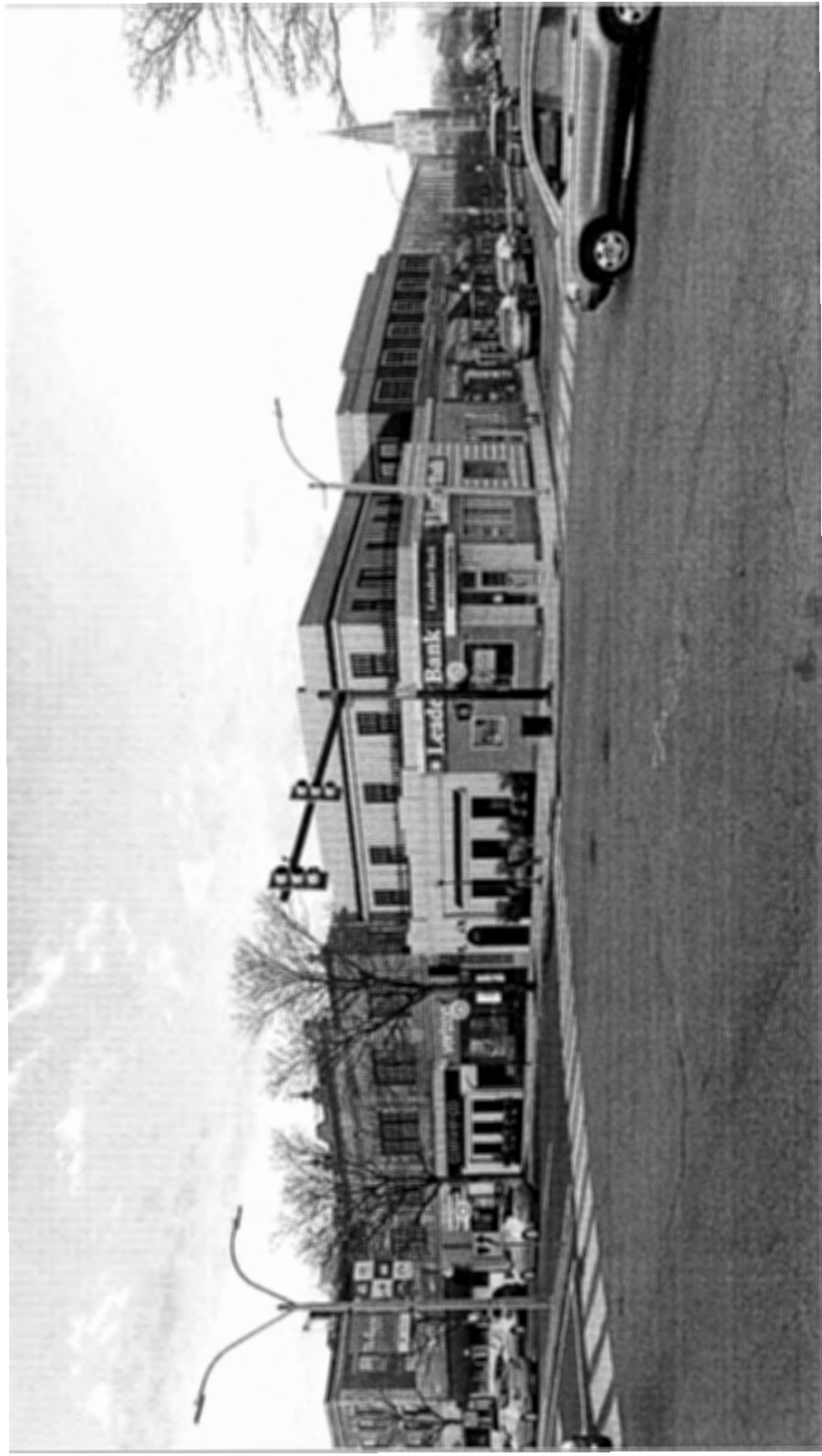


Title: CORNER RENDER  
A9.01

Scale:  
Drawn by: pps  
Checked by: ALM  
Project No.: 2020034  
Date: 08/20/21

NOT FOR CONSTRUCTION

ARLINGTON MIXED USE  
455 MASS AVE  
ARLINGTON, MA



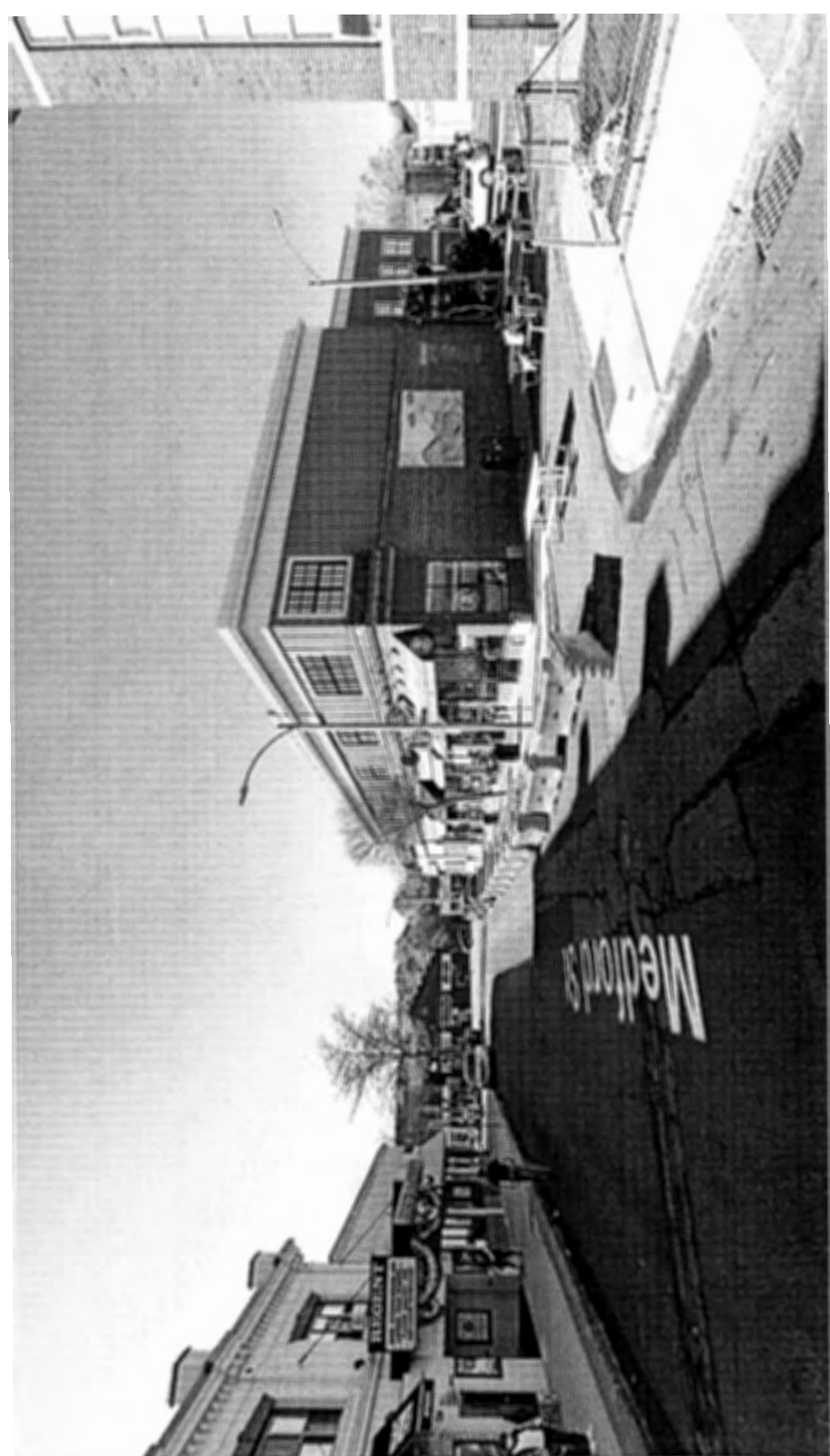
Title  
MEDFORD ST  
RENDER  
A9.02

Scale:  
Drawn By: PPS  
Checked By: ALW  
Project No.: 2020031  
Date: 08/20/21  
Schematic Design

Revisions  
By: PPS  
Date: 08/20/21  
NOT FOR CONSTRUCTION

ARLINGTON MIXED USE  
455 MASS AVE  
ARLINGTON, MA

AR  
SQUARE  
ARCHITECTS  
100 CAMPBELL ST. 3RD FL.  
CAMBRIDGE, MA 02142  
PH: 617.691.0022





166 of 347

John --

With the 455 Mass Ave. project the process here to develop the upper level residential...

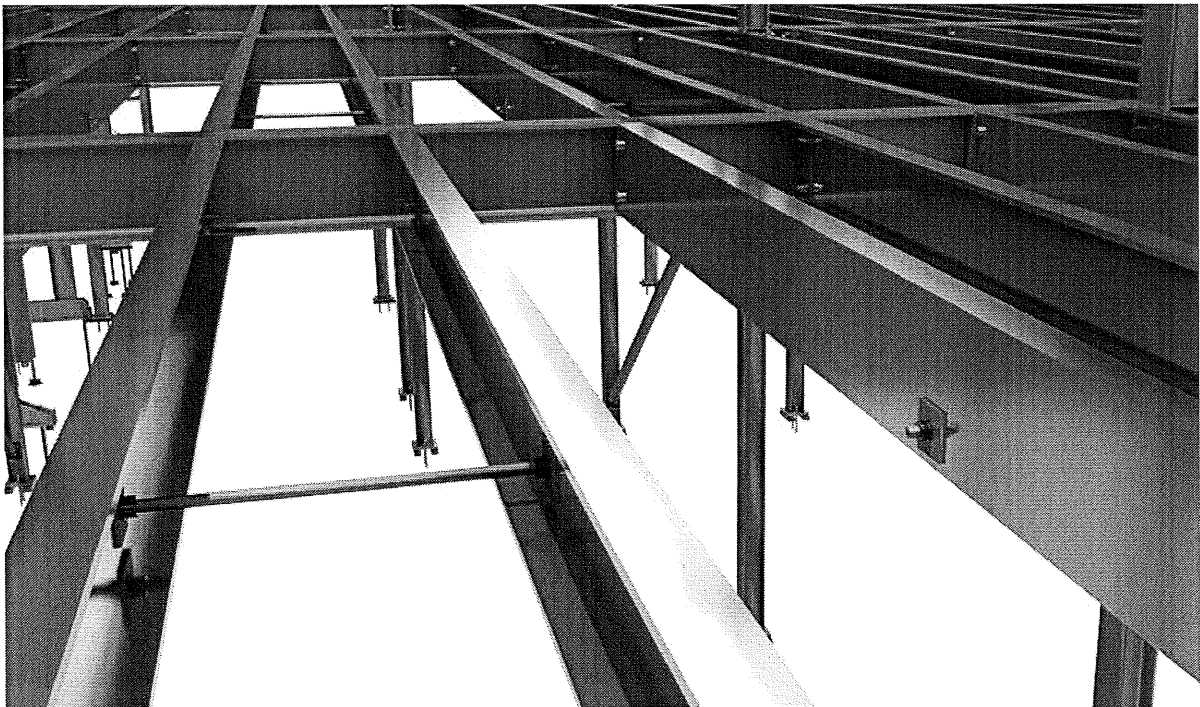
- Prep existing masonry structure to receive new structural supports / beams that we would call "fly over" construction, these beams would run over the existing roof structure allowing a small interstitial space to run MEP mains, branch lines, water & sewer.
- The fly over beams ideally would have intermediate structural steel beams, smaller in size to space from one fly over beam to the next, thereby creating a grid size roughly 8 ft x 8 ft, we would install 4-inch-thick metal decking, we would install t-studs and pour a light-weight concrete slab.
- We would use the new upper-level slab as our starting point to frame in either wood or light gauge metal framing (LGMF) all new walls and roof system for the new upper residential units.
- We would extend all MEP items that run through the roof and continue these through the upper floor and through the new roof, some may be consolidated if possible.
- We would construct the new addition on a standard spread footing below frost extending up above grade with a new 12" thick foundation, this foundation will support the upper level, through a formed concrete or "cast in place" structural wall and in combination support structural steel beams to support the new second floor system, this will match the height of the new fly-over steel/concrete slab so all upper levels are on the same level.
- And same method used for the fly-over slab we would frame in either wood or light gauge metal framing (LGMF) all new walls and roof system for the new upper residential units.
- The new stair tower and the renovated lobby we are recommending to be built out of a masonry CMU (Masonry Block) walls, with proper fire ratings, assume a steel diamond plate stair system with standard railings to the new 2<sup>nd</sup> floor.
- We need to allow for demolition of the existing 2<sup>nd</sup> floor building and framing, this would all need to be removed and we can install a temporary rubber roof system installed over the existing wood framed floor system, this system is similar to the existing roof framing that would remain on the first section.
- Provisions need to be made to enhance the existing brick bearing walls to accommodate the new structural flyover construction, this will mean that on selective evenings we will need to enter the retail spaces below to reinforce the masonry wall to accept new beams



above, this can be done relatively quickly and most of the work would be prefabricated 4-inch tube steel columns installed directly to the walls and bolted or welded to the structure, drilling through the wall from the outside and running galvanized threaded rod through the masonry wall and attached to the new steel columns.

I hope this helps explain the process to build this exciting project.









Issued: August 20, 2021

Jennifer Raitt  
Director of Planning & Community  
Development  
730 Massachusetts Ave  
Arlington, MA 02476

RE: Mixed-Use Redevelopment  
Drainage Summary Letter  
455-457 Massachusetts Ave  
Arlington, MA 02476

Dear Ms. Raitt,

On behalf of our Client, 2-4 Medford Street, LLC, Allen & Major Associates (A&M) is pleased to provide this letter in support of the Special Permit application for the Mixed-Use Redevelopment project at 455-457 Massachusetts Ave. This letter will summarize the changes to the stormwater management system which are proposed as part of the redevelopment efforts.

**Existing Conditions**

The site is located on the corner of Medford Street and Massachusetts Avenue. There is an existing curb cut to the parcel located off Park Terrace to the rear of the parcel. The existing lot is entirely impervious with existing building and pavement. Elevations onsite range from elevation 38 along Mass Ave to elevation 36 at the rear along Park Terrace. The majority of the stormwater on-site flows via sheet flow off-site to Park Terrace. A review of the NRCS soil report for Middlesex County indicates that the soil onsite is considered Merrimac-Urban Land which has a Hydrologic Soil Group rating of an "A". A copy of the Existing Watershed Plan is included herewith.

**Proposed Conditions**

The project, proposes to demolish a portion of the existing structure and construct and addition in a similar location. The project proposes to maintain existing retail, restaurant and service uses on the ground floor, and construct new apartments on the second floor. There are 16 parking stalls proposed on the first level. The stormwater management system will be greatly improved with the installation of pervious paver systems and underground infiltration system. These systems will receive clean roof runoff for stormwater recharge. These systems will greatly reduce the quantity of stormwater runoff from the parcel. The proposed work will also result in approximately 1,008 square feet of impervious material being replaced with landscaped areas.

Runoff flows were estimated for both pre and post development conditions using HydroCAD 10.00 software, at a specific "Study Point" (SP-1). Study Point 1 is the flows that will flow via sheet flow onto Park Terrace. The table below shows that the project causes a reduction in the peak rate of runoff and volume of stormwater leaving the site at the Study Point. Copies of the HydroCAD worksheets and Watershed Plans are included herewith.

<b>STUDY POINT #1 (flow to municipal system)</b>			
	2-Year	10-Year	100-Year
Existing Flow (CFS)	1.41	2.16	3.93
Proposed Flow (CFS)	0.08	0.13	0.50
<b>Decrease (CFS)</b>	<b>1.33</b>	<b>2.03</b>	<b>3.43</b>
Existing Volume (CF)	4,728	7,356	13,644
Proposed Volume (CF)	235	400	975
<b>Decrease (CF)</b>	<b>4,493</b>	<b>6,956</b>	<b>12,669</b>

The surface water drainage requirements of the Town of Arlington Zoning Bylaw Environmental Design Review Standards have been reviewed and met with the proposed design. The proposed project will introduce stormwater infiltration systems and landscaped areas to the site to reduce the impervious area. The Town of Arlington, Article 15 Stormwater Mitigation, shall not apply as the proposed development will introduce a reduction in impervious area. However, with the proposed infiltration systems the project will reduce the runoff rates for all design storms, and comply with this bylaw.

#### **Summary**

As shown in the table above, the proposed development will have a positive impact on the stormwater management system by reducing the rate and volume of stormwater runoff from the site.

Very truly yours,

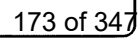
ALLEN & MAJOR ASSOCIATES, INC.






Aaron Mackey, PE  
Project Engineer

#### **Attachments:**

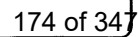
1. Existing Watershed Plan
2. Proposed Watershed Plan
3. Pre development HydroCAD Calculations
4. Post development HydroCAD Calculations
5. Extreme Precipitation Tables
6. NRCS Soil Report



**LEGEND**

EXISTING WATERSHED	
SUBCATCHMENT BOUNDARY	
SUBCATCHMENT LABEL	

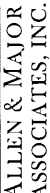




AUGUST 11, 2021

2-14 MEDFORD STREET, LLC  
455 MASSACHUSETTS AVE, STE 1  
ARLINGTON, MA 02474

PROJECT NO.	2729-03	DATE:	8/11/2021
SCALE:	1" = 10'	DWG. NAME:	C2729-03
DESIGNED BY:	JG/ARM	CHECKED BY:	ARM/BDJ

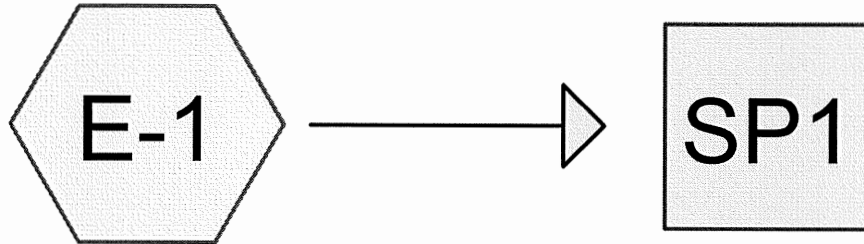


WOBURN, MA • LAKEVILLE, MA • MANCHESTER, NH

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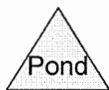
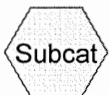
DRAWING TITLE:	SHEET NO.
PROPOSED WATERSHED PLAN	PWP

City of San Diego, Department of Water, Sanitation, & Irrigation



Subcat E-1

Study Point 1



## 2729-03\_Existing-Conditions

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Page 2

### Area Listing (all nodes)

Area (sq-ft)	CN	Description (subcatchment-numbers)
18,929	98	Paved parking, HSG A (E-1)
<b>18,929</b>	<b>98</b>	<b>TOTAL AREA</b>



**2729-03\_Existing-Conditions**

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**Soil Listing (all nodes)**

Area (sq-ft)	Soil Group	Subcatchment Numbers
18,929	HSG A	E-1
0	HSG B	
0	HSG C	
0	HSG D	
0	Other	
18,929		TOTAL AREA

2729-03\_Existing-Conditions

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Ground Covers (all nodes)							
HSG-A (sq-ft)	HSG-B (sq-ft)	HSG-C (sq-ft)	HSG-D (sq-ft)	Other (sq-ft)	Total (sq-ft)	Ground Cover	Subcatchment Numbers
18,929	0	0	0	0	18,929	Paved parking	E-1
18,929	0	0	0	0	18,929	TOTAL AREA	

## 2729-03\_Existing-Conditions

Type III 24-hr 2-Year Rainfall=3.23"

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Time span=0.00-72.00 hrs, dt=0.01 hrs, 7201 points x 3

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN

Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

### SubcatchmentE-1: Subcat E-1

Runoff Area=18,929 sf 100.00% Impervious Runoff Depth=3.00"

Tc=5.0 min CN=98 Runoff=1.41 cfs 4,728 cf

### Reach SP1: Study Point 1

Inflow=1.41 cfs 4,728 cf

Outflow=1.41 cfs 4,728 cf

**Total Runoff Area = 18,929 sf   Runoff Volume = 4,728 cf   Average Runoff Depth = 3.00"**  
**0.00% Pervious = 0 sf   100.00% Impervious = 18,929 sf**



**2729-03 Existing-Conditions**

Type III 24-hr 2-Year Rainfall=3.23"

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**Summary for Subcatchment E-1: Subcat E-1**

Runoff = 1.41 cfs @ 12.07 hrs, Volume= 4,728 cf, Depth= 3.00"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs

Type III 24-hr 2-Year Rainfall=3.23"

Area (sf)	CN	Description
18,929	98	Paved parking, HSG A
18,929		100.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Assumed

**Summary for Reach SP1: Study Point 1**

Inflow Area = 18,929 sf, 100.00% Impervious, Inflow Depth = 3.00" for 2-Year event

Inflow = 1.41 cfs @ 12.07 hrs, Volume= 4,728 cf

Outflow = 1.41 cfs @ 12.07 hrs, Volume= 4,728 cf, Atten= 0%, Lag= 0.0 min

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3

**2729-03\_Existing-Conditions***Type III 24-hr 10-Year Rainfall=4.90"*

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Time span=0.00-72.00 hrs, dt=0.01 hrs, 7201 points x 3

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN

Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

**SubcatchmentE-1: Subcat E-1**

Runoff Area=18,929 sf 100.00% Impervious Runoff Depth=4.66"

Tc=5.0 min CN=98 Runoff=2.16 cfs 7,356 cf

**Reach SP1: Study Point 1**

Inflow=2.16 cfs 7,356 cf

Outflow=2.16 cfs 7,356 cf

**Total Runoff Area = 18,929 sf Runoff Volume = 7,356 cf Average Runoff Depth = 4.66"**  
**0.00% Pervious = 0 sf 100.00% Impervious = 18,929 sf**

**2729-03 Existing-Conditions**

Type III 24-hr 10-Year Rainfall=4.90"

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**Summary for Subcatchment E-1: Subcat E-1**

Runoff = 2.16 cfs @ 12.07 hrs, Volume= 7,356 cf, Depth= 4.66"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs

Type III 24-hr 10-Year Rainfall=4.90"

Area (sf)	CN	Description
18,929	98	Paved parking, HSG A
18,929		100.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Assumed

**Summary for Reach SP1: Study Point 1**

Inflow Area = 18,929 sf, 100.00% Impervious, Inflow Depth = 4.66" for 10-Year event

Inflow = 2.16 cfs @ 12.07 hrs, Volume= 7,356 cf

Outflow = 2.16 cfs @ 12.07 hrs, Volume= 7,356 cf, Atten= 0%, Lag= 0.0 min

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3



**2729-03\_Existing-Conditions***Type III 24-hr 100-Year Rainfall=8.89"*

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Time span=0.00-72.00 hrs, dt=0.01 hrs, 7201 points x 3

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN

Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

**SubcatchmentE-1: SubcatE-1**

Runoff Area=18,929 sf 100.00% Impervious Runoff Depth=8.65"

Tc=5.0 min CN=98 Runoff=3.93 cfs 13,644 cf

**Reach SP1: Study Point 1**

Inflow=3.93 cfs 13,644 cf

Outflow=3.93 cfs 13,644 cf

**Total Runoff Area = 18,929 sf Runoff Volume = 13,644 cf Average Runoff Depth = 8.65"**  
**0.00% Pervious = 0 sf 100.00% Impervious = 18,929 sf**

**2729-03\_Existing-Conditions**

Type III 24-hr 100-Year Rainfall=8.89"

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**Summary for Subcatchment E-1: Subcat E-1**

Runoff = 3.93 cfs @ 12.07 hrs, Volume= 13,644 cf, Depth= 8.65"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs

Type III 24-hr 100-Year Rainfall=8.89"

Area (sf)	CN	Description
18,929	98	Paved parking, HSG A
18,929		100.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Assumed

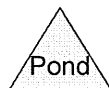
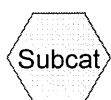
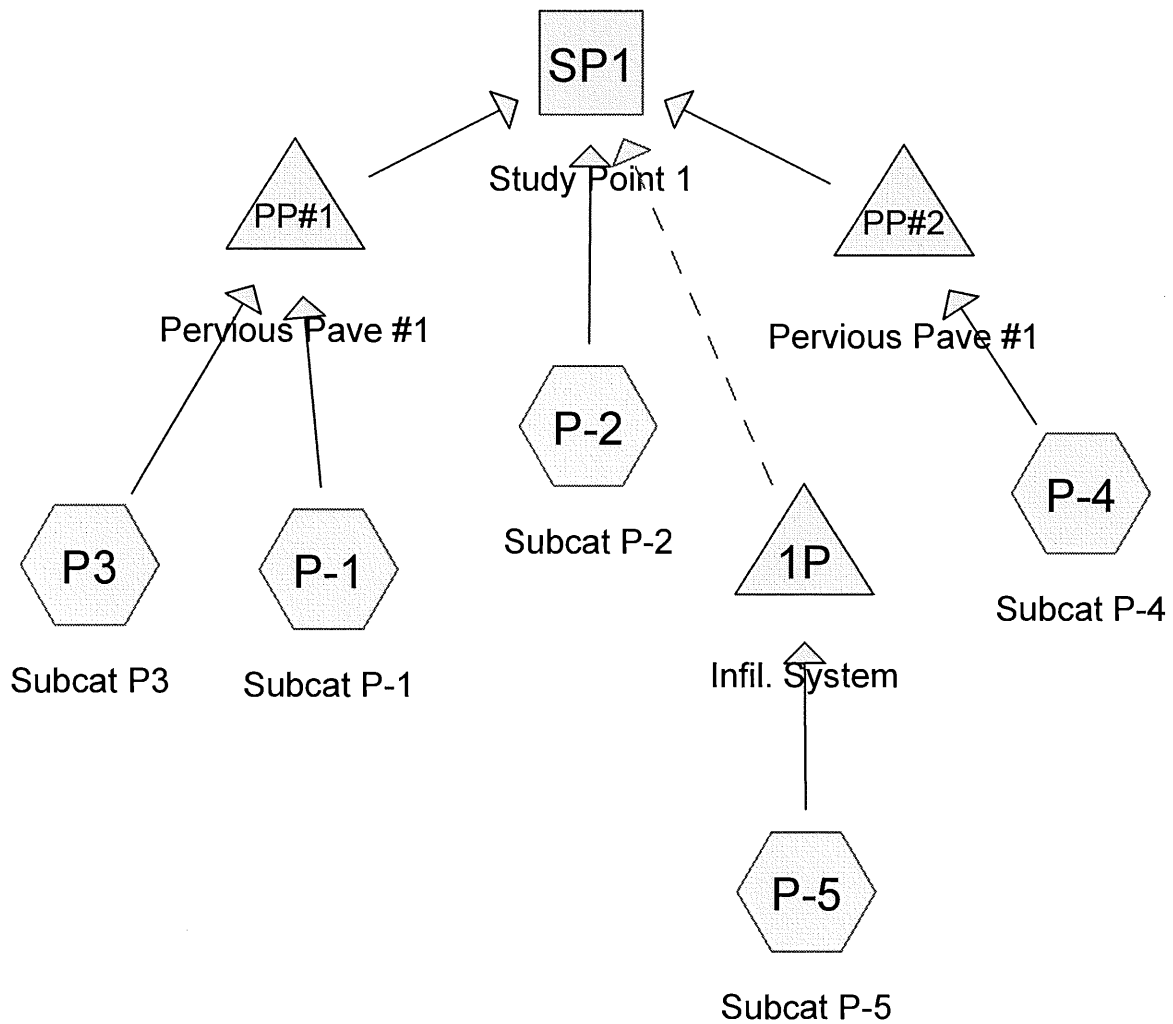
**Summary for Reach SP1: Study Point 1**

Inflow Area = 18,929 sf, 100.00% Impervious, Inflow Depth = 8.65" for 100-Year event

Inflow = 3.93 cfs @ 12.07 hrs, Volume= 13,644 cf

Outflow = 3.93 cfs @ 12.07 hrs, Volume= 13,644 cf, Atten= 0%, Lag= 0.0 min

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3





## 2729-03\_Proposed-Conditions

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### Area Listing (all nodes)

Area (sq-ft)	CN	Description (subcatchment-numbers)
1,082	39	>75% Grass cover, Good, HSG A (P-1, P-2, P-4, P3)
2,803	98	Paved parking, HSG A (P-1, P-2, P-4, P-5, P3)
15,043	98	Roofs, HSG A (P-1, P-5)
<b>18,929</b>	<b>95</b>	<b>TOTAL AREA</b>

**2729-03\_Proposed-Conditions**

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**Soil Listing (all nodes)**

Area (sq-ft)	Soil Group	Subcatchment Numbers
18,929	HSG A	P-1, P-2, P-4, P-5, P3
0	HSG B	
0	HSG C	
0	HSG D	
0	Other	
<b>18,929</b>		<b>TOTAL AREA</b>

**2729-03\_Proposed-Conditions**

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**Ground Covers (all nodes)**

HSG-A (sq-ft)	HSG-B (sq-ft)	HSG-C (sq-ft)	HSG-D (sq-ft)	Other (sq-ft)	Total (sq-ft)	Ground Cover	Subcatchment Numbers
1,082	0	0	0	0	1,082	>75% Grass cover, Good	P-1, P-2, P-4, P3
2,803	0	0	0	0	2,803	Paved parking	P-1, P-2, P-4, P-5, P3
15,043	0	0	0	0	15,043	Roofs	P-1, P-5
<b>18,929</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>18,929</b>	<b>TOTAL AREA</b>	



**2729-03\_Proposed-Conditions**

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**Notes Listing (all nodes)**

Line#	Node Number	Notes
1	1P	Exfiltration Rate = 7.0 in./hr. (Merrimac-Urban Land = 100 micrometer per second = 14.17 in./hr. / 2 = 7.0 in/hr
2	PP#1	Exfiltration Rate = 7.0 in./hr. (Merrimac-Urban Land = 100 micrometer per second = 14.17 in./hr. / 2 = 7.0 in/hr
3	PP#2	Exfiltration Rate = 7.0 in./hr. (Merrimac-Urban Land = 100 micrometer per second = 14.17 in./hr. / 2 = 7.0 in/hr

**2729-03\_Proposed-Conditions**

Type III 24-hr 2-Year Rainfall=3.23"

Prepared by Allen &amp; Major Associates Inc.

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Time span=0.00-72.00 hrs, dt=0.01 hrs, 7201 points x 3  
Runoff by SCS TR-20 method, UH=SCS, Weighted-CN  
Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

**SubcatchmentP-1: Subcat P-1**

Runoff Area=4,358 sf 99.99% Impervious Runoff Depth=3.00"  
Tc=5.0 min CN=98 Runoff=0.33 cfs 1,089 cf

**SubcatchmentP-2: Subcat P-2**

Runoff Area=1,234 sf 87.57% Impervious Runoff Depth=2.29"  
Tc=5.0 min CN=91 Runoff=0.08 cfs 235 cf

**SubcatchmentP-4: Subcat P-4**

Runoff Area=972 sf 55.84% Impervious Runoff Depth=0.95"  
Tc=0.0 min CN=72 Runoff=0.03 cfs 77 cf

**SubcatchmentP-5: Subcat P-5**

Runoff Area=11,040 sf 100.00% Impervious Runoff Depth=3.00"  
Tc=5.0 min CN=98 Runoff=0.82 cfs 2,758 cf

**SubcatchmentP3: Subcat P3**

Runoff Area=1,324 sf 62.26% Impervious Runoff Depth=1.17"  
Tc=5.0 min CN=76 Runoff=0.04 cfs 129 cf

**Reach SP1: Study Point 1**

Inflow=0.08 cfs 235 cf  
Outflow=0.08 cfs 235 cf

**Pond 1P: Infil. System**

Peak Elev=30.70' Storage=674 cf Inflow=0.82 cfs 2,758 cf  
Discarded=0.14 cfs 2,759 cf Secondary=0.00 cfs 0 cf Outflow=0.14 cfs 2,759 cf

**Pond PP#1: Pervious Pave #1**

Peak Elev=34.30' Storage=149 cf Inflow=0.37 cfs 1,218 cf  
Discarded=0.13 cfs 1,219 cf Primary=0.00 cfs 0 cf Outflow=0.13 cfs 1,219 cf

**Pond PP#2: Pervious Pave #1**

Peak Elev=35.30' Storage=0 cf Inflow=0.03 cfs 77 cf  
Discarded=0.03 cfs 77 cf Primary=0.00 cfs 0 cf Outflow=0.03 cfs 77 cf

**Total Runoff Area = 18,929 sf Runoff Volume = 4,288 cf Average Runoff Depth = 2.72"**  
**5.72% Pervious = 1,082 sf 94.28% Impervious = 17,846 sf**

**2729-03\_Proposed-Conditions**

Type III 24-hr 2-Year Rainfall=3.23"

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**Summary for Subcatchment P-1: Subcat P-1**

Runoff = 0.33 cfs @ 12.07 hrs, Volume= 1,089 cf, Depth= 3.00"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs

Type III 24-hr 2-Year Rainfall=3.23"

Area (sf)	CN	Description
0	98	Paved parking, HSG A
0	39	>75% Grass cover, Good, HSG A
4,358	98	Roofs, HSG A
4,358	98	Weighted Average
0		0.01% Pervious Area
4,358		99.99% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Assumed

**Summary for Subcatchment P-2: Subcat P-2**

Runoff = 0.08 cfs @ 12.07 hrs, Volume= 235 cf, Depth= 2.29"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs

Type III 24-hr 2-Year Rainfall=3.23"

Area (sf)	CN	Description
1,081	98	Paved parking, HSG A
153	39	>75% Grass cover, Good, HSG A
1,234	91	Weighted Average
153		12.43% Pervious Area
1,081		87.57% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Min tc

**Summary for Subcatchment P-4: Subcat P-4**

Runoff = 0.03 cfs @ 12.00 hrs, Volume= 77 cf, Depth= 0.95"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs

Type III 24-hr 2-Year Rainfall=3.23"

Area (sf)	CN	Description
543	98	Paved parking, HSG A
429	39	>75% Grass cover, Good, HSG A
972	72	Weighted Average
429		44.16% Pervious Area
543		55.84% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
0.0					Direct Entry, min tc

**Summary for Subcatchment P-5: Subcat P-5**

Runoff = 0.82 cfs @ 12.07 hrs, Volume= 2,758 cf, Depth= 3.00"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs

Type III 24-hr 2-Year Rainfall=3.23"



**2729-03\_Proposed-Conditions**

Type III 24-hr 2-Year Rainfall=3.23"

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Area (sf)	CN	Description
10,685	98	Roofs, HSG A
355	98	Paved parking, HSG A
11,040	98	Weighted Average
11,040		100.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, min tc

**Summary for Subcatchment P3: Subcat P3**

Runoff = 0.04 cfs @ 12.08 hrs, Volume= 129 cf, Depth= 1.17"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs  
Type III 24-hr 2-Year Rainfall=3.23"

Area (sf)	CN	Description
500	39	>75% Grass cover, Good, HSG A
825	98	Paved parking, HSG A
1,324	76	Weighted Average
500		37.74% Pervious Area
825		62.26% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, min tc

**Summary for Reach SP1: Study Point 1**Inflow Area = 7,889 sf, 86.28% Impervious, Inflow Depth = 0.36" for 2-Year event  
Inflow = 0.08 cfs @ 12.07 hrs, Volume= 235 cf  
Outflow = 0.08 cfs @ 12.07 hrs, Volume= 235 cf, Atten= 0%, Lag= 0.0 min

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3

**Summary for Pond 1P: Infil. System**Inflow Area = 11,040 sf, 100.00% Impervious, Inflow Depth = 3.00" for 2-Year event  
Inflow = 0.82 cfs @ 12.07 hrs, Volume= 2,758 cf  
Outflow = 0.14 cfs @ 11.80 hrs, Volume= 2,759 cf, Atten= 83%, Lag= 0.0 min  
Discarded = 0.14 cfs @ 11.80 hrs, Volume= 2,759 cf  
Secondary = 0.00 cfs @ 0.00 hrs, Volume= 0 cf

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3

Peak Elev= 30.70' @ 12.51 hrs Surf.Area= 885 sf Storage= 674 cf

Flood Elev= 37.00' Surf.Area= 885 sf Storage= 3,172 cf

Plug-Flow detention time= (not calculated: outflow precedes inflow)

Center-of-Mass det. time= 24.0 min ( 779.3 - 755.3 )

Volume	Invert	Avail.Storage	Storage Description
#1A	29.00'	1,573 cf	<b>15.83'W x 55.89'L x 6.25'H Field A</b> 5,531 cf Overall - 1,599 cf Embedded = 3,932 cf x 40.0% Voids
#2A	30.50'	1,599 cf	<b>ADS_StormTech MC-3500 d +Cap x 14 Inside #1</b> Effective Size= 70.4"W x 45.0"H => 15.33 sf x 7.17'L = 110.0 cf Overall Size= 77.0"W x 45.0"H x 7.50'L with 0.33' Overlap 14 Chambers in 2 Rows Cap Storage= +14.9 cf x 2 x 2 rows = 59.6 cf
		3,172 cf	Total Available Storage

Storage Group A created with Chamber Wizard

**2729-03\_Proposed-Conditions**

Type III 24-hr 2-Year Rainfall=3.23"

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Device	Routing	Invert	Outlet Devices
#1	Discarded	29.00'	<b>7.000 in/hr Exfiltration over Surface area</b>
#2	Secondary	36.00'	<b>5.0' long x 0.5' breadth Broad-Crested Rectangular Weir</b>
			Head (feet) 0.20 0.40 0.60 0.80 1.00
			Coef. (English) 2.80 2.92 3.08 3.30 3.32

**Discarded OutFlow** Max=0.14 cfs @ 11.80 hrs HW=29.09' (Free Discharge)↑**1=Exfiltration** (Exfiltration Controls 0.14 cfs)**Secondary OutFlow** Max=0.00 cfs @ 0.00 hrs HW=29.00' TW=0.00' (Dynamic Tailwater)↑**2=Broad-Crested Rectangular Weir** (Controls 0.00 cfs)**Summary for Pond PP#1: Pervious Pave #1**

Inflow Area = 5,683 sf, 91.20% Impervious, Inflow Depth = 2.57" for 2-Year event  
 Inflow = 0.37 cfs @ 12.07 hrs, Volume= 1,218 cf  
 Outflow = 0.13 cfs @ 12.32 hrs, Volume= 1,219 cf, Atten= 65%, Lag= 14.9 min  
 Discarded = 0.13 cfs @ 12.32 hrs, Volume= 1,219 cf  
 Primary = 0.00 cfs @ 0.00 hrs, Volume= 0 cf

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3

Peak Elev= 34.30' @ 12.32 hrs Surf.Area= 753 sf Storage= 149 cf

Flood Elev= 37.00' Surf.Area= 753 sf Storage= 904 cf

Plug-Flow detention time= (not calculated: outflow precedes inflow)

Center-of-Mass det. time= 4.6 min ( 770.4 - 765.8 )

Volume	Invert	Avail.Storage	Storage Description
#1	33.80'	904 cf	<b>Subbase (Conic)</b> Listed below (Recalc) 2,259 cf Overall x 40.0% Voids

Elevation (feet)	Surf.Area (sq-ft)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)	Wet.Area (sq-ft)
33.80	753	0	0	753
36.80	753	2,259	2,259	1,045

Device	Routing	Invert	Outlet Devices
#1	Discarded	33.80'	<b>7.000 in/hr Exfiltration over Wetted area</b>
#2	Primary	36.80'	<b>4.5' long x 3.0' breadth Broad-Crested Rectangular Weir</b>
			Head (feet) 0.20 0.40 0.60 0.80 1.00 1.20 1.40 1.60 1.80 2.00 2.50 3.00 3.50 4.00 4.50
			Coef. (English) 2.44 2.58 2.68 2.67 2.65 2.64 2.64 2.68 2.68 2.72 2.81 2.92 2.97 3.07 3.32

**Discarded OutFlow** Max=0.13 cfs @ 12.32 hrs HW=34.30' (Free Discharge)↑**1=Exfiltration** (Exfiltration Controls 0.13 cfs)**Primary OutFlow** Max=0.00 cfs @ 0.00 hrs HW=33.80' TW=0.00' (Dynamic Tailwater)↑**2=Broad-Crested Rectangular Weir** (Controls 0.00 cfs)**Summary for Pond PP#2: Pervious Pave #1**

Inflow Area = 972 sf, 55.84% Impervious, Inflow Depth = 0.95" for 2-Year event  
 Inflow = 0.03 cfs @ 12.00 hrs, Volume= 77 cf  
 Outflow = 0.03 cfs @ 12.00 hrs, Volume= 77 cf, Atten= 0%, Lag= 0.0 min  
 Discarded = 0.03 cfs @ 12.00 hrs, Volume= 77 cf  
 Primary = 0.00 cfs @ 0.00 hrs, Volume= 0 cf

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3

Peak Elev= 35.30' @ 0.00 hrs Surf.Area= 546 sf Storage= 0 cf

Flood Elev= 37.00' Surf.Area= 546 sf Storage= 328 cf

Plug-Flow detention time= (not calculated: outflow precedes inflow)

Center-of-Mass det. time= 0.0 min ( 862.6 - 862.6 )

**2729-03\_Proposed-Conditions**

Type III 24-hr 2-Year Rainfall=3.23"

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Volume	Invert	Avail.Storage	Storage Description
#1	35.30'	328 cf	<b>Subbase (Conic)</b> Listed below (Recalc) 819 cf Overall x 40.0% Voids

Elevation (feet)	Surf.Area (sq-ft)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)	Wet.Area (sq-ft)
35.30	546	0	0	546
36.80	546	819	819	670

Device	Routing	Invert	Outlet Devices
#1	Discarded	35.30'	<b>7.000 in/hr Exfiltration over Wetted area</b>
#2	Primary	36.80'	<b>4.5' long x 3.0' breadth Broad-Crested Rectangular Weir</b>
			Head (feet) 0.20 0.40 0.60 0.80 1.00 1.20 1.40 1.60 1.80 2.00 2.50 3.00 3.50 4.00 4.50
			Coef. (English) 2.44 2.58 2.68 2.67 2.65 2.64 2.64 2.68 2.68 2.72 2.81 2.92 2.97 3.07 3.32

**Discarded OutFlow** Max=0.00 cfs @ 12.00 hrs HW=35.30' (Free Discharge)↑**1=Exfiltration** (Passes 0.00 cfs of 0.09 cfs potential flow)**Primary OutFlow** Max=0.00 cfs @ 0.00 hrs HW=35.30' TW=0.00' (Dynamic Tailwater)↑**2=Broad-Crested Rectangular Weir**( Controls 0.00 cfs)



**2729-03\_Proposed-Conditions**

Type III 24-hr 10-Year Rainfall=4.90"

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Time span=0.00-72.00 hrs, dt=0.01 hrs, 7201 points x 3  
Runoff by SCS TR-20 method, UH=SCS, Weighted-CN  
Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

<b>SubcatchmentP-1: Subcat P-1</b>	Runoff Area=4,358 sf 99.99% Impervious Runoff Depth=4.66" Tc=5.0 min CN=98 Runoff=0.50 cfs 1,694 cf
<b>SubcatchmentP-2: Subcat P-2</b>	Runoff Area=1,234 sf 87.57% Impervious Runoff Depth=3.89" Tc=5.0 min CN=91 Runoff=0.13 cfs 400 cf
<b>SubcatchmentP-4: Subcat P-4</b>	Runoff Area=972 sf 55.84% Impervious Runoff Depth=2.12" Tc=0.0 min CN=72 Runoff=0.07 cfs 172 cf
<b>SubcatchmentP-5: Subcat P-5</b>	Runoff Area=11,040 sf 100.00% Impervious Runoff Depth=4.66" Tc=5.0 min CN=98 Runoff=1.26 cfs 4,290 cf
<b>SubcatchmentP3: Subcat P3</b>	Runoff Area=1,324 sf 62.26% Impervious Runoff Depth=2.45" Tc=5.0 min CN=76 Runoff=0.09 cfs 271 cf
<b>Reach SP1: Study Point 1</b>	Inflow=0.13 cfs 400 cf Outflow=0.13 cfs 400 cf
<b>Pond 1P: Infil. System</b>	Peak Elev=31.58' Storage=1,292 cf Inflow=1.26 cfs 4,290 cf Discarded=0.14 cfs 4,291 cf Secondary=0.00 cfs 0 cf Outflow=0.14 cfs 4,291 cf
<b>Pond PP#1: Pervious Pave #1</b>	Peak Elev=35.10' Storage=391 cf Inflow=0.59 cfs 1,964 cf Discarded=0.14 cfs 1,966 cf Primary=0.00 cfs 0 cf Outflow=0.14 cfs 1,966 cf
<b>Pond PP#2: Pervious Pave #1</b>	Peak Elev=35.30' Storage=0 cf Inflow=0.07 cfs 172 cf Discarded=0.07 cfs 172 cf Primary=0.00 cfs 0 cf Outflow=0.07 cfs 172 cf
<b>Total Runoff Area = 18,929 sf Runoff Volume = 6,826 cf Average Runoff Depth = 4.33"</b> <b>5.72% Pervious = 1,082 sf 94.28% Impervious = 17,846 sf</b>	

**2729-03\_Proposed-Conditions**

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Type III 24-hr 10-Year Rainfall=4.90"

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**Summary for Subcatchment P-1: Subcat P-1**

Runoff = 0.50 cfs @ 12.07 hrs, Volume= 1,694 cf, Depth= 4.66"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs

Type III 24-hr 10-Year Rainfall=4.90"

Area (sf)	CN	Description
0	98	Paved parking, HSG A
0	39	>75% Grass cover, Good, HSG A
4,358	98	Roofs, HSG A
4,358	98	Weighted Average
0		0.01% Pervious Area
4,358		99.99% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Assumed

**Summary for Subcatchment P-2: Subcat P-2**

Runoff = 0.13 cfs @ 12.07 hrs, Volume= 400 cf, Depth= 3.89"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs

Type III 24-hr 10-Year Rainfall=4.90"

Area (sf)	CN	Description
1,081	98	Paved parking, HSG A
153	39	>75% Grass cover, Good, HSG A
1,234	91	Weighted Average
153		12.43% Pervious Area
1,081		87.57% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Min tc

**Summary for Subcatchment P-4: Subcat P-4**

Runoff = 0.07 cfs @ 12.00 hrs, Volume= 172 cf, Depth= 2.12"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs

Type III 24-hr 10-Year Rainfall=4.90"

Area (sf)	CN	Description
543	98	Paved parking, HSG A
429	39	>75% Grass cover, Good, HSG A
972	72	Weighted Average
429		44.16% Pervious Area
543		55.84% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
0.0					Direct Entry, min tc

**Summary for Subcatchment P-5: Subcat P-5**

Runoff = 1.26 cfs @ 12.07 hrs, Volume= 4,290 cf, Depth= 4.66"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs

Type III 24-hr 10-Year Rainfall=4.90"

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Type III 24-hr 10-Year Rainfall=4.90"

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Area (sf)	CN	Description
10,685	98	Roofs, HSG A
355	98	Paved parking, HSG A
11,040	98	Weighted Average
11,040		100.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, min tc

**Summary for Subcatchment P3: Subcat P3**

Runoff = 0.09 cfs @ 12.08 hrs, Volume= 271 cf, Depth= 2.45"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs  
Type III 24-hr 10-Year Rainfall=4.90"

Area (sf)	CN	Description
500	39	>75% Grass cover, Good, HSG A
825	98	Paved parking, HSG A
1,324	76	Weighted Average
500		37.74% Pervious Area
825		62.26% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, min tc

**Summary for Reach SP1: Study Point 1**Inflow Area = 7,889 sf, 86.28% Impervious, Inflow Depth = 0.61" for 10-Year event  
Inflow = 0.13 cfs @ 12.07 hrs, Volume= 400 cf  
Outflow = 0.13 cfs @ 12.07 hrs, Volume= 400 cf, Atten= 0%, Lag= 0.0 min

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3

**Summary for Pond 1P: Infil. System**Inflow Area = 11,040 sf, 100.00% Impervious, Inflow Depth = 4.66" for 10-Year event  
Inflow = 1.26 cfs @ 12.07 hrs, Volume= 4,290 cf  
Outflow = 0.14 cfs @ 11.69 hrs, Volume= 4,291 cf, Atten= 89%, Lag= 0.0 min  
Discarded = 0.14 cfs @ 11.69 hrs, Volume= 4,291 cf  
Secondary = 0.00 cfs @ 0.00 hrs, Volume= 0 cf

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3

Peak Elev= 31.58' @ 12.64 hrs Surf.Area= 885 sf Storage= 1,292 cf

Flood Elev= 37.00' Surf.Area= 885 sf Storage= 3,172 cf

Plug-Flow detention time= (not calculated: outflow precedes inflow)

Center-of-Mass det. time= 54.6 min ( 802.0 - 747.4 )

Volume	Invert	Avail.Storage	Storage Description
#1A	29.00'	1,573 cf	<b>15.83'W x 55.89'L x 6.25'H Field A</b> 5,531 cf Overall - 1,599 cf Embedded = 3,932 cf x 40.0% Voids
#2A	30.50'	1,599 cf	<b>ADS StormTech MC-3500 d +Cap x 14 Inside #1</b> Effective Size= 70.4"W x 45.0"H => 15.33 sf x 7.17'L = 110.0 cf Overall Size= 77.0"W x 45.0"H x 7.50'L with 0.33' Overlap 14 Chambers in 2 Rows Cap Storage= +14.9 cf x 2 x 2 rows = 59.6 cf
		3,172 cf	Total Available Storage

Storage Group A created with Chamber Wizard



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Type III 24-hr 10-Year Rainfall=4.90"

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Device	Routing	Invert	Outlet Devices
#1	Discarded	29.00'	<b>7.000 in/hr Exfiltration over Surface area</b>
#2	Secondary	36.00'	<b>5.0' long x 0.5' breadth Broad-Crested Rectangular Weir</b>
			Head (feet) 0.20 0.40 0.60 0.80 1.00
			Coef. (English) 2.80 2.92 3.08 3.30 3.32

**Discarded OutFlow** Max=0.14 cfs @ 11.69 hrs HW=29.08' (Free Discharge)

↑1=Exfiltration (Exfiltration Controls 0.14 cfs)

**Secondary OutFlow** Max=0.00 cfs @ 0.00 hrs HW=29.00' TW=0.00' (Dynamic Tailwater)

↑2=Broad-Crested Rectangular Weir( Controls 0.00 cfs)

**Summary for Pond PP#1: Pervious Pave #1**

Inflow Area = 5,683 sf, 91.20% Impervious, Inflow Depth = 4.15" for 10-Year event  
 Inflow = 0.59 cfs @ 12.07 hrs, Volume= 1,964 cf  
 Outflow = 0.14 cfs @ 12.44 hrs, Volume= 1,966 cf, Atten= 76%, Lag= 22.3 min  
 Discarded = 0.14 cfs @ 12.44 hrs, Volume= 1,966 cf  
 Primary = 0.00 cfs @ 0.00 hrs, Volume= 0 cf

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3

Peak Elev= 35.10' @ 12.44 hrs Surf.Area= 753 sf Storage= 391 cf

Flood Elev= 37.00' Surf.Area= 753 sf Storage= 904 cf

Plug-Flow detention time= (not calculated: outflow precedes inflow)

Center-of-Mass det. time= 13.3 min ( 772.4 - 759.1 )

Volume	Invert	Avail.Storage	Storage Description
#1	33.80'	904 cf	<b>Subbase (Conic)</b> Listed below (Recalc) 2,259 cf Overall x 40.0% Voids

Elevation (feet)	Surf.Area (sq-ft)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)	Wet.Area (sq-ft)
33.80	753	0	0	753
36.80	753	2,259	2,259	1,045

Device	Routing	Invert	Outlet Devices
#1	Discarded	33.80'	<b>7.000 in/hr Exfiltration over Wetted area</b>
#2	Primary	36.80'	<b>4.5' long x 3.0' breadth Broad-Crested Rectangular Weir</b>
			Head (feet) 0.20 0.40 0.60 0.80 1.00 1.20 1.40 1.60 1.80 2.00 2.50 3.00 3.50 4.00 4.50
			Coef. (English) 2.44 2.58 2.68 2.67 2.65 2.64 2.64 2.68 2.68 2.72 2.81 2.92 2.97 3.07 3.32

**Discarded OutFlow** Max=0.14 cfs @ 12.44 hrs HW=35.10' (Free Discharge)

↑1=Exfiltration (Exfiltration Controls 0.14 cfs)

**Primary OutFlow** Max=0.00 cfs @ 0.00 hrs HW=33.80' TW=0.00' (Dynamic Tailwater)

↑2=Broad-Crested Rectangular Weir( Controls 0.00 cfs)

**Summary for Pond PP#2: Pervious Pave #1**

Inflow Area = 972 sf, 55.84% Impervious, Inflow Depth = 2.12" for 10-Year event  
 Inflow = 0.07 cfs @ 12.00 hrs, Volume= 172 cf  
 Outflow = 0.07 cfs @ 12.00 hrs, Volume= 172 cf, Atten= 0%, Lag= 0.0 min  
 Discarded = 0.07 cfs @ 12.00 hrs, Volume= 172 cf  
 Primary = 0.00 cfs @ 0.00 hrs, Volume= 0 cf

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3

Peak Elev= 35.30' @ 12.00 hrs Surf.Area= 546 sf Storage= 0 cf

Flood Elev= 37.00' Surf.Area= 546 sf Storage= 328 cf

Plug-Flow detention time= (not calculated: outflow precedes inflow)

Center-of-Mass det. time= 0.0 min ( 838.0 - 838.0 )

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Type III 24-hr 10-Year Rainfall=4.90"

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Volume	Invert	Avail.Storage	Storage Description
#1	35.30'	328 cf	<b>Subbase (Conic)</b> Listed below (Recalc) 819 cf Overall x 40.0% Voids

Elevation (feet)	Surf.Area (sq-ft)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)	Wet.Area (sq-ft)
35.30	546	0	0	546
36.80	546	819	819	670

Device	Routing	Invert	Outlet Devices
#1	Discarded	35.30'	<b>7.000 in/hr Exfiltration over Wetted area</b>
#2	Primary	36.80'	<b>4.5' long x 3.0' breadth Broad-Crested Rectangular Weir</b>
			Head (feet) 0.20 0.40 0.60 0.80 1.00 1.20 1.40 1.60 1.80 2.00 2.50 3.00 3.50 4.00 4.50
			Coef. (English) 2.44 2.58 2.68 2.67 2.65 2.64 2.64 2.68 2.68 2.72 2.81 2.92 2.97 3.07 3.32

**Discarded OutFlow** Max=0.09 cfs @ 12.00 hrs HW=35.30' (Free Discharge)↑**1=Exfiltration** (Exfiltration Controls 0.09 cfs)**Primary OutFlow** Max=0.00 cfs @ 0.00 hrs HW=35.30' TW=0.00' (Dynamic Tailwater)↑**2=Broad-Crested Rectangular Weir**( Controls 0.00 cfs)

**2729-03\_Proposed-Conditions**

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Type III 24-hr 100-Year Rainfall=8.89"

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Time span=0.00-72.00 hrs, dt=0.01 hrs, 7201 points x 3

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN

Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

**SubcatchmentP-1: Subcat P-1**Runoff Area=4,358 sf 99.99% Impervious Runoff Depth=8.65"  
Tc=5.0 min CN=98 Runoff=0.91 cfs 3,142 cf**SubcatchmentP-2: Subcat P-2**Runoff Area=1,234 sf 87.57% Impervious Runoff Depth=7.80"  
Tc=5.0 min CN=91 Runoff=0.25 cfs 803 cf**SubcatchmentP-4: Subcat P-4**Runoff Area=972 sf 55.84% Impervious Runoff Depth=5.48"  
Tc=0.0 min CN=72 Runoff=0.18 cfs 444 cf**SubcatchmentP-5: Subcat P-5**Runoff Area=11,040 sf 100.00% Impervious Runoff Depth=8.65"  
Tc=5.0 min CN=98 Runoff=2.29 cfs 7,958 cf**SubcatchmentP3: Subcat P3**Runoff Area=1,324 sf 62.26% Impervious Runoff Depth=5.97"  
Tc=5.0 min CN=76 Runoff=0.22 cfs 659 cf**Reach SP1: Study Point 1**Inflow=0.50 cfs 975 cf  
Outflow=0.50 cfs 975 cf**Pond 1P: Infil. System**Peak Elev=34.98' Storage=3,078 cf Inflow=2.29 cfs 7,958 cf  
Discarded=0.14 cfs 7,958 cf Secondary=0.00 cfs 0 cf Outflow=0.14 cfs 7,958 cf**Pond PP#1: Pervious Pave #1**Peak Elev=36.91' Storage=904 cf Inflow=1.12 cfs 3,801 cf  
Discarded=0.17 cfs 3,629 cf Primary=0.40 cfs 172 cf Outflow=0.57 cfs 3,801 cf**Pond PP#2: Pervious Pave #1**Peak Elev=35.42' Storage=26 cf Inflow=0.18 cfs 444 cf  
Discarded=0.09 cfs 444 cf Primary=0.00 cfs 0 cf Outflow=0.09 cfs 444 cf**Total Runoff Area = 18,929 sf Runoff Volume = 13,005 cf Average Runoff Depth = 8.24"**  
**5.72% Pervious = 1,082 sf 94.28% Impervious = 17,846 sf**



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**Summary for Subcatchment P-1: Subcat P-1**

Runoff = 0.91 cfs @ 12.07 hrs, Volume= 3,142 cf, Depth= 8.65"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs

Type III 24-hr 100-Year Rainfall=8.89"

Area (sf)	CN	Description
0	98	Paved parking, HSG A
0	39	>75% Grass cover, Good, HSG A
4,358	98	Roofs, HSG A
4,358	98	Weighted Average
0		0.01% Pervious Area
4,358		99.99% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Assumed

**Summary for Subcatchment P-2: Subcat P-2**

Runoff = 0.25 cfs @ 12.07 hrs, Volume= 803 cf, Depth= 7.80"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs

Type III 24-hr 100-Year Rainfall=8.89"

Area (sf)	CN	Description
1,081	98	Paved parking, HSG A
153	39	>75% Grass cover, Good, HSG A
1,234	91	Weighted Average
153		12.43% Pervious Area
1,081		87.57% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Min tc

**Summary for Subcatchment P-4: Subcat P-4**

Runoff = 0.18 cfs @ 12.00 hrs, Volume= 444 cf, Depth= 5.48"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs

Type III 24-hr 100-Year Rainfall=8.89"

Area (sf)	CN	Description
543	98	Paved parking, HSG A
429	39	>75% Grass cover, Good, HSG A
972	72	Weighted Average
429		44.16% Pervious Area
543		55.84% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
0.0					Direct Entry, min tc

**Summary for Subcatchment P-5: Subcat P-5**

Runoff = 2.29 cfs @ 12.07 hrs, Volume= 7,958 cf, Depth= 8.65"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs

Type III 24-hr 100-Year Rainfall=8.89"

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Area (sf)	CN	Description
10,685	98	Roofs, HSG A
355	98	Paved parking, HSG A
11,040	98	Weighted Average
11,040		100.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, min tc

**Summary for Subcatchment P3: Subcat P3**

Runoff = 0.22 cfs @ 12.07 hrs, Volume= 659 cf, Depth= 5.97"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs  
Type III 24-hr 100-Year Rainfall=8.89"

Area (sf)	CN	Description
500	39	>75% Grass cover, Good, HSG A
825	98	Paved parking, HSG A
1,324	76	Weighted Average
500		37.74% Pervious Area
825		62.26% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, min tc

**Summary for Reach SP1: Study Point 1**Inflow Area = 7,889 sf, 86.28% Impervious, Inflow Depth = 1.48" for 100-Year event  
Inflow = 0.50 cfs @ 12.24 hrs, Volume= 975 cf  
Outflow = 0.50 cfs @ 12.24 hrs, Volume= 975 cf, Atten= 0%, Lag= 0.0 min

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3

**Summary for Pond 1P: Infil. System**Inflow Area = 11,040 sf, 100.00% Impervious, Inflow Depth = 8.65" for 100-Year event  
Inflow = 2.29 cfs @ 12.07 hrs, Volume= 7,958 cf  
Outflow = 0.14 cfs @ 11.19 hrs, Volume= 7,958 cf, Atten= 94%, Lag= 0.0 min  
Discarded = 0.14 cfs @ 11.19 hrs, Volume= 7,958 cf  
Secondary = 0.00 cfs @ 0.00 hrs, Volume= 0 cf

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3

Peak Elev= 34.98' @ 13.49 hrs Surf.Area= 885 sf Storage= 3,078 cf

Flood Elev= 37.00' Surf.Area= 885 sf Storage= 3,172 cf

Plug-Flow detention time= (not calculated: outflow precedes inflow)

Center-of-Mass det. time= 159.1 min ( 898.1 - 739.0 )

Volume	Invert	Avail.Storage	Storage Description
#1A	29.00'	1,573 cf	<b>15.83'W x 55.89'L x 6.25'H Field A</b> 5,531 cf Overall - 1,599 cf Embedded = 3,932 cf x 40.0% Voids
#2A	30.50'	1,599 cf	<b>ADS_StormTech MC-3500 d +Cap x 14 Inside #1</b> Effective Size= 70.4"W x 45.0"H => 15.33 sf x 7.17'L = 110.0 cf Overall Size= 77.0"W x 45.0"H x 7.50'L with 0.33' Overlap 14 Chambers in 2 Rows Cap Storage= +14.9 cf x 2 x 2 rows = 59.6 cf
		3,172 cf	Total Available Storage

Storage Group A created with Chamber Wizard

**2729-03\_Proposed-Conditions**

Type III 24-hr 100-Year Rainfall=8.89"

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Device	Routing	Invert	Outlet Devices
#1	Discarded	29.00'	<b>7.000 in/hr Exfiltration over Surface area</b>
#2	Secondary	36.00'	<b>5.0' long x 0.5' breadth Broad-Crested Rectangular Weir</b>
			Head (feet) 0.20 0.40 0.60 0.80 1.00
			Coef. (English) 2.80 2.92 3.08 3.30 3.32

**Discarded OutFlow** Max=0.14 cfs @ 11.19 hrs HW=29.08' (Free Discharge)

└─1=Exfiltration (Exfiltration Controls 0.14 cfs)

**Secondary OutFlow** Max=0.00 cfs @ 0.00 hrs HW=29.00' TW=0.00' (Dynamic Tailwater)

└─2=Broad-Crested Rectangular Weir( Controls 0.00 cfs)

**Summary for Pond PP#1: Pervious Pave #1**

Inflow Area = 5,683 sf, 91.20% Impervious, Inflow Depth = 8.03" for 100-Year event  
 Inflow = 1.12 cfs @ 12.07 hrs, Volume= 3,801 cf  
 Outflow = 0.57 cfs @ 12.24 hrs, Volume= 3,801 cf, Atten= 50%, Lag= 10.3 min  
 Discarded = 0.17 cfs @ 12.24 hrs, Volume= 3,629 cf  
 Primary = 0.40 cfs @ 12.24 hrs, Volume= 172 cf

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3

Peak Elev= 36.91' @ 12.24 hrs Surf.Area= 753 sf Storage= 904 cf

Flood Elev= 37.00' Surf.Area= 753 sf Storage= 904 cf

Plug-Flow detention time= (not calculated: outflow precedes inflow)

Center-of-Mass det. time= 30.9 min ( 781.7 - 750.8 )

Volume	Invert	Avail.Storage	Storage Description
#1	33.80'	904 cf	<b>Subbase (Conic)</b> Listed below (Recalc) 2,259 cf Overall x 40.0% Voids

Elevation (feet)	Surf.Area (sq-ft)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)	Wet.Area (sq-ft)
33.80	753	0	0	753
36.80	753	2,259	2,259	1,045

Device	Routing	Invert	Outlet Devices
#1	Discarded	33.80'	<b>7.000 in/hr Exfiltration over Wetted area</b>
#2	Primary	36.80'	<b>4.5' long x 3.0' breadth Broad-Crested Rectangular Weir</b>
			Head (feet) 0.20 0.40 0.60 0.80 1.00 1.20 1.40 1.60 1.80 2.00 2.50 3.00 3.50 4.00 4.50
			Coef. (English) 2.44 2.58 2.68 2.67 2.65 2.64 2.64 2.68 2.68 2.72 2.81 2.92 2.97 3.07 3.32

**Discarded OutFlow** Max=0.17 cfs @ 12.24 hrs HW=36.91' (Free Discharge)

└─1=Exfiltration (Exfiltration Controls 0.17 cfs)

**Primary OutFlow** Max=0.35 cfs @ 12.24 hrs HW=36.90' TW=0.00' (Dynamic Tailwater)

└─2=Broad-Crested Rectangular Weir(Weir Controls 0.35 cfs @ 0.77 fps)

**Summary for Pond PP#2: Pervious Pave #1**

Inflow Area = 972 sf, 55.84% Impervious, Inflow Depth = 5.48" for 100-Year event  
 Inflow = 0.18 cfs @ 12.00 hrs, Volume= 444 cf  
 Outflow = 0.09 cfs @ 12.09 hrs, Volume= 444 cf, Atten= 49%, Lag= 5.1 min  
 Discarded = 0.09 cfs @ 12.09 hrs, Volume= 444 cf  
 Primary = 0.00 cfs @ 0.00 hrs, Volume= 0 cf

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3

Peak Elev= 35.42' @ 12.09 hrs Surf.Area= 546 sf Storage= 26 cf

Flood Elev= 37.00' Surf.Area= 546 sf Storage= 328 cf

Plug-Flow detention time= (not calculated: outflow precedes inflow)

Center-of-Mass det. time= 0.9 min ( 811.4 - 810.6 )



**2729-03\_Proposed-Conditions**

Type III 24-hr 100-Year Rainfall=8.89"

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Volume	Invert	Avail.Storage	Storage Description
#1	35.30'	328 cf	<b>Subbase (Conic)</b> Listed below (Recalc) 819 cf Overall x 40.0% Voids

Elevation (feet)	Surf.Area (sq-ft)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)	Wet.Area (sq-ft)
35.30	546	0	0	546
36.80	546	819	819	670

Device	Routing	Invert	Outlet Devices
#1	Discarded	35.30'	<b>7.000 in/hr Exfiltration over Wetted area</b>
#2	Primary	36.80'	<b>4.5' long x 3.0' breadth Broad-Crested Rectangular Weir</b> Head (feet) 0.20 0.40 0.60 0.80 1.00 1.20 1.40 1.60 1.80 2.00 2.50 3.00 3.50 4.00 4.50 Coef. (English) 2.44 2.58 2.68 2.67 2.65 2.64 2.64 2.68 2.68 2.72 2.81 2.92 2.97 3.07 3.32

**Discarded OutFlow** Max=0.09 cfs @ 12.09 hrs HW=35.42' (Free Discharge)↑**1=Exfiltration** (Exfiltration Controls 0.09 cfs)**Primary OutFlow** Max=0.00 cfs @ 0.00 hrs HW=35.30' TW=0.00' (Dynamic Tailwater)↑**2=Broad-Crested Rectangular Weir**( Controls 0.00 cfs)

# Extreme Precipitation Tables

## Northeast Regional Climate Center

*Data represents point estimates calculated from partial duration series. All precipitation amounts are displayed in inches.*

Smoothing	Yes
State	Massachusetts
Location	
Longitude	71.142 degrees West
Latitude	42.405 degrees North
Elevation	0 feet
Date/Time	Fri, 28 Aug 2020 14:10:00 -0400

### Extreme Precipitation Estimates

	5min	10min	15min	30min	60min	120min	1hr	2hr	3hr	6hr	12hr	24hr	48hr		1day	2day	4day	7day	10day	
1yr	0.28	0.43	0.53	0.70	0.87	1.10	0.75	1.04	1.28	1.63	2.09	2.69	2.94	1yr	2.38	2.83	3.29	3.98	4.65	1yr
2yr	0.35	0.54	0.67	0.88	1.11	1.40	0.96	1.28	1.62	2.04	2.57	3.23	3.59	2yr	2.86	3.45	3.95	4.70	5.35	2yr
5yr	0.42	0.65	0.81	1.09	1.39	1.77	1.20	1.61	2.06	2.60	3.26	4.09	4.56	5yr	3.62	4.38	5.00	5.97	6.69	5yr
10yr	0.47	0.74	0.93	1.27	1.65	2.12	1.42	1.91	2.47	3.12	3.92	4.90	5.47	10yr	4.33	5.26	5.99	7.15	7.92	10yr
25yr	0.56	0.89	1.13	1.56	2.06	2.67	1.78	2.40	3.13	3.96	4.98	6.20	6.96	25yr	5.49	6.69	7.59	9.10	9.91	25yr
50yr	0.63	1.01	1.30	1.82	2.45	3.21	2.12	2.86	3.77	4.78	5.98	7.43	8.36	50yr	6.57	8.03	9.08	10.92	11.75	50yr
100yr	0.73	1.18	1.52	2.14	2.92	3.84	2.52	3.40	4.52	5.73	7.17	8.89	10.04	100yr	7.87	9.65	10.88	13.10	13.94	100yr
200yr	0.83	1.36	1.76	2.52	3.47	4.60	2.99	4.05	5.43	6.89	8.61	10.65	12.07	200yr	9.43	11.60	13.03	15.73	16.54	200yr
500yr	1.01	1.65	2.16	3.13	4.37	5.83	3.77	5.11	6.90	8.77	10.97	13.54	15.40	500yr	11.98	14.81	16.55	20.05	20.75	500yr

### Lower Confidence Limits

	5min	10min	15min	30min	60min	120min	1hr	2hr	3hr	6hr	12hr	24hr	48hr	1day	2day	4day	7day	10day		
1yr	0.25	0.38	0.46	0.62	0.76	0.85	0.66	0.83	1.15	1.44	1.78	2.44	2.50	1yr	2.16	2.41	2.93	3.53	4.05	1yr
2yr	0.33	0.51	0.63	0.85	1.05	1.26	0.91	1.23	1.45	1.91	2.48	3.13	3.47	2yr	2.77	3.33	3.82	4.53	5.18	2yr
5yr	0.39	0.60	0.75	1.02	1.30	1.51	1.12	1.47	1.73	2.24	2.89	3.77	4.18	5yr	3.34	4.02	4.59	5.47	6.17	5yr
10yr	0.44	0.67	0.83	1.16	1.50	1.73	1.29	1.69	1.95	2.53	3.24	4.35	4.83	10yr	3.85	4.65	5.27	6.29	7.01	10yr

	5min	10min	15min	30min	60min	120min	1hr	2hr	3hr	6hr	12hr	24hr	48hr	1day	2day	4day	7day	10day	
25yr	0.50	0.77	0.95	1.36	1.79	2.05	1.54	2.00	2.31	2.96	3.78	5.23	5.82	4.63	5.60	6.31	7.52	8.29	25yr
50yr	0.56	0.85	1.06	1.52	2.05	2.35	1.77	2.30	2.61	3.34	4.24	5.99	6.70	5.30	6.44	7.22	8.60	9.39	50yr
100yr	0.63	0.95	1.18	1.71	2.35	2.68	2.03	2.62	2.96	3.62	4.77	6.89	7.70	6.10	7.41	8.27	9.79	10.65	100yr
200yr	0.70	1.06	1.34	1.94	2.71	3.06	2.34	2.99	3.36	4.05	5.37	7.91	8.86	7.00	8.52	9.46	11.12	12.03	200yr
500yr	0.82	1.23	1.58	2.29	3.26	3.65	2.81	3.57	3.97	4.70	6.29	9.50	10.64	8.41	10.23	11.30	13.12	14.12	500yr

## Upper Confidence Limits

	5min	10min	15min	30min	60min	120min	1hr	2hr	3hr	6hr	12hr	24hr	48hr	1day	2day	4day	7day	10day	
1yr	0.31	0.48	0.58	0.79	0.97	1.13	0.83	1.11	1.32	1.77	2.25	2.86	3.17	2.53	3.05	3.51	4.29	5.03	1yr
2yr	0.36	0.56	0.69	0.94	1.15	1.36	1.00	1.33	1.57	2.08	2.68	3.35	3.74	2.97	3.59	4.11	4.89	5.55	2yr
5yr	0.45	0.70	0.86	1.19	1.51	1.79	1.30	1.75	2.05	2.66	3.39	4.44	5.00	3.93	4.81	5.43	6.48	7.21	5yr
10yr	0.55	0.84	1.05	1.46	1.89	2.20	1.63	2.15	2.55	3.22	4.07	5.51	6.25	4.88	6.01	6.72	8.04	8.83	10yr
25yr	0.71	1.08	1.35	1.92	2.53	2.90	2.19	2.83	3.39	4.16	5.17	7.32	8.42	6.48	8.09	8.92	10.74	11.56	25yr
50yr	0.86	1.31	1.64	2.35	3.17	3.59	2.73	3.51	4.21	5.05	6.22	9.08	10.54	8.04	10.14	11.04	13.40	14.18	50yr
100yr	1.06	1.60	2.00	2.89	3.96	4.42	3.42	4.32	5.22	6.37	7.47	11.28	13.22	9.98	12.71	13.68	16.75	17.43	100yr
200yr	1.29	1.94	2.45	3.55	4.95	5.46	4.27	5.34	6.49	7.78	8.96	14.02	16.60	12.41	15.96	16.97	20.95	21.46	200yr
500yr	1.68	2.50	3.21	4.67	6.63	7.20	5.72	7.04	8.66	10.14	11.41	18.71	22.44	16.56	21.58	22.57	28.20	28.29	500yr





United States  
Department of  
Agriculture

**NRCS**

Natural  
Resources  
Conservation  
Service

A product of the National  
Cooperative Soil Survey,  
a joint effort of the United  
States Department of  
Agriculture and other  
Federal agencies, State  
agencies including the  
Agricultural Experiment  
Stations, and local  
participants

# Custom Soil Resource Report for **Middlesex County, Massachusetts**



# Preface

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Soil surveys contain information that affects land use planning in survey areas. They highlight soil limitations that affect various land uses and provide information about the properties of the soils in the survey areas. Soil surveys are designed for many different users, including farmers, ranchers, foresters, agronomists, urban planners, community officials, engineers, developers, builders, and home buyers. Also, conservationists, teachers, students, and specialists in recreation, waste disposal, and pollution control can use the surveys to help them understand, protect, or enhance the environment.

Various land use regulations of Federal, State, and local governments may impose special restrictions on land use or land treatment. Soil surveys identify soil properties that are used in making various land use or land treatment decisions. The information is intended to help the land users identify and reduce the effects of soil limitations on various land uses. The landowner or user is responsible for identifying and complying with existing laws and regulations.

Although soil survey information can be used for general farm, local, and wider area planning, onsite investigation is needed to supplement this information in some cases. Examples include soil quality assessments (<http://www.nrcs.usda.gov/wps/portal/nrcs/main/soils/health/>) and certain conservation and engineering applications. For more detailed information, contact your local USDA Service Center (<https://offices.sc.egov.usda.gov/locator/app?agency=nrcs>) or your NRCS State Soil Scientist ([http://www.nrcs.usda.gov/wps/portal/nrcs/detail/soils/contactus/?cid=nrcs142p2\\_053951](http://www.nrcs.usda.gov/wps/portal/nrcs/detail/soils/contactus/?cid=nrcs142p2_053951)).

Great differences in soil properties can occur within short distances. Some soils are seasonally wet or subject to flooding. Some are too unstable to be used as a foundation for buildings or roads. Clayey or wet soils are poorly suited to use as septic tank absorption fields. A high water table makes a soil poorly suited to basements or underground installations.

The National Cooperative Soil Survey is a joint effort of the United States Department of Agriculture and other Federal agencies, State agencies including the Agricultural Experiment Stations, and local agencies. The Natural Resources Conservation Service (NRCS) has leadership for the Federal part of the National Cooperative Soil Survey.

Information about soils is updated periodically. Updated information is available through the NRCS Web Soil Survey, the site for official soil survey information.

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# How Soil Surveys Are Made

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Soil surveys are made to provide information about the soils and miscellaneous areas in a specific area. They include a description of the soils and miscellaneous areas and their location on the landscape and tables that show soil properties and limitations affecting various uses. Soil scientists observed the steepness, length, and shape of the slopes; the general pattern of drainage; the kinds of crops and native plants; and the kinds of bedrock. They observed and described many soil profiles. A soil profile is the sequence of natural layers, or horizons, in a soil. The profile extends from the surface down into the unconsolidated material in which the soil formed or from the surface down to bedrock. The unconsolidated material is devoid of roots and other living organisms and has not been changed by other biological activity.

Currently, soils are mapped according to the boundaries of major land resource areas (MLRAs). MLRAs are geographically associated land resource units that share common characteristics related to physiography, geology, climate, water resources, soils, biological resources, and land uses (USDA, 2006). Soil survey areas typically consist of parts of one or more MLRA.

The soils and miscellaneous areas in a survey area occur in an orderly pattern that is related to the geology, landforms, relief, climate, and natural vegetation of the area. Each kind of soil and miscellaneous area is associated with a particular kind of landform or with a segment of the landform. By observing the soils and miscellaneous areas in the survey area and relating their position to specific segments of the landform, a soil scientist develops a concept, or model, of how they were formed. Thus, during mapping, this model enables the soil scientist to predict with a considerable degree of accuracy the kind of soil or miscellaneous area at a specific location on the landscape.

Commonly, individual soils on the landscape merge into one another as their characteristics gradually change. To construct an accurate soil map, however, soil scientists must determine the boundaries between the soils. They can observe only a limited number of soil profiles. Nevertheless, these observations, supplemented by an understanding of the soil-vegetation-landscape relationship, are sufficient to verify predictions of the kinds of soil in an area and to determine the boundaries.

Soil scientists recorded the characteristics of the soil profiles that they studied. They noted soil color, texture, size and shape of soil aggregates, kind and amount of rock fragments, distribution of plant roots, reaction, and other features that enable them to identify soils. After describing the soils in the survey area and determining their properties, the soil scientists assigned the soils to taxonomic classes (units). Taxonomic classes are concepts. Each taxonomic class has a set of soil characteristics with precisely defined limits. The classes are used as a basis for comparison to classify soils systematically. Soil taxonomy, the system of taxonomic classification used in the United States, is based mainly on the kind and character of soil properties and the arrangement of horizons within the profile. After the soil

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scientists classified and named the soils in the survey area, they compared the individual soils with similar soils in the same taxonomic class in other areas so that they could confirm data and assemble additional data based on experience and research.

The objective of soil mapping is not to delineate pure map unit components; the objective is to separate the landscape into landforms or landform segments that have similar use and management requirements. Each map unit is defined by a unique combination of soil components and/or miscellaneous areas in predictable proportions. Some components may be highly contrasting to the other components of the map unit. The presence of minor components in a map unit in no way diminishes the usefulness or accuracy of the data. The delineation of such landforms and landform segments on the map provides sufficient information for the development of resource plans. If intensive use of small areas is planned, onsite investigation is needed to define and locate the soils and miscellaneous areas.

Soil scientists make many field observations in the process of producing a soil map. The frequency of observation is dependent upon several factors, including scale of mapping, intensity of mapping, design of map units, complexity of the landscape, and experience of the soil scientist. Observations are made to test and refine the soil-landscape model and predictions and to verify the classification of the soils at specific locations. Once the soil-landscape model is refined, a significantly smaller number of measurements of individual soil properties are made and recorded. These measurements may include field measurements, such as those for color, depth to bedrock, and texture, and laboratory measurements, such as those for content of sand, silt, clay, salt, and other components. Properties of each soil typically vary from one point to another across the landscape.

Observations for map unit components are aggregated to develop ranges of characteristics for the components. The aggregated values are presented. Direct measurements do not exist for every property presented for every map unit component. Values for some properties are estimated from combinations of other properties.

While a soil survey is in progress, samples of some of the soils in the area generally are collected for laboratory analyses and for engineering tests. Soil scientists interpret the data from these analyses and tests as well as the field-observed characteristics and the soil properties to determine the expected behavior of the soils under different uses. Interpretations for all of the soils are field tested through observation of the soils in different uses and under different levels of management. Some interpretations are modified to fit local conditions, and some new interpretations are developed to meet local needs. Data are assembled from other sources, such as research information, production records, and field experience of specialists. For example, data on crop yields under defined levels of management are assembled from farm records and from field or plot experiments on the same kinds of soil.

Predictions about soil behavior are based not only on soil properties but also on such variables as climate and biological activity. Soil conditions are predictable over long periods of time, but they are not predictable from year to year. For example, soil scientists can predict with a fairly high degree of accuracy that a given soil will have a high water table within certain depths in most years, but they cannot predict that a high water table will always be at a specific level in the soil on a specific date.

After soil scientists located and identified the significant natural bodies of soil in the survey area, they drew the boundaries of these bodies on aerial photographs and



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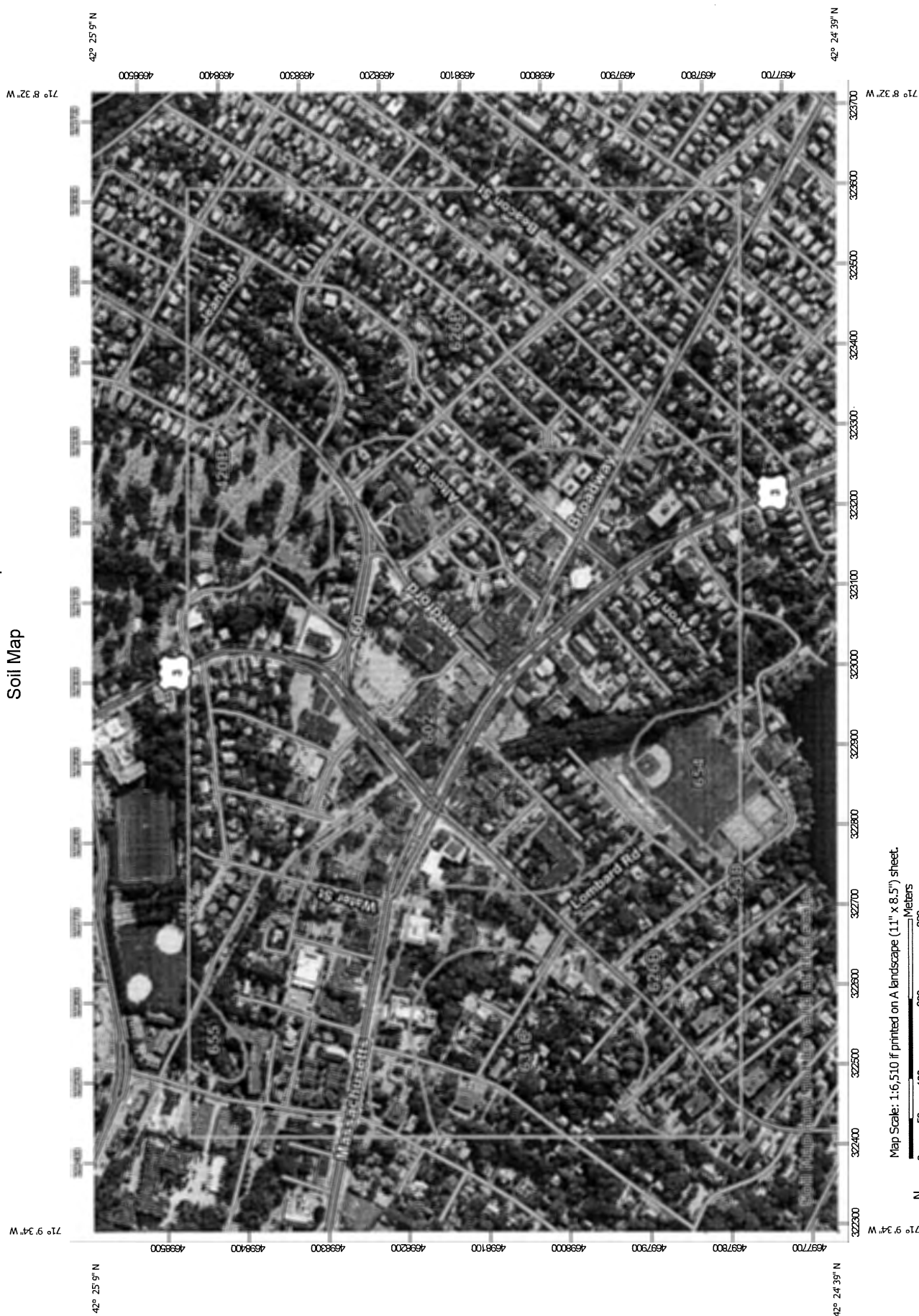
identified each as a specific map unit. Aerial photographs show trees, buildings, fields, roads, and rivers, all of which help in locating boundaries accurately.

# Soil Map

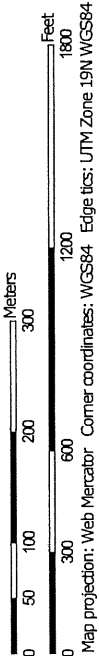
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The soil map section includes the soil map for the defined area of interest, a list of soil map units on the map and extent of each map unit, and cartographic symbols displayed on the map. Also presented are various metadata about data used to produce the map, and a description of each soil map unit.

# Custom Soil Resource Report Soil Map



Map Scale: 1:6,510 if printed on A landscape (11" x 8.5") sheet.



Map projection: Web Mercator Corner coordinates: WGS84 Edge tics: UTM Zone 19N WGS84



MAP LEGEND

MAP INFORMATION

- Area of Interest (AOI)  
Area of Interest (AOI)
- Soils  
Soil Map Unit Polygons  
Soil Map Unit Lines  
Soil Map Unit Points
- Special Point Features  
Blowout  
Borrow Pit  
Clay Spot  
Closed Depression  
Gravel Pit  
Gravelly Spot  
Landfill  
Lava Flow  
Marsh or swamp  
Mine or Quarry  
Miscellaneous Water  
Perennial Water  
Rock Outcrop  
Saline Spot  
Sandy Spot  
Severely Eroded Spot  
Sinkhole  
Slide or Slip  
Sodic Spot
- Spoil Area  
Stony Spot  
Very Stony Spot  
Wet Spot  
Other  
Special Line Features  
Water Features  
Streams and Canals
- Transportation  
Rails  
Interstate Highways  
US Routes  
Major Roads  
Local Roads
- Background  
Aerial Photography

The soil surveys that comprise your AOI were mapped at 1:25,000.

Warning: Soil Map may not be valid at this scale.

Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service  
Web Soil Survey URL:  
Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distance or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Middlesex County, Massachusetts  
Survey Area Data: Version 20, Jun 9, 2020

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Aug 13, 2020—Sep 15, 2020

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

## Map Unit Legend

Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
253D	Hinckley loamy sand, 15 to 25 percent slopes	0.1	0.0%
420B	Canton fine sandy loam, 3 to 8 percent slopes	2.8	1.4%
602	Urban land	97.4	48.4%
626B	Merrimac-Urban land complex, 0 to 8 percent slopes	80.9	40.2%
631C	Charlton-Urban land-Hollis complex, 3 to 15 percent slopes, rocky	14.2	7.0%
654	Udorthents, loamy	4.7	2.3%
655	Udorthents, wet substratum	1.4	0.7%
<b>Totals for Area of Interest</b>		<b>201.4</b>	<b>100.0%</b>

## Map Unit Descriptions

The map units delineated on the detailed soil maps in a soil survey represent the soils or miscellaneous areas in the survey area. The map unit descriptions, along with the maps, can be used to determine the composition and properties of a unit.

A map unit delineation on a soil map represents an area dominated by one or more major kinds of soil or miscellaneous areas. A map unit is identified and named according to the taxonomic classification of the dominant soils. Within a taxonomic class there are precisely defined limits for the properties of the soils. On the landscape, however, the soils are natural phenomena, and they have the characteristic variability of all natural phenomena. Thus, the range of some observed properties may extend beyond the limits defined for a taxonomic class. Areas of soils of a single taxonomic class rarely, if ever, can be mapped without including areas of other taxonomic classes. Consequently, every map unit is made up of the soils or miscellaneous areas for which it is named and some minor components that belong to taxonomic classes other than those of the major soils.

Most minor soils have properties similar to those of the dominant soil or soils in the map unit, and thus they do not affect use and management. These are called noncontrasting, or similar, components. They may or may not be mentioned in a particular map unit description. Other minor components, however, have properties and behavioral characteristics divergent enough to affect use or to require different management. These are called contrasting, or dissimilar, components. They generally are in small areas and could not be mapped separately because of the scale used. Some small areas of strongly contrasting soils or miscellaneous areas are identified by a special symbol on the maps. If included in the database for a given area, the contrasting minor components are identified in the map unit descriptions along with some characteristics of each. A few areas of minor components may not have been observed, and consequently they are not

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mentioned in the descriptions, especially where the pattern was so complex that it was impractical to make enough observations to identify all the soils and miscellaneous areas on the landscape.

The presence of minor components in a map unit in no way diminishes the usefulness or accuracy of the data. The objective of mapping is not to delineate pure taxonomic classes but rather to separate the landscape into landforms or landform segments that have similar use and management requirements. The delineation of such segments on the map provides sufficient information for the development of resource plans. If intensive use of small areas is planned, however, onsite investigation is needed to define and locate the soils and miscellaneous areas.

An identifying symbol precedes the map unit name in the map unit descriptions. Each description includes general facts about the unit and gives important soil properties and qualities.

Soils that have profiles that are almost alike make up a *soil series*. Except for differences in texture of the surface layer, all the soils of a series have major horizons that are similar in composition, thickness, and arrangement.

Soils of one series can differ in texture of the surface layer, slope, stoniness, salinity, degree of erosion, and other characteristics that affect their use. On the basis of such differences, a soil series is divided into *soil phases*. Most of the areas shown on the detailed soil maps are phases of soil series. The name of a soil phase commonly indicates a feature that affects use or management. For example, Alpha silt loam, 0 to 2 percent slopes, is a phase of the Alpha series.

Some map units are made up of two or more major soils or miscellaneous areas. These map units are complexes, associations, or undifferentiated groups.

A *complex* consists of two or more soils or miscellaneous areas in such an intricate pattern or in such small areas that they cannot be shown separately on the maps. The pattern and proportion of the soils or miscellaneous areas are somewhat similar in all areas. Alpha-Beta complex, 0 to 6 percent slopes, is an example.

An *association* is made up of two or more geographically associated soils or miscellaneous areas that are shown as one unit on the maps. Because of present or anticipated uses of the map units in the survey area, it was not considered practical or necessary to map the soils or miscellaneous areas separately. The pattern and relative proportion of the soils or miscellaneous areas are somewhat similar. Alpha-Beta association, 0 to 2 percent slopes, is an example.

An *undifferentiated group* is made up of two or more soils or miscellaneous areas that could be mapped individually but are mapped as one unit because similar interpretations can be made for use and management. The pattern and proportion of the soils or miscellaneous areas in a mapped area are not uniform. An area can be made up of only one of the major soils or miscellaneous areas, or it can be made up of all of them. Alpha and Beta soils, 0 to 2 percent slopes, is an example.

Some surveys include *miscellaneous areas*. Such areas have little or no soil material and support little or no vegetation. Rock outcrop is an example.



## Middlesex County, Massachusetts

### 253D—Hinckley loamy sand, 15 to 25 percent slopes

#### Map Unit Setting

*National map unit symbol:* 2svmc  
*Elevation:* 0 to 1,460 feet  
*Mean annual precipitation:* 36 to 71 inches  
*Mean annual air temperature:* 39 to 55 degrees F  
*Frost-free period:* 140 to 240 days  
*Farmland classification:* Not prime farmland

#### Map Unit Composition

*Hinckley and similar soils:* 85 percent  
*Minor components:* 15 percent  
*Estimates are based on observations, descriptions, and transects of the mapunit.*

#### Description of Hinckley

##### Setting

*Landform:* Outwash terraces, outwash deltas, kame terraces, kames, outwash plains, eskers, moraines  
*Landform position (two-dimensional):* Backslope  
*Landform position (three-dimensional):* Nose slope, side slope, crest, head slope, riser  
*Down-slope shape:* Linear, concave, convex  
*Across-slope shape:* Convex, linear, concave  
*Parent material:* Sandy and gravelly glaciofluvial deposits derived from gneiss and/or granite and/or schist

##### Typical profile

*Oe - 0 to 1 inches:* moderately decomposed plant material  
*A - 1 to 8 inches:* loamy sand  
*Bw1 - 8 to 11 inches:* gravelly loamy sand  
*Bw2 - 11 to 16 inches:* gravelly loamy sand  
*BC - 16 to 19 inches:* very gravelly loamy sand  
*C - 19 to 65 inches:* very gravelly sand

##### Properties and qualities

*Slope:* 15 to 25 percent  
*Depth to restrictive feature:* More than 80 inches  
*Drainage class:* Excessively drained  
*Runoff class:* Low  
*Capacity of the most limiting layer to transmit water (Ksat):* Moderately high to very high (1.42 to 99.90 in/hr)  
*Depth to water table:* More than 80 inches  
*Frequency of flooding:* None  
*Frequency of ponding:* None  
*Maximum salinity:* Nonsaline (0.0 to 1.9 mmhos/cm)  
*Available water capacity:* Low (about 3.1 inches)

##### Interpretive groups

*Land capability classification (irrigated):* None specified  
*Land capability classification (nonirrigated):* 6e  
*Hydrologic Soil Group:* A

## Custom Soil Resource Report

*Ecological site:* F144AY022MA - Dry Outwash

*Hydric soil rating:* No

### Minor Components

#### Merrimac

*Percent of map unit:* 8 percent

*Landform:* Kames, outwash terraces, eskers, moraines, outwash plains

*Landform position (two-dimensional):* Backslope

*Landform position (three-dimensional):* Side slope, crest, head slope, nose slope, riser

*Down-slope shape:* Convex

*Across-slope shape:* Convex

*Hydric soil rating:* No

#### Windsor

*Percent of map unit:* 5 percent

*Landform:* Outwash deltas, eskers, moraines, kame terraces, kames, outwash plains, outwash terraces

*Landform position (two-dimensional):* Backslope

*Landform position (three-dimensional):* Side slope, crest, head slope, nose slope, riser

*Down-slope shape:* Convex, concave, linear

*Across-slope shape:* Concave, linear, convex

*Hydric soil rating:* No

#### Sudbury

*Percent of map unit:* 2 percent

*Landform:* Outwash plains, moraines, outwash deltas, kame terraces, eskers, outwash terraces

*Landform position (two-dimensional):* Backslope, footslope

*Landform position (three-dimensional):* Base slope, tread

*Down-slope shape:* Concave, linear, convex

*Across-slope shape:* Linear, concave, convex

*Hydric soil rating:* No

## 420B—Canton fine sandy loam, 3 to 8 percent slopes

### Map Unit Setting

*National map unit symbol:* 2w81b

*Elevation:* 0 to 1,180 feet

*Mean annual precipitation:* 36 to 71 inches

*Mean annual air temperature:* 39 to 55 degrees F

*Frost-free period:* 140 to 240 days

*Farmland classification:* All areas are prime farmland

### Map Unit Composition

*Canton and similar soils:* 80 percent

*Minor components:* 20 percent

*Estimates are based on observations, descriptions, and transects of the mapunit.*

## Description of Canton

### Setting

*Landform:* Ridges, moraines, hills  
*Landform position (two-dimensional):* Backslope, summit, shoulder  
*Landform position (three-dimensional):* Side slope, crest, nose slope  
*Down-slope shape:* Convex, linear  
*Across-slope shape:* Convex  
*Parent material:* Coarse-loamy over sandy melt-out till derived from gneiss, granite, and/or schist

### Typical profile

*Ap - 0 to 7 inches:* fine sandy loam  
*Bw1 - 7 to 15 inches:* fine sandy loam  
*Bw2 - 15 to 26 inches:* gravelly fine sandy loam  
*2C - 26 to 65 inches:* gravelly loamy sand

### Properties and qualities

*Slope:* 3 to 8 percent  
*Depth to restrictive feature:* 19 to 39 inches to strongly contrasting textural stratification  
*Drainage class:* Well drained  
*Runoff class:* Low  
*Capacity of the most limiting layer to transmit water (Ksat):* Moderately low to high (0.14 to 14.17 in/hr)  
*Depth to water table:* More than 80 inches  
*Frequency of flooding:* None  
*Frequency of ponding:* None  
*Available water capacity:* Very low (about 2.7 inches)

### Interpretive groups

*Land capability classification (irrigated):* None specified  
*Land capability classification (nonirrigated):* 2s  
*Hydrologic Soil Group:* B  
*Ecological site:* F144AY034CT - Well Drained Till Uplands  
*Hydric soil rating:* No

## Minor Components

### Scituate

*Percent of map unit:* 10 percent  
*Landform:* Drumlins, hills, ground moraines  
*Landform position (two-dimensional):* Footslope, backslope, summit  
*Landform position (three-dimensional):* Crest, side slope  
*Down-slope shape:* Linear, convex  
*Across-slope shape:* Convex  
*Hydric soil rating:* No

### Montauk

*Percent of map unit:* 5 percent  
*Landform:* Drumlins, hills, ground moraines, moraines  
*Landform position (two-dimensional):* Backslope, shoulder, summit  
*Landform position (three-dimensional):* Side slope, crest  
*Down-slope shape:* Linear, convex  
*Across-slope shape:* Convex  
*Hydric soil rating:* No



**Charlton**

*Percent of map unit:* 4 percent  
*Landform:* Hills, ground moraines, ridges  
*Landform position (two-dimensional):* Backslope, shoulder, summit  
*Landform position (three-dimensional):* Crest, side slope  
*Down-slope shape:* Linear, convex  
*Across-slope shape:* Convex  
*Hydric soil rating:* No

**Swansea**

*Percent of map unit:* 1 percent  
*Landform:* Kettles, swamps, bogs, depressions, marshes  
*Down-slope shape:* Concave  
*Across-slope shape:* Concave  
*Hydric soil rating:* Yes

**602—Urban land**

**Map Unit Setting**

*National map unit symbol:* 9950  
*Elevation:* 0 to 3,000 feet  
*Mean annual precipitation:* 32 to 50 inches  
*Mean annual air temperature:* 45 to 50 degrees F  
*Frost-free period:* 110 to 200 days  
*Farmland classification:* Not prime farmland

**Map Unit Composition**

*Urban land:* 85 percent  
*Minor components:* 15 percent  
*Estimates are based on observations, descriptions, and transects of the mapunit.*

**Description of Urban Land**

**Setting**

*Landform position (two-dimensional):* Footslope  
*Landform position (three-dimensional):* Base slope  
*Down-slope shape:* Linear  
*Across-slope shape:* Linear  
*Parent material:* Excavated and filled land

**Minor Components**

**Rock outcrop**

*Percent of map unit:* 5 percent  
*Landform:* Ledges  
*Landform position (two-dimensional):* Summit  
*Landform position (three-dimensional):* Head slope  
*Down-slope shape:* Concave  
*Across-slope shape:* Concave

**Udorthents, wet substratum**

*Percent of map unit: 5 percent*

*Hydric soil rating: No*

**Udorthents, loamy**

*Percent of map unit: 5 percent*

*Hydric soil rating: No*

**626B—Merrimac-Urban land complex, 0 to 8 percent slopes**

**Map Unit Setting**

*National map unit symbol: 2tyr9*

*Elevation: 0 to 820 feet*

*Mean annual precipitation: 36 to 71 inches*

*Mean annual air temperature: 39 to 55 degrees F*

*Frost-free period: 140 to 250 days*

*Farmland classification: Not prime farmland*

**Map Unit Composition**

*Merrimac and similar soils: 45 percent*

*Urban land: 40 percent*

*Minor components: 15 percent*

*Estimates are based on observations, descriptions, and transects of the mapunit.*

**Description of Merrimac**

**Setting**

*Landform: Eskers, moraines, outwash terraces, outwash plains, kames*

*Landform position (two-dimensional): Backslope, footslope, summit, shoulder*

*Landform position (three-dimensional): Side slope, crest, riser, tread*

*Down-slope shape: Convex*

*Across-slope shape: Convex*

*Parent material: Loamy glaciofluvial deposits derived from granite, schist, and gneiss over sandy and gravelly glaciofluvial deposits derived from granite, schist, and gneiss*

**Typical profile**

*Ap - 0 to 10 inches: fine sandy loam*

*Bw1 - 10 to 22 inches: fine sandy loam*

*Bw2 - 22 to 26 inches: stratified gravel to gravelly loamy sand*

*2C - 26 to 65 inches: stratified gravel to very gravelly sand*

**Properties and qualities**

*Slope: 0 to 8 percent*

*Depth to restrictive feature: More than 80 inches*

*Drainage class: Somewhat excessively drained*

*Runoff class: Very low*

*Capacity of the most limiting layer to transmit water (Ksat): Moderately high to very high (1.42 to 99.90 in/hr)*

*Depth to water table: More than 80 inches*

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*Frequency of flooding:* None  
*Frequency of ponding:* None  
*Calcium carbonate, maximum content:* 2 percent  
*Maximum salinity:* Nonsaline (0.0 to 1.4 mmhos/cm)  
*Sodium adsorption ratio, maximum:* 1.0  
*Available water capacity:* Low (about 4.6 inches)

### Interpretive groups

*Land capability classification (irrigated):* None specified  
*Land capability classification (nonirrigated):* 2e  
*Hydrologic Soil Group:* A  
*Ecological site:* F144AY022MA - Dry Outwash  
*Hydric soil rating:* No

## Description of Urban Land

### Typical profile

*M - 0 to 10 inches:* cemented material

### Properties and qualities

*Slope:* 0 to 8 percent  
*Depth to restrictive feature:* 0 inches to manufactured layer  
*Runoff class:* Very high  
*Capacity of the most limiting layer to transmit water (Ksat):* Very low (0.00 to 0.00 in/hr)  
*Available water capacity:* Very low (about 0.0 inches)

### Interpretive groups

*Land capability classification (irrigated):* None specified  
*Land capability classification (nonirrigated):* 8  
*Hydrologic Soil Group:* D  
*Hydric soil rating:* Unranked

## Minor Components

### Windsor

*Percent of map unit:* 5 percent  
*Landform:* Dunes, outwash terraces, deltas, outwash plains  
*Landform position (three-dimensional):* Tread, riser  
*Down-slope shape:* Convex, linear  
*Across-slope shape:* Convex, linear  
*Hydric soil rating:* No

### Sudbury

*Percent of map unit:* 5 percent  
*Landform:* Outwash plains, terraces, deltas  
*Landform position (two-dimensional):* Footslope  
*Landform position (three-dimensional):* Tread, dip  
*Down-slope shape:* Concave  
*Across-slope shape:* Linear  
*Hydric soil rating:* No

### Hinckley

*Percent of map unit:* 5 percent  
*Landform:* Eskers, kames, deltas, outwash plains  
*Landform position (two-dimensional):* Summit, shoulder, backslope  
*Landform position (three-dimensional):* Nose slope, side slope, crest, head slope, rise



## Custom Soil Resource Report

*Down-slope shape:* Convex  
*Across-slope shape:* Convex, linear  
*Hydric soil rating:* No

### **631C—Charlton-Urban land-Hollis complex, 3 to 15 percent slopes, rocky**

#### **Map Unit Setting**

*National map unit symbol:* vr1g  
*Elevation:* 0 to 1,000 feet  
*Mean annual precipitation:* 32 to 54 inches  
*Mean annual air temperature:* 43 to 54 degrees F  
*Frost-free period:* 110 to 240 days  
*Farmland classification:* Not prime farmland

#### **Map Unit Composition**

*Charlton and similar soils:* 45 percent  
*Urban land:* 35 percent  
*Hollis and similar soils:* 10 percent  
*Minor components:* 10 percent  
*Estimates are based on observations, descriptions, and transects of the mapunit.*

#### **Description of Charlton**

##### **Setting**

*Landform:* Drumlins, ground moraines  
*Landform position (two-dimensional):* Backslope  
*Landform position (three-dimensional):* Side slope  
*Down-slope shape:* Linear  
*Across-slope shape:* Convex  
*Parent material:* Friable loamy eolian deposits over friable loamy basal till derived from granite and gneiss

##### **Typical profile**

*H1 - 0 to 5 inches:* fine sandy loam  
*H2 - 5 to 22 inches:* sandy loam  
*H3 - 22 to 65 inches:* gravelly sandy loam

##### **Properties and qualities**

*Slope:* 3 to 15 percent  
*Depth to restrictive feature:* More than 80 inches  
*Drainage class:* Well drained  
*Capacity of the most limiting layer to transmit water (Ksat):* Moderately high to high (0.60 to 6.00 in/hr)  
*Depth to water table:* More than 80 inches  
*Frequency of flooding:* None  
*Frequency of ponding:* None  
*Available water capacity:* Moderate (about 7.3 inches)

## Custom Soil Resource Report

### Interpretive groups

*Land capability classification (irrigated):* None specified  
*Land capability classification (nonirrigated):* 3e  
*Hydrologic Soil Group:* A  
*Ecological site:* F144AY034CT - Well Drained Till Uplands  
*Hydric soil rating:* No

### Description of Urban Land

#### Setting

*Landform position (two-dimensional):* Footslope  
*Landform position (three-dimensional):* Base slope  
*Down-slope shape:* Linear  
*Across-slope shape:* Linear  
*Parent material:* Excavated and filled land

### Description of Hollis

#### Setting

*Landform:* Ridges, hillslopes  
*Landform position (two-dimensional):* Backslope  
*Landform position (three-dimensional):* Side slope  
*Down-slope shape:* Linear  
*Across-slope shape:* Convex  
*Parent material:* Friable, shallow loamy basal till over granite and gneiss

#### Typical profile

*H1 - 0 to 2 inches:* fine sandy loam  
*H2 - 2 to 14 inches:* fine sandy loam  
*H3 - 14 to 18 inches:* unweathered bedrock

#### Properties and qualities

*Slope:* 3 to 15 percent  
*Surface area covered with cobbles, stones or boulders:* 9.0 percent  
*Depth to restrictive feature:* 8 to 20 inches to lithic bedrock  
*Drainage class:* Well drained  
*Capacity of the most limiting layer to transmit water (Ksat):* Very low to moderately low (0.00 to 0.14 in/hr)  
*Depth to water table:* More than 80 inches  
*Frequency of flooding:* None  
*Frequency of ponding:* None  
*Available water capacity:* Very low (about 2.0 inches)

### Interpretive groups

*Land capability classification (irrigated):* None specified  
*Land capability classification (nonirrigated):* 6s  
*Hydrologic Soil Group:* D  
*Ecological site:* F144AY033MA - Shallow Dry Till Uplands  
*Hydric soil rating:* No

### Minor Components

#### Canton

*Percent of map unit:* 4 percent  
*Landform:* Hills  
*Landform position (two-dimensional):* Backslope, toeslope  
*Landform position (three-dimensional):* Side slope, base slope

## Custom Soil Resource Report

*Down-slope shape:* Linear  
*Across-slope shape:* Convex  
*Hydric soil rating:* No

### **Udorthents, loamy**

*Percent of map unit:* 2 percent  
*Hydric soil rating:* No

### **Rock outcrop**

*Percent of map unit:* 2 percent  
*Landform:* Ledges  
*Landform position (two-dimensional):* Summit  
*Landform position (three-dimensional):* Head slope  
*Down-slope shape:* Concave  
*Across-slope shape:* Concave

### **Scituate**

*Percent of map unit:* 1 percent  
*Landform:* Depressions, hillslopes  
*Landform position (two-dimensional):* Toeslope, summit  
*Landform position (three-dimensional):* Base slope, head slope  
*Down-slope shape:* Linear  
*Across-slope shape:* Concave  
*Hydric soil rating:* No

### **Montauk**

*Percent of map unit:* 1 percent  
*Landform:* Hillslopes  
*Landform position (two-dimensional):* Shoulder, summit  
*Landform position (three-dimensional):* Nose slope, head slope  
*Down-slope shape:* Convex  
*Across-slope shape:* Convex  
*Hydric soil rating:* No

## **654—Udorthents, loamy**

### **Map Unit Setting**

*National map unit symbol:* vr1l  
*Elevation:* 0 to 3,000 feet  
*Mean annual precipitation:* 32 to 50 inches  
*Mean annual air temperature:* 45 to 50 degrees F  
*Frost-free period:* 110 to 200 days  
*Farmland classification:* Not prime farmland

### **Map Unit Composition**

*Udorthents, loamy, and similar soils:* 80 percent  
*Minor components:* 20 percent  
*Estimates are based on observations, descriptions, and transects of the mapunit.*



## Description of Udorthents, Loamy

### Setting

*Parent material:* Loamy alluvium and/or sandy glaciofluvial deposits and/or loamy glaciolacustrine deposits and/or loamy marine deposits and/or loamy basal till and/or loamy lodgment till

### Properties and qualities

*Depth to restrictive feature:* More than 80 inches  
*Depth to water table:* More than 80 inches  
*Frequency of flooding:* None  
*Frequency of ponding:* None

## Minor Components

### Udorthents, sandy

*Percent of map unit:* 10 percent  
*Hydric soil rating:* No

### Udorthents, wet substratum

*Percent of map unit:* 5 percent  
*Hydric soil rating:* Yes

### Urban land

*Percent of map unit:* 5 percent  
*Landform position (two-dimensional):* Footslope  
*Landform position (three-dimensional):* Base slope  
*Down-slope shape:* Linear  
*Across-slope shape:* Linear

## 655—Udorthents, wet substratum

### Map Unit Setting

*National map unit symbol:* vr1n  
*Elevation:* 0 to 3,000 feet  
*Mean annual precipitation:* 32 to 54 inches  
*Mean annual air temperature:* 43 to 54 degrees F  
*Frost-free period:* 110 to 240 days  
*Farmland classification:* Not prime farmland

### Map Unit Composition

*Udorthents, wet substratum, and similar soils:* 85 percent  
*Minor components:* 15 percent  
*Estimates are based on observations, descriptions, and transects of the mapunit.*

## Description of Udorthents, Wet Substratum

### Setting

*Parent material:* Loamy alluvium and/or sandy glaciofluvial deposits and/or loamy glaciolacustrine deposits and/or loamy marine deposits and/or loamy basal till and/or loamy lodgment till

## Custom Soil Resource Report

### Properties and qualities

*Slope:* 0 to 8 percent  
*Depth to restrictive feature:* More than 80 inches  
*Depth to water table:* More than 80 inches  
*Frequency of flooding:* None  
*Frequency of ponding:* None

### Minor Components

#### Urban land

*Percent of map unit:* 8 percent  
*Landform position (two-dimensional):* Footslope  
*Landform position (three-dimensional):* Base slope  
*Down-slope shape:* Linear  
*Across-slope shape:* Linear

#### Freetown

*Percent of map unit:* 4 percent  
*Landform:* Depressions, bogs  
*Landform position (two-dimensional):* Toeslope  
*Landform position (three-dimensional):* Dip  
*Down-slope shape:* Concave  
*Across-slope shape:* Concave  
*Hydric soil rating:* Yes

#### Swansea

*Percent of map unit:* 3 percent  
*Landform:* Bogs, depressions  
*Landform position (two-dimensional):* Toeslope  
*Landform position (three-dimensional):* Dip  
*Down-slope shape:* Concave  
*Across-slope shape:* Concave  
*Hydric soil rating:* Yes

# Soil Information for All Uses

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## Soil Properties and Qualities

The Soil Properties and Qualities section includes various soil properties and qualities displayed as thematic maps with a summary table for the soil map units in the selected area of interest. A single value or rating for each map unit is generated by aggregating the interpretive ratings of individual map unit components. This aggregation process is defined for each property or quality.

## Soil Physical Properties

Soil Physical Properties are measured or inferred from direct observations in the field or laboratory. Examples of soil physical properties include percent clay, organic matter, saturated hydraulic conductivity, available water capacity, and bulk density.

## Saturated Hydraulic Conductivity (Ksat)

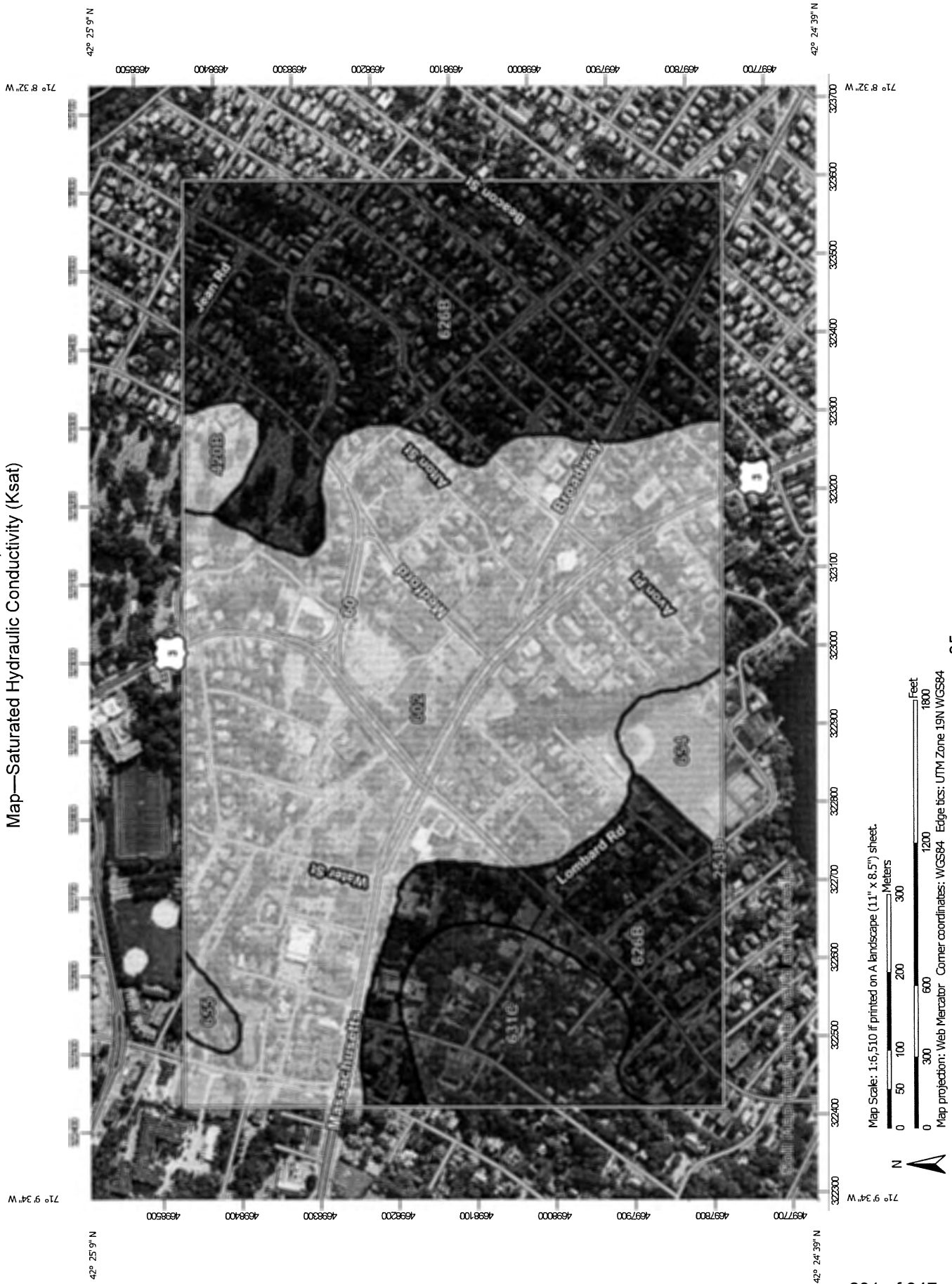
Saturated hydraulic conductivity (Ksat) refers to the ease with which pores in a saturated soil transmit water. The estimates are expressed in terms of micrometers per second. They are based on soil characteristics observed in the field, particularly structure, porosity, and texture. Saturated hydraulic conductivity is considered in the design of soil drainage systems and septic tank absorption fields.

For each soil layer, this attribute is actually recorded as three separate values in the database. A low value and a high value indicate the range of this attribute for the soil component. A "representative" value indicates the expected value of this attribute for the component. For this soil property, only the representative value is used.

The numeric Ksat values have been grouped according to standard Ksat class limits.



# Custom Soil Resource Report Map—Saturated Hydraulic Conductivity (Ksat)



## MAP LEGEND

## MAP INFORMATION

**Area of Interest (AOI)**

Area of Interest (AOI)

**Background**

**Soils**

**Soil Rating Polygons**

- <= 23.2900
- > 23.2900 and <= 54.0000
- > 54.0000 and <= 100.0000
- Not rated or not available

**Soil Rating Lines**

- <= 23.2900
- > 23.2900 and <= 54.0000
- > 54.0000 and <= 100.0000
- Not rated or not available

**Soil Rating Points**

- <= 23.2900
- > 23.2900 and <= 54.0000
- > 54.0000 and <= 100.0000
- Not rated or not available

**Water Features**

- Streams and Canals

**Transportation**

- Rails
- Interstate Highways
- US Routes
- Major Roads
- Local Roads

The soil surveys that comprise your AOI were mapped at 1:25,000.

Warning: Soil Map may not be valid at this scale.

Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service  
Web Soil Survey URL:  
Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distance or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Middlesex County, Massachusetts  
Survey Area Data: Version 20. Jun 9. 2020

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Aug 13, 2020—Sep 15, 2020

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

**Table—Saturated Hydraulic Conductivity (Ksat)**

Map unit symbol	Map unit name	Rating (micrometers per second)	Acres in AOI	Percent of AOI
253D	Hinckley loamy sand, 15 to 25 percent slopes	100.0000	0.1	0.0%
420B	Canton fine sandy loam, 3 to 8 percent slopes	54.0000	2.8	1.4%
602	Urban land		97.4	48.4%
626B	Merrimac-Urban land complex, 0 to 8 percent slopes	100.0000	80.9	40.2%
631C	Charlton-Urban land-Hollis complex, 3 to 15 percent slopes, rocky	23.2900	14.2	7.0%
654	Udorthents, loamy		4.7	2.3%
655	Udorthents, wet substratum		1.4	0.7%
<b>Totals for Area of Interest</b>			<b>201.4</b>	<b>100.0%</b>

**Rating Options—Saturated Hydraulic Conductivity (Ksat)***Units of Measure:* micrometers per second*Aggregation Method:* Dominant Component*Component Percent Cutoff:* None Specified*Tie-break Rule:* Fastest*Interpret Nulls as Zero:* No*Layer Options (Horizon Aggregation Method):* Depth Range (Weighted Average)*Top Depth:* 12*Bottom Depth:* 120*Units of Measure:* Inches**Soil Qualities and Features**

Soil qualities are behavior and performance attributes that are not directly measured, but are inferred from observations of dynamic conditions and from soil properties. Example soil qualities include natural drainage, and frost action. Soil features are attributes that are not directly part of the soil. Example soil features include slope and depth to restrictive layer. These features can greatly impact the use and management of the soil.



## Hydrologic Soil Group

Hydrologic soil groups are based on estimates of runoff potential. Soils are assigned to one of four groups according to the rate of water infiltration when the soils are not protected by vegetation, are thoroughly wet, and receive precipitation from long-duration storms.

The soils in the United States are assigned to four groups (A, B, C, and D) and three dual classes (A/D, B/D, and C/D). The groups are defined as follows:

Group A. Soils having a high infiltration rate (low runoff potential) when thoroughly wet. These consist mainly of deep, well drained to excessively drained sands or gravelly sands. These soils have a high rate of water transmission.

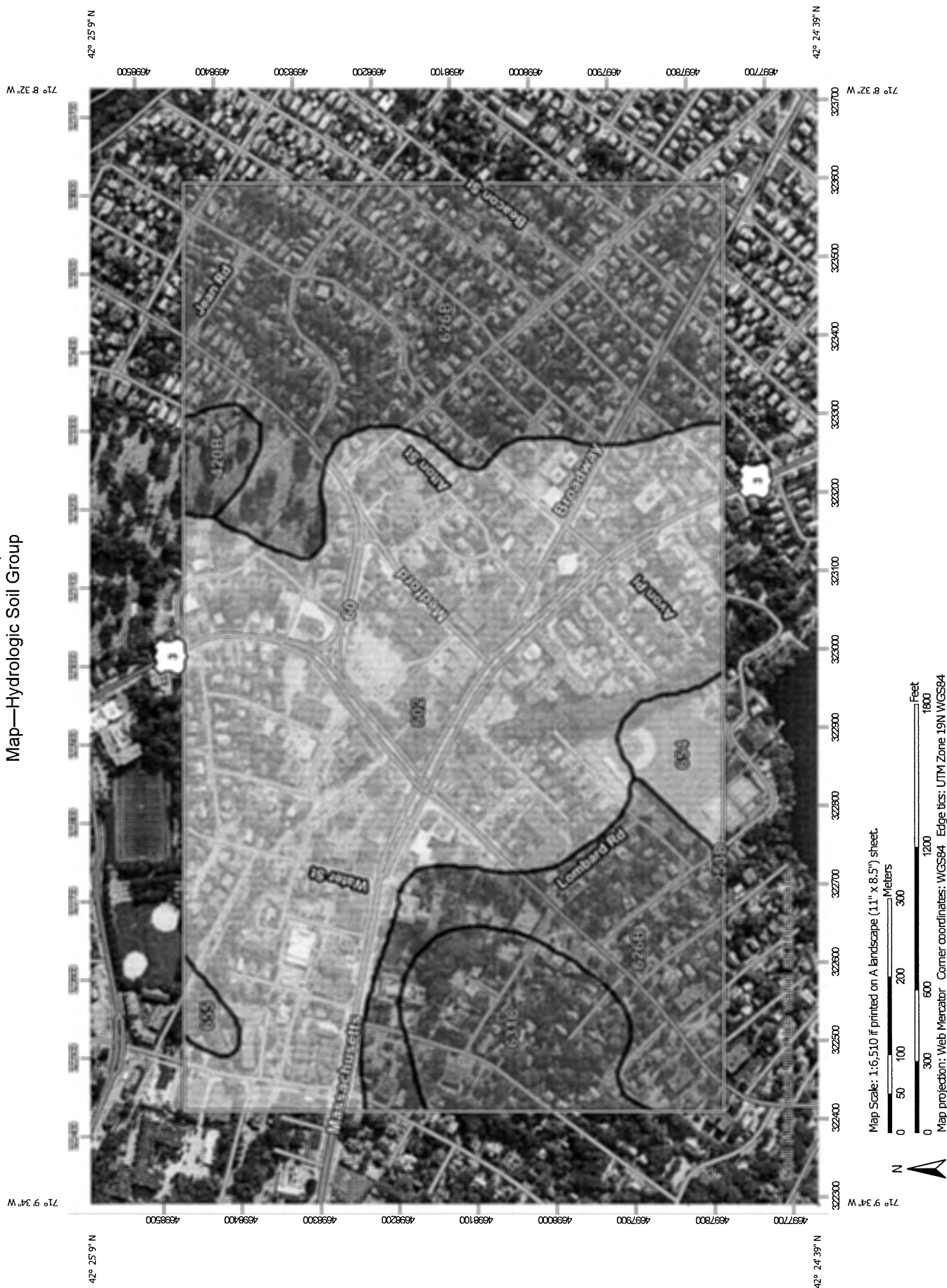
Group B. Soils having a moderate infiltration rate when thoroughly wet. These consist chiefly of moderately deep or deep, moderately well drained or well drained soils that have moderately fine texture to moderately coarse texture. These soils have a moderate rate of water transmission.

Group C. Soils having a slow infiltration rate when thoroughly wet. These consist chiefly of soils having a layer that impedes the downward movement of water or soils of moderately fine texture or fine texture. These soils have a slow rate of water transmission.

Group D. Soils having a very slow infiltration rate (high runoff potential) when thoroughly wet. These consist chiefly of clays that have a high shrink-swell potential, soils that have a high water table, soils that have a claypan or clay layer at or near the surface, and soils that are shallow over nearly impervious material. These soils have a very slow rate of water transmission.

If a soil is assigned to a dual hydrologic group (A/D, B/D, or C/D), the first letter is for drained areas and the second is for undrained areas. Only the soils that in their natural condition are in group D are assigned to dual classes.

Custom Soil Resource Report  
Map—Hydrologic Soil Group



MAP LEGEND

**Area of Interest (AOI)**

Area of Interest (AOI)

**Soils**

**Soil Rating Polygons**

A

A/D

B

B/D

C

C/D

D

Not rated or not available

**Water Features**

Streams and Canals

**Transportation**

Rails

Interstate Highways

US Routes

Major Roads

Local Roads

**Background**

Aerial Photography

**Soil Rating Lines**

A

A/D

B

B/D

C

C/D

D

Not rated or not available

**Soil Rating Points**

A

A/D

B

B/D

**C**

C/D

D

Not rated or not available

MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:25,000.

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Web Soil Survey URL:  
Coordinate System: Web Mercator (EPSG:3857)

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Survey Area Data: Version 20, Jun 9, 2020

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**Table—Hydrologic Soil Group**

Map unit symbol	Map unit name	Rating	Acres in AOI	Percent of AOI
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420B	Canton fine sandy loam, 3 to 8 percent slopes	B	2.8	1.4%
602	Urban land		97.4	48.4%
626B	Merrimac-Urban land complex, 0 to 8 percent slopes	A	80.9	40.2%
631C	Charlton-Urban land-Hollis complex, 3 to 15 percent slopes, rocky	A	14.2	7.0%
654	Udorthents, loamy		4.7	2.3%
655	Udorthents, wet substratum		1.4	0.7%
<b>Totals for Area of Interest</b>			<b>201.4</b>	<b>100.0%</b>

**Rating Options—Hydrologic Soil Group***Aggregation Method: Dominant Condition**Component Percent Cutoff: None Specified**Tie-break Rule: Higher*

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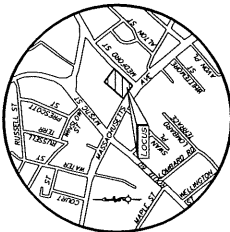
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# SITE DEVELOPMENT PLAN SET

## 455-457 MASSACHUSETTS AVENUE

### ARLINGTON, MA 02476



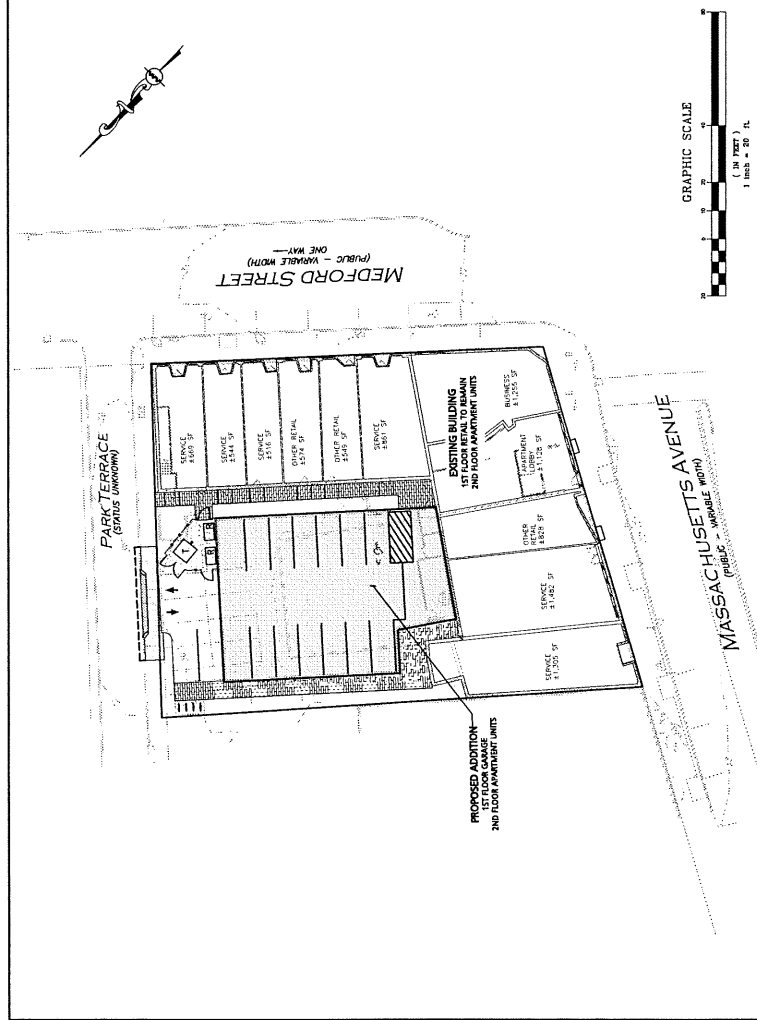
LOCUS MAP  
NOT TO SCALE

**APPLICANT:**  
2-14 MEDFORD STREET, LLC  
455 MASSACHUSETTS AVE, STE 1  
ARLINGTON, MA 02474

**ARCHITECT:**  
MARKET SQUARE ARCHITECTS  
104 CONGRESS STREET, STE 203  
PORTSMOUTH, NH 03801  
(603) 501-0202

**CIVIL ENGINEER, LANDSCAPE ARCHITECT &  
LAND SURVEYOR:**  
ALLEN & MAJOR ASSOCIATES, INC.  
100 COMMERCE WAY, SUITE 5  
WOBBURN, MA 01801  
(781) 985-6889

DRAWING TITLE	SHEET	ISSUED	REVISED
EXISTING CONDITIONS	1	8/1/2021	-
SITE PREPARATION PLAN	C-101	8/20/2021	-
LAYOUT & MATERIALS PLAN	C-102	8/20/2021	-
GRADING & DRAINAGE PLAN	C-103	8/20/2021	-
DETAILS	C-S01	8/20/2021	-
DETAILS	C-S02	8/20/2021	-
LANDSCAPE PLAN	L-101	8/20/2021	-
LANDSCAPE DETAILS	L-S01	8/20/2021	-



8/20/21

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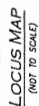
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ISSUED FOR ARB REVIEW: 8/20/2021

P:\PROJECTS\1729-CIVIL\DRAWINGS\CURRENT\C-279-01-COVER.DWG



08/19/21

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2-14 MEDFORD STREET, LLC  
455 MASSACHUSETTS AVENUE, SUITE 1  
ARLINGTON, MA 02474

PROJECT:  
455-457  
MASSACHUSETTS  
AVENUE  
ARLINGTON, MA

PROJECT NO.	2729-03	DATE:	08/19/21
SCALE:	1" = 20'	DWG. NAME:	S-2729-03-EC
DRAFTED BY:	AJR	CHECKED BY:	NE



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DRAWING TITLE:		SHEET No.
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## EXISTING CONDITIONS

Call today 800 368 6868

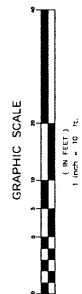
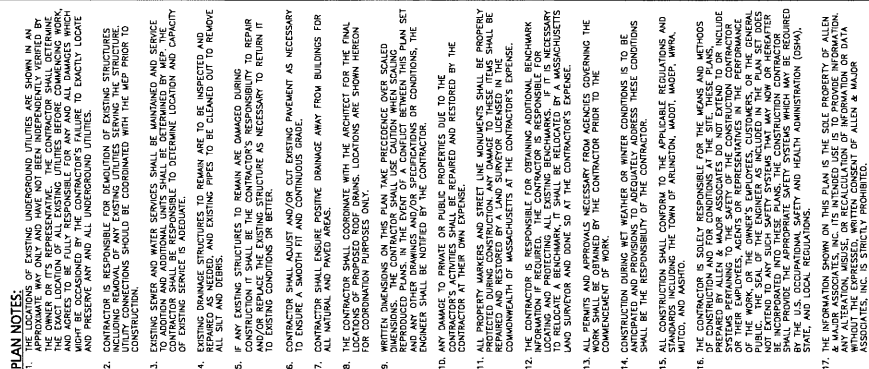
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-888-344-7233

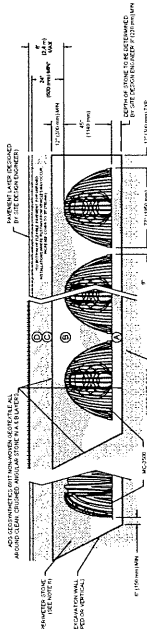
100

1. CHAMBERS SHALL BE STOWED AT MAX-3500 OR APPROXIMATE EQUAL.
2. CHAMBERS SHALL BE MANUFACTURED FROM HIGH POLYMER POLYMER OR POLYETHYLENE RESINS TESTED AGAIN ASTM STANDARDS.
3. CHAMBERS SHALL BE DESIGNED IN ACCORDANCE WITH ASTM 12797, STANDARD PRACTICE FOR STRUCTURAL DESIGN OF THERMOPLASTIC CHAMBERS.
4. CHAMBERS SHALL BE PROVIDED WITH UNOBSTRUCTED INTERNAL SPACE WITH NO INTERNAL SUPPORT PILES.
5. ONLY CHAMBERS THAT ARE APPROVED BY THE ENGINEER WILL BE ALLOWED.
6. CHAMBERS SHALL BE PRODUCED AT AN ISO 9001 CERTIFIED MANUFACTURING FACILITY.
7. ALL DESIGN SPECIFICATIONS FOR CHAMBERS SHALL BE IN ACCORDANCE WITH THE MANUFACTURER'S LATEST PUBLISHED INFORMATION.
8. THE INSTALLATION OF CHAMBERS SHALL BE IN ACCORDANCE WITH THE MANUFACTURER'S LATEST INSTALLATION INSTRUCTIONS.

DESC.	SIGN	SIZE	MOUNTING HEIGHT	DESCRIPTION	REFLECTORIZED
R7-8M (MODIFIED)		12" x 26"	7' - 0"	WHITE TEXT ON BLUE FIELD WITH WHITE BORDER	YES

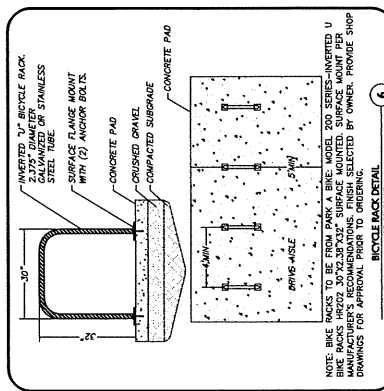
1. TRAFFIC AND SAFETY SIGNAGE SHALL COMPLY WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) STANDARDS.
2. MOUNTING HEIGHT IS DEFINED AS THE DISTANCE FROM THE BOTTOM OF THE SIGN TO THE NEAR EDGE OF THE PAVEMENT.

**SIGN TABLE**  
**NOT TO SCALE**

[illegible]

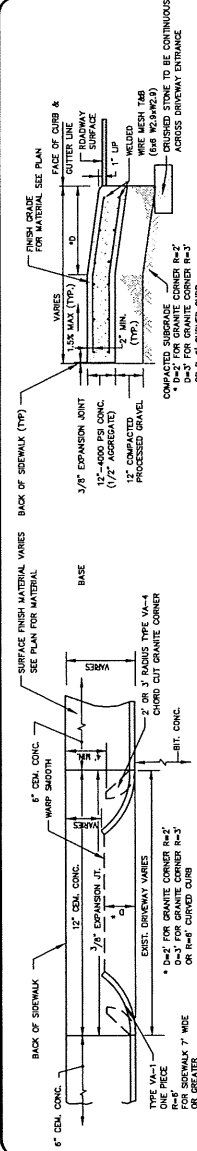
- [illegible]

**CHAMBER SYSTEM TYPICAL CROSS SECTION DETAIL**

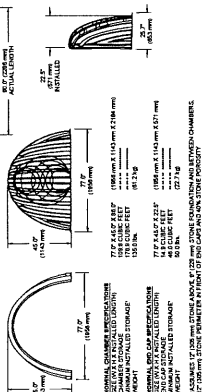


- [illegible]

7



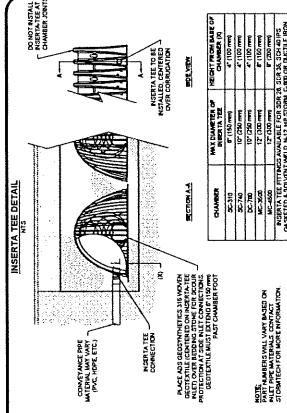
TYPICAL COMMERCIAL DRIVEWAY APRON 9



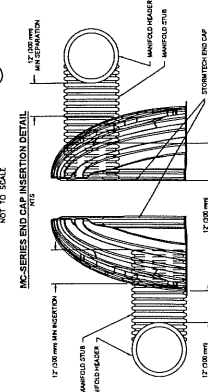
RESULTS AT TOP OF END CAP FOR PART NUMBER ENDS WITH "H"		C	
PART #	STUB	1	2
MC-2002S1P001	6" (152 mm)	1.68" (43 mm)	3.31" (84 mm)
MC-2002S2P001	6" (152 mm)	1.71" (43 mm)	3.31" (84 mm)
MC-2002S3P001	6" (152 mm)	1.81" (46 mm)	3.31" (84 mm)
MC-2002S4P001	6" (152 mm)	1.87" (47 mm)	3.31" (84 mm)
MC-2002S5P001	6" (152 mm)	1.93" (49 mm)	3.31" (84 mm)
MC-2002S6P001	6" (152 mm)	2.00" (51 mm)	3.31" (84 mm)
MC-2002S7P001	6" (152 mm)	2.06" (52 mm)	3.31" (84 mm)
MC-2002S8P001	6" (152 mm)	2.13" (54 mm)	3.31" (84 mm)
MC-2002S9P001	6" (152 mm)	2.20" (56 mm)	3.31" (84 mm)
MC-2002S10P001	6" (152 mm)	2.27" (58 mm)	3.31" (84 mm)
MC-2002S11P001	6" (152 mm)	2.34" (59 mm)	3.31" (84 mm)
MC-2002S12P001	6" (152 mm)	2.41" (61 mm)	3.31" (84 mm)
MC-2002S13P001	6" (152 mm)	2.48" (63 mm)	3.31" (84 mm)
MC-2002S14P001	6" (152 mm)	2.55" (65 mm)	3.31" (84 mm)
MC-2002S15P001	6" (152 mm)	2.62" (67 mm)	3.31" (84 mm)
MC-2002S16P001	6" (152 mm)	2.69" (69 mm)	3.31" (84 mm)
MC-2002S17P001	6" (152 mm)	2.76" (70 mm)	3.31" (84 mm)
MC-2002S18P001	6" (152 mm)	2.83" (72 mm)	3.31" (84 mm)
MC-2002S19P001	6" (152 mm)	2.90" (74 mm)	3.31" (84 mm)
MC-2002S20P001	6" (152 mm)	2.97" (76 mm)	3.31" (84 mm)
MC-2002S21P001	6" (152 mm)	3.04" (77 mm)	3.31" (84 mm)
MC-2002S22P001	6" (152 mm)	3.11" (79 mm)	3.31" (84 mm)
MC-2002S23P001	6" (152 mm)	3.18" (81 mm)	3.31" (84 mm)
MC-2002S24P001	6" (152 mm)	3.25" (83 mm)	3.31" (84 mm)
MC-2002S25P001	6" (152 mm)	3.32" (84 mm)	3.31" (84 mm)
MC-2002S26P001	6" (152 mm)	3.39" (86 mm)	3.31" (84 mm)
MC-2002S27P001	6" (152 mm)	3.46" (88 mm)	3.31" (84 mm)
MC-2002S28P001	6" (152 mm)	3.53" (90 mm)	3.31" (84 mm)
MC-2002S29P001	6" (152 mm)	3.60" (92 mm)	3.31" (84 mm)
MC-2002S30P001	6" (152 mm)	3.67" (94 mm)	3.31" (84 mm)
MC-2002S31P001	6" (152 mm)	3.74" (96 mm)	3.31" (84 mm)
MC-2002S32P001	6" (152 mm)	3.81" (98 mm)	3.31" (84 mm)
MC-2002S33P001	6" (152 mm)	3.88" (100 mm)	3.31" (84 mm)
MC-2002S34P001	6" (152 mm)	3.95" (102 mm)	3.31" (84 mm)
MC-2002S35P001	6" (152 mm)	4.02" (104 mm)	3.31" (84 mm)
MC-2002S36P001	6" (152 mm)	4.09" (106 mm)	3.31" (84 mm)
MC-2002S37P001	6" (152 mm)	4.16" (108 mm)	3.31" (84 mm)
MC-2002S38P001	6" (152 mm)	4.23" (110 mm)	3.31" (84 mm)
MC-2002S39P001	6" (152 mm)	4.30" (112 mm)	3.31" (84 mm)
MC-2002S40P001	6" (152 mm)	4.37" (114 mm)	3.31" (84 mm)
MC-2002S41P001	6" (152 mm)	4.44" (116 mm)	3.31" (84 mm)
MC-2002S42P001	6" (152 mm)	4.51" (118 mm)	3.31" (84 mm)
MC-2002S43P001	6" (152 mm)	4.58" (120 mm)	3.31" (84 mm)
MC-2002S44P001	6" (152 mm)	4.65" (122 mm)	3.31" (84 mm)
MC-2002S45P001	6" (152 mm)	4.72" (124 mm)	3.31" (84 mm)
MC-2002S46P001	6" (152 mm)	4.79" (126 mm)	3.31" (84 mm)
MC-2002S47P001	6" (152 mm)	4.86" (128 mm)	3.31" (84 mm)
MC-2002S48P001	6" (152 mm)	4.93" (130 mm)	3.31" (84 mm)
MC-2002S49P001	6" (152 mm)	5.00" (132 mm)	3.31" (84 mm)
MC-2002S50P001	6" (152 mm)	5.07" (134 mm)	3.31" (84 mm)
MC-2002S51P001	6" (152 mm)	5.14" (136 mm)	3.31" (84 mm)
MC-2002S52P001	6" (152 mm)	5.21" (138 mm)	3.31" (84 mm)
MC-2002S53P001	6" (152 mm)	5.28" (140 mm)	3.31" (84 mm)
MC-2002S54P001	6" (152 mm)	5.35" (142 mm)	3.31" (84 mm)
MC-2002S55P001	6" (152 mm)	5.42" (144 mm)	3.31" (84 mm)
MC-2002S56P001	6" (152 mm)	5.49" (146 mm)	3.31" (84 mm)
MC-2002S57P001	6" (152 mm)	5.56" (148 mm)	3.31" (84 mm)
MC-2002S58P001	6" (152 mm)	5.63" (150 mm)	3

- NOTE: ALL DIMENSIONS ARE NOMINAL.

## SALE



**4** INSERTA TEE CONNECTION DETAIL



FOR A PROPER FIT IN END CAP OPENING.

**CHAMBER SYSTEM TYPICAL END CAP DETAIL** (5)





LEGEND

DECIDUOUS TREE

EVERGREEN TREE

FLOWERING TREE

SHRUBS

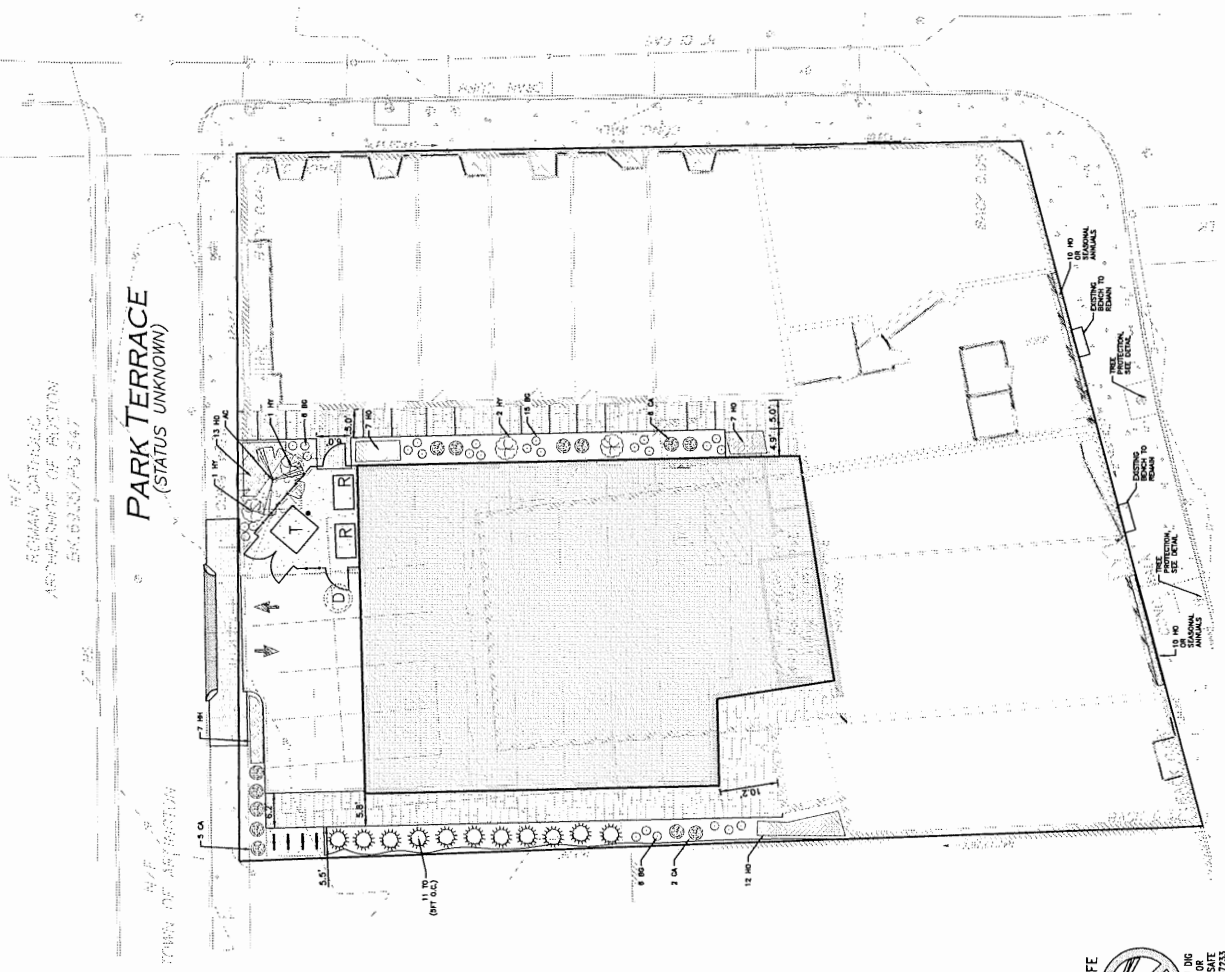
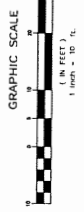
MULCH BED

PERENNIALS/GROUNDCOVER

PLANTING SCHEDULE-TREES, SHRUBS, GROUNDCOVERS & PERENNIALS

KEY	QUANTITY	BOTANICAL NAME	COMMON NAME	MIN. SIZE	SPACING	COMMENTS
EVERGREEN TREES						
TO	11	THUJA G. 'WINTERGREEN'	WINTERGREEN ARBORVITAE	6-7' HT	AS SHOWN	BAB
FLOWERING TREES						
AC	1	AMELANCHIER CANADENSIS	SHADBLOW SERVICEBERRY	2.5-3' CAL	AS SHOWN	BAB
SHRUBS/PERENNIALS						
CA	13	CORUS ALBA 'VORY HALD'	PORY HALD DOGWOOD	13	AS SHOWN	POT
BD	27	BUXUS 'GREEN MOUNTAIN'	GREEN MOUNTAIN BOXWOOD	12	AS SHOWN	POT
HY	4	HYDRANGEA 'ARBORESCENS'	ARBORESCENS HYDRANGEA	15	AS SHOWN	POT
HO	48	HEUCHERA 'LUNAR'	LUNAR HEUCHERA	11	24" O.C.	STAGGERED
HF	7	HIERACIUM 'BIG TIME'	BIG TIME HAPPY DAVILLY	11	24" O.C.	STAGGERED

- NOTES:
- THE INFORMATION SHOWN ON THIS PLAN IS THE SOLE PROPERTY OF ALLEN & MAJOR ASSOCIATES, INC. IT IS INTENDED FOR THE USE OF THE CONTRACTOR AND IS NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF ALLEN & MAJOR ASSOCIATES, INC. ANY VIOLATION OF THIS NOTICE IS STRICTLY PROHIBITED.
  - ALL UNDESIRABLE AREAS WITH SHRUBS, TREES, AND PERENNIALS TO BE REMOVED SHALL BE IDENTIFIED BY A "T" IN THE TOPSOIL AREA. ALL UNDESIRABLE AREAS WITH SHRUBS, TREES, AND PERENNIALS TO BE MAINTAINED SHALL BE IDENTIFIED BY A "P" IN THE TOPSOIL AREA. ALL UNDESIRABLE AREAS WITH SHRUBS, TREES, AND PERENNIALS TO BE MAINTAINED SHALL BE IDENTIFIED BY A "P" IN THE TOPSOIL AREA. ALL UNDESIRABLE AREAS WITH SHRUBS, TREES, AND PERENNIALS TO BE MAINTAINED SHALL BE IDENTIFIED BY A "P" IN THE TOPSOIL AREA.



REGISTERED LANDSCAPE ARCHITECT FOR  
ALLEN & MAJOR ASSOCIATES, INC.

214 MEDFORD STREET, LLC  
455 MASSACHUSETTS AVE, STE 1  
ARLINGTON, MA 02474

PROJECT  
455-457  
MASSACHUSETTS AVE  
ARLINGTON, MA 02475

PROJECT NO. 275849 DATE 8/20/2021  
SCALE 1" = 10' DWG NAME CITING  
DESIGNED BY: JLS, JED CHECKED BY: JAM/RO



ALLEN & MAJOR  
ASSOCIATES, INC.  
Landscape Architecture  
100 COMMERCIAL WAY, SUITE 5  
TULLAH, MS 38881  
TEL: (662) 841-1000  
FAX: (662) 841-1000

THIS DRAWING WAS PREPARED IN DIGITAL FORM. THE ORIGINAL DRAWING IS THE ONLY TRUE COPY. ANY REPRODUCTION OF THIS DRAWING IS UNAUTHORIZED. THE USER OF THIS DRAWING SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND FOR THE PROPER INSTALLATION AND MAINTENANCE OF THE LANDSCAPE. THE USER SHALL BE RESPONSIBLE FOR THE PROPER MAINTENANCE OF THE LANDSCAPE. THE USER SHALL BE RESPONSIBLE FOR THE PROPER MAINTENANCE OF THE LANDSCAPE. THE USER SHALL BE RESPONSIBLE FOR THE PROPER MAINTENANCE OF THE LANDSCAPE.

DRAWING TITLE LANDSCAPE PLAN SHEET NO. L-101

DIG SAFE

BEFORE YOU DIG  
CALL 811 OR  
1-888-800-SAFE  
1-888-344-7233



## Robert Annese

---

**From:** John H. Murphy III <john@summit-res.net>  
**Sent:** Monday, August 30, 2021 11:03 AM  
**To:** Robert Annese  
**Subject:** Fwd: 455 FAR

John Murphy

Vice President

**SUMMIT** Real Estate Strategies LLC  
60 Summer Street. Manchester. MA 01944  
PH: 978.704.9022  
FX: 978.704.9728  
Cell: 207.776.1751

Begin forwarded message:

**From:** Peter Slowik <pslowik@marketsquarearchitects.com>  
**Date:** August 30, 2021 at 11:00:02 AM EDT  
**To:** John Murphy <john@summit-res.net>  
**Subject:** Re: 455 FAR

In regards to the previous email-

basement = 10873 sf  
first floor = 15012 sf  
second floor = 13567 sf  
total = 39452 sf

minus basement and 206 sf of riser, electrical, elevator mech. = 28,373 gsf

divide this by site area (18,929) = 1.5 FAR

**Peter Slowik**  
**Market Square Architects, PLLC**  
Project Coordinator  
P: (603) 501-0202  
www.MarketSquareArchitects.com

On Aug 30, 2021, at 8:49 AM, Peter Slowik <pslowik@marketsquarearchitects.com> wrote:

John,

We digitally measured Gross Floor Area (GFA) in accordance with the Arlington Bylaws. Total area was measured along the exterior surface of the exterior walls of each floor.

From this we subtracted the areas which are allowed to be excluded, in this case mechanical spaces and service basements. This GFA was then divided by total site area provided by Civil.

Best,

**Peter Slowik**

**Market Square Architects, PLLC**

Project Coordinator

P: (603) 501-0202

[www.MarketSquareArchitects.com](http://www.MarketSquareArchitects.com)



## **Robert Annese**

---

**From:** John H. Murphy III <john@summit-res.net>  
**Sent:** Monday, August 30, 2021 9:54 AM  
**To:** Robert Annese  
**Subject:** Fwd: 455 FAR

John Murphy

Vice President

**SUMMIT** Real Estate Strategies LLC

60 Summer Street. Manchester. MA 01944

PH: 978.704.9022

FX: 978.704.9728

Cell: 207.776.1751

Begin forwarded message:

**From:** Peter Slowik <pslowik@marketsquarearchitects.com>  
**Date:** August 30, 2021 at 9:50:00 AM EDT  
**To:** John Murphy <john@summit-res.net>  
**Subject:** 455 FAR

John,

We digitally measured Gross Floor Area (GFA) in accordance with the Arlington Bylaws. Total area was measured along the exterior surface of the exterior walls of each floor. From this we subtracted the areas which are allowed to be excluded, in this case mechanical spaces and service basements. This GFA was then divided by total site area provided by Civil.

Best,

**Peter Slowik**  
**Market Square Architects, PLLC**  
Project Coordinator  
P: (603) 501-0202  
www.MarketSquareArchitects.com

<b>1ST FLOOR</b>	<b>Existing</b>	<b>Proposed</b>	<b>Loss</b>
Lucky dragon	764	538	226
La Victoria	524	546	-22
Gail Ann	536	517	19
Artful Heart	533	574	-41
7 star Grocery	527	550	-23
European Hair	739	862	-123
Leader Bank	1404	1255	149
Frozen Bakery	1302	1064	238
Verizon	1425	828	597
Papa Ginos	3269	1460	1809
Grace Nails	1255	1162	93
<b>Subtotal</b>	<b>12278</b>	<b>9356</b>	<b>2922</b>
<b>2ND FLOOR</b>			
Maid Pro	797	0	797
Trophy	984	0	984
Property manager	462	0	462
Realtor	462	0	462
Economist	228	0	228
<b>Subtotal</b>	<b>2933</b>	<b>0</b>	<b>2933</b>
<b>Total Commercial Space Lost</b>			<b>5855</b>

**From:** Peter Slowik <[pslowik@marketsquarearchitects.com](mailto:pslowik@marketsquarearchitects.com)>  
**Subject:** 455 Mass Ave supplemental info - bike rack specs and lighting intent  
**Date:** September 27, 2021 at 11:51:56 AM EDT  
**To:** John Murphy <[john@summit-res.net](mailto:john@summit-res.net)>

John,

Should the questions arise, please see attached for bike rack specifications for indoor bike storage. These were used in another conforming project in Arlington, and have been spaced to exceed the Bylaw guide minimums.

For exterior lighting, we intend to provide the minimum feasible to maintain a safe living environment, particularly at the two rear pedestrian corridors surrounding the garage. Existing street lights will provide ample illumination in proximity to Park Terrace, and will be supplemented with a single adjustable downlight fixture provided at each entrance. The interior of the garage will be illuminated with ceiling lights, and the wall opening shall be constructed such that the head height sufficiently shields direct light from neighboring structures. Mass Ave and both side streets are illuminated by street lights.

**Peter Slowik**  
**Market Square Architects, PLLC**  
Project Coordinator  
P: (603) 501-0202  
[www.MarketSquareArchitects.com](http://www.MarketSquareArchitects.com)

SECTION 129310

BICYCLE RACKS

PART 1 - GENERAL

1.1 GENERAL PROVISIONS

- A. Attention is directed to the CONTRACT AND GENERAL CONDITIONS and all Sections within DIVISION 01 - GENERAL REQUIREMENTS which are hereby made a part of this Section of the Specifications.

1.2 DESCRIPTION OF WORK

- A. Work Included: Provide labor, materials and equipment necessary to complete the work of this Section, including but not limited to the following:
  - 1. Bicycle racks.
- B. Related Work: The following items are not included in this Section and will be performed under the designated Sections:
  - 1. Section 033000 - CAST-IN-PLACE CONCRETE for lintels, sleeves, anchors, inserts, plates and similar items.

1.3 SUBMITTALS

- A. Product Data: Include for each product specified.

1.4 QUALITY ASSURANCE

- A. Source Limitations: Obtain products through one source from a single manufacturer.

1.5 COORDINATION

- A. Coordinate size and location of blocking and mounting details.

PART 2 - PRODUCTS

2.1 BICYCLE RACKS

- A. Basis of Design: Dero Bike Rack Co.; Hoop Rack.
- B. Materials:
  - 1. Pipe: 1.5 inch schedule 40 steel pip with powder coat finish.
  - 2. Mounting Rails: 3 x 1.4 x 3/16 inch thick galvanized steel rails.
- C. Configuration:
  - 1. U-Hoop Style bike rack for mounting 2 bikes per hoop.



2. Hoops are bolted to two parallel rails, anchored to the ground.
3. Angle: 90 degrees.
4. Distance Between Racks: 3 feet.

### PART 3 - EXECUTION

#### 3.1 EXAMINATION

- A. Examine substrates, areas, and conditions, with Installer present, for compliance with requirements for installation tolerances, operational clearances, and other conditions affecting performance.
- B. Proceed with installation only after unsatisfactory conditions have been corrected.

#### 3.2 INSTALLATION

- A. Install in accordance with Manufacturer's recommendations.

#### 3.3 CLEANING AND PROTECTION

- A. Protect installed products until completion of project and touch-up, repair or replace damaged products before Substantial Completion.

END OF SECTION



## ARLINGTON REDEVELOPMENT BOARD

TOWN HALL ARLINGTON, MASSACHUSETTS 02476  
TELEPHONE 781-316-3090

### By Certified First Mail

Notification of Bylaw Violations at 821 Massachusetts Avenue

July 21, 2021

Geoffrey Noyes  
208 Beacon Street  
Marblehead, MA 01945

Dear Mr. Noyes:

Please receive this letter as notice of numerous violations of Town of Arlington Bylaws and Zoning Bylaws as a result of specific actions taken by your contractors and agents on or about June 28, 2021 through July 2, 2021 at your property known as the "Atwood House;" as well as ongoing, long-term conditions at such property, which are governed not only by Town ordinances, but a special permit issued by the Arlington Redevelopment Board.

In brief, despite a scheduled hearing on August 3m 2021 regarding a demolition permit on an historic building before the Arlington Historical Commission (AHC), your contractors and agents conducted significant demolition of the Atwood House over an approximately weeklong period in advance of the July 4<sup>th</sup> holiday weekend without any permits or notices to the Town. As a result, Director of Inspectional Services Michael Ciampa issued a stop-work order on or about July 3, 2021 to your contractor, Groom Construction. Performing such work, particularly given your pending application for a demolition permit and scheduled hearing before the AHC, as well as this Department's efforts to work collaboratively with you on options for redevelopment is particularly egregious. As the property owner of record, you are hereby noticed of the following fines *at a minimum* for violations of the following sections of the Town Bylaws due to unpermitted, premature, and improper demolition and site maintenance activities on or about the week of June 28 through July 2, 2021, including the following:

Title V, Article 7 Section C	\$1,350
Title V, Article 9.1	\$350
Title V, Article 17	\$700
Title IX, Article 1	\$80
	<u>\$2,480</u>

Further, as the property owner of record, despite last registering your property on January 29, 2020, you have remained non-compliant with the Town's Vacant Storefront Bylaw (Title V Article 17) and are hereby noticed that absent immediate compliance, the Town will pursue up to two years of this Bylaw provision, totaling \$73,000. Please also be advised that you will be fined an additional \$100 per day until the building conditions are corrected to the Town's satisfaction. If you wish to avoid these fines, conditions at the Atwood House property including securing the building, properly registering it on the Vacant Storefront Registry, must improve swiftly and dramatically. Secure building conditions shall encompass the following: removal of all boarded windows and installing new windows and siding which shall be reviewed and approved by the Arlington Historical Commission and installation of a 24-hour security and monitoring system.

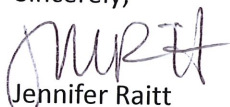
Moreover, the Town intends to reopen your Environmental Design Review Special Permit Docket #3348 for 833 Mass Ave due to continued non-compliance with Special Condition #5. This Special Permit is recorded at the Middlesex South Registry of Deeds Book 131350 and Page 69 on August 18, 2009. Pursuant to this Decision, as the property owner you are required to abide by all permit conditions. The Arlington Redevelopment Board requested follow-up on actions with regard to future redevelopment of the "Atwood House" as referenced in Special Condition #5 in the original Special Permit. You remain in non-compliance of the letter sent to you via certified mail on May 4, 2020. You were required to do the following and failed to follow through: you were to apply for a demolition permit within 30 days following the Board's vote or by May 27, 2020; you were to apply for an Environmental Design Review Special Permit following expiration of the demolition delay period or earlier contingent upon Arlington Historical Commission rulings per Town Bylaw Article 6 Historically or Architecturally Significant Buildings. If you had not filed for a demolition permit, you were to apply for an Environmental Design Review Special Permit with a renovation plan for the existing building within 60 days or by June 27, 2020. Please be advised that the Arlington Redevelopment Board reserves the right to revoke this Special Permit and staff may advise the Board of this option accordingly. As the owner, you have demonstrated willful and reckless negligence since this Special Permit was granted in 2009.

Finally, Inspectional Services advises this Department that commencing work prior to obtaining a permit constitutes a violation of the State Building Code, 780 CMR Sections 105.1 and 109.4, each of which are punishable by \$1,000 (or one year imprisonment) per violation, with each day constituting a separate violation. Inspectional Services will separately contact you and/or your contractor regarding penalties for commencing work without a permit.

Unless and until all of the above fines are paid and the violations are corrected to the satisfaction of the Arlington Historical Commission, the Town of Arlington intends to the full extent permitted by law to place a municipal lien on your property.

Please contact this office with questions regarding this notice at [781-316-3090](tel:781-316-3090).

Sincerely,



Jennifer Raitt

Director of Planning and Community Development



Michael Ciampa

Interim Director of Inspectional Services

cc: Robert J. Annese, Esquire

Rachel Zsembery, AIA, Chair, Arlington Redevelopment Board

JoAnn Robinson, Chair, Arlington Historical Commission

Douglas Heim, Town Counsel

Christine Bongiorno, Director of Health and Human Services

Chief Juliann Flaherty, Police Department

Chief Kevin Kelley, Fire Department

**ROBERT J. ANNESE**  
ATTORNEY AT LAW

September 23, 2020

*Via e-mail: robinsarcn.com*

JoAnn Robinson, Chair  
Historical Commission  
Whittemore Robbins House  
670R Massachusetts Ave  
Arlington, Ma 02476

RE: Noyes Realty LLLP  
(Atwood House) 821 Massachusetts Avenue, Arlington, Massachusetts

Dear Ms. Robinson:

My client is going to develop the real estate located at 821 Massachusetts Avenue, Arlington, i.e. the "Atwood House" and is filing a demolition application in connection with its building plans.

The building plans have not yet been finalized and will be the subject of further hearings before the Arlington Redevelopment Board.

I am sending a preliminary draft of the development plans in connection with the development of the property which will likely change as any hearings progress before the Arlington Redevelopment Board.

I am sending this material along to the Historical Commission at this time and would request that the Commission schedule if necessary a hearing regarding this matter with respect to my client's demolition application and plans.

Very truly yours,

  
Robert J. Annese

RJA:lm

Enclosures

cc: Mr. Geoffrey P. Noyes  
Noyes Realty, LLLP





**ARLINGTON REDEVELOPMENT BOARD**

TOWN HALL ARLINGTON, MASSACHUSETTS 02476

TELEPHONE 781-316-3090

May 4, 2020

Geoffrey Noyes  
208 Beacon Street  
Marblehead, MA 01945

Re: Environmental Design Review Special Permit 3348

Dear Mr. Noyes,

I am writing to follow-up from the April 27, 2020 Redevelopment Board meeting regarding the property you own located at 833 Massachusetts Avenue in Arlington, MA. This property has a Special Permit 3348 recorded at the Middlesex South Registry of Deeds Book 131350 and Page 69 on August 18, 2009. Pursuant to this Decision, as the property owner you are required to abide by all permit conditions. The Arlington Redevelopment Board requested follow-up on actions with regard to future redevelopment of the "Atwood House" as referenced in Special Condition #5 in the original Special Permit.

I am writing to inform you that on Monday, April 27, 2020, the Arlington Redevelopment Board voted unanimously (5-0) to close the hearing. In their vote, closing this hearing was conditioned on the following actions:

- The property owner shall apply for a demolition permit within 30 days following this vote or by May 27, 2020.
- The property owner shall apply for an Environmental Design Review Special Permit following expiration of the demolition delay period or earlier contingent upon Arlington Historical Commission rulings per Town Bylaw Article 6 Historically or Architecturally Significant Buildings.
- If the property owner does not file a demolition permit, the owner shall apply for an Environmental Design Review Special Permit with a renovation plan for the existing building within 60 days or by June 27, 2020.

Should you have any questions about this vote, please contact me at 781-316-3092.

Sincerely,

A handwritten signature in blue ink, appearing to read "Jennifer Raitt".

Jennifer Raitt  
Secretary Ex-Officio

cc: Andrew Bunnell, Chair  
Robert J. Annese, Esquire



**ARLINGTON REDEVELOPMENT BOARD**

TOWN HALL ARLINGTON, MASSACHUSETTS 02476

TELEPHONE 781-316-3090

November 18, 2019

Geoffrey Noyes  
208 Beacon Street  
Marblehead, MA 01945

Re: Environmental Design Review Special Permit 3348

Dear Mr. Noyes,

I am writing to follow-up from the October 7, 2019 Redevelopment Board meeting regarding the property you own located at 833 Massachusetts Avenue in Arlington, MA. This property has a Special Permit 3348 recorded at the Middlesex South Registry of Deeds Book 131350 and Page 69 on August 18, 2009. Pursuant to this Decision, as the property owner you are required to abide by all permit conditions. The Arlington Redevelopment Board requests follow-up on actions with regard to future redevelopment of the "Atwood House" which was discussed with the Board on October 7<sup>th</sup> and is referenced in about Special Condition #5 in the original Special Permit.

The Board requests your attendance at their public hearing on December 16<sup>th</sup> at 8:30 p.m. in the Town Hall Auditorium at 730 Massachusetts Avenue. The Board requests your attendance at this meeting in order to provide a property update regarding this special condition and your ability to meet the permit requirement.

Please contact me at your earliest convenience should you wish to discuss these matters. Otherwise, your presence is expected at the hearing date, time, and location noted above.

Sincerely,

A handwritten signature in cursive script, appearing to read "J. Raitt".

Jennifer Raitt  
Secretary Ex-Officio

cc: Andrew Bunnell, Chair  
Robert J. Annese, Esquire



## ARLINGTON REDEVELOPMENT BOARD

TOWN HALL ARLINGTON, MASSACHUSETTS 02476

TELEPHONE 781-316-3090

December 10, 2019

CVS  
c/o Gary McCoy, Poyant Signs  
125 Samuel Barnet Boulevard  
New Bedford, MA 02745

Re: 833 Massachusetts Avenue, Docket No. 3348


Dear Mr. McCoy,

Enclosed please find a copy of special permit, Docket No. 3348, for the property located at 833 Massachusetts Avenue, Arlington, MA 02476. The Decision was filed with the Town Clerk on November 18, 2019 and then certified on December 8, 2019 after the expiration of the appeal period.

Please pay close attention to the general and special conditions. Some of them, such as recording the Decision at the Registry of Deeds, must be done before you can apply for a building permit. It is our practice to forward the special permit to the Building Inspector after such conditions have been fulfilled and we have been provided evidence of same.

Should you have any questions, please contact me at your earliest convenience.

Yours truly,

  
Jennifer Raitt  
Secretary ex-officio

Encl: Special Permit

Cc: Michael Byrne, Building Inspector

I hereby certify this is a True Copy of the Decision of the Arlington Redevelopment Board as filed with the Office of the Town Clerk of the Town of Arlington, Massachusetts on and that 20 days have elapsed after the Decision and no Appeal has been filed. ATTEST:

Date of Issue 12-9-2019 ASST. Town Clerk



TOWN CLERK'S OFFICE  
ARLINGTON, MA 02174

2019 NOV 18 AM 9:34

RECEIVED

## ARLINGTON REDEVELOPMENT BOARD

Arlington, Massachusetts  
Middlesex, ss

DOCKET NO. 3348

DECISION  
Special Permit Under  
ENVIRONMENTAL DESIGN REVIEW

Applicant: CVS  
One CVS Drive, Woonsocket, RI 02895  
Property Address: 833 Massachusetts Avenue, Arlington, Massachusetts 02476

Date of Hearings: August 12, 2019, November 4, 2019  
Date of Decision: November 4, 2019

20 Day Appeal Period Ends: December 8, 2019

Members  
Approved

Opposed

Eugene B. Benson  
David M. Woot  
[Signature]  
[Signature]

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Jessie G. Weber  
ASST. Town Clerk's Certification

12-9-2019  
Date



I hereby certify this is a True Copy of the Decision of the Arlington Redevelopment Board as filed with the Office of the Town Clerk of the Town of Arlington, Massachusetts on and that 20 days have elapsed after the Decision and no Appeal has been filed ATTEST:

Date of Issue 12-9-2019



TOWN CLERK'S OFFICE  
ARLINGTON, MA 02178

2019 NOV 18 AM 9:33

RECEIVED

Town Clerk  
**Town of Arlington, Massachusetts**

**Redevelopment Board**

730 Massachusetts Avenue, Arlington, Massachusetts 02476

## **DECISION OF THE BOARD**

**Environmental Design Review Docket #3348**

**833 Massachusetts Avenue, Arlington, MA 02476**

**CVS**

**November 4, 2019**

This Decision applies to the re-opening of Special Permit Docket 3348 by CVS to install new signage consistent with CVS rebranding. The CVS store is located at 833 Massachusetts Avenue within a B4 Vehicular Oriented Business District. The re-opening of the Special Permit is to allow the Board to review and approve the signage, under Section 3.4, Environmental Design Review, and section 6.2, Signs. A public hearing was held on August 12, 2019, and continued to November 4, 2019, when this decision was rendered.

Materials submitted for consideration of this application:

Application for Environmental Design Review Special Permit application dated June 27, 2019.

**The following criteria have been met, per Section 3.3.3, Arlington Zoning Bylaw:**

1. The retail pharmacy is allowed in the B-4 Vehicular Oriented Business District.
2. The retail pharmacy has operated in this location for many years.
3. There are no exterior alterations other than signage.
4. The retail pharmacy will not overload any public utilities: public water, drainage or sewer system or any other municipal system.
5. No special regulations are applicable to the use.
6. The use does not impair the integrity or character of the neighborhood. Although additional directional signs will be installed to assist in circulation on the site, the large wall signs will be smaller than the existing signage on the building.
7. The use will not be in excess or detrimental to the character of the neighborhood.

The following criteria have been met, per Section 3.4.4, Arlington Zoning Bylaw:

**A. EDR-1 Preservation of Landscape**

There are no changes to the site that would impact existing natural features.

**B. EDR-2 Relation of the Building to the Environment**

There are no changes to the exterior of the building other than the installation of new signage to replace the existing signage.

**C. EDR-3 Open Space**

The 2009 Decision indicated that landscaping would be installed between the front of the building and the Massachusetts Avenue sidewalk. This area is entirely sidewalk and three benches are present. The tenant and the property owner will work with the Department of Planning and Community Development to come to a reasonable solution that reflects the previous Decision. There are no other changes to open space as a result of the signage rebranding.

**D. EDR-4 Circulation**

The existing circulation does not change; however, the addition of a Do Not Enter sign will help ensure that internal circulation occurs as it is intended.

**E. EDR-5 Surface Water Drainage**

The signage rebranding will not affect surface water run-off.

**F. EDR-6 Utilities Service**

There are no changes to the utility service as a result of the signage rebranding.

**G. EDR-7 Advertising Features**

The existing CVS signage includes a slash, and reads as CVS/pharmacy. The rebranding eliminates the slash, but includes a heart shape in front of the words CVS pharmacy. The rebranding retains the typical red color associated with CVS.

The new signage includes removing the large signage above the main entrance of the building and other plaques, and replacing it with updated signage. A Do Not Enter sign will be installed. All other directional signage will be retained.

The signage on the Massachusetts Avenue frontage is currently 75.18 square feet and will be replaced with signage that measures approximately 33.08 square feet. The reason for the reduction is the size of the letters. The existing letters are approximately 36 inches and the proposed letters are 22.5 inches. Additionally, the new signage will include channel LED illumination.

The main signage facing the parking lot is currently 33.41 square feet and will be replaced with signage that measures approximately 33.08 square feet. The existing letters are

approximately 24 inches and the proposed letters are 22.5 inches. Additionally, the new signage will include channel LED illumination.

Three plaques on the property will be updated. A plaque at the main entrance will be replaced. This plaque conveys information regarding the opening hours, the store manager, and the pharmacy manager. The plaque will remain but the CVS/pharmacy will be replaced with the heart branding. The receiving entrance plaque will be replaced with a 3 square foot plaque. A directional sign will be replaced at the drive-thru pharmacy that indicates both lanes offer full service. It is approximately 4.17 square feet.

A Do Not Enter sign will be installed at the end of the main drive aisle in the parking lot. At the rear of the site, the circulation is one way in order to access the drive-thru pharmacy. The Do Not Enter sign will reinforce the circulation pattern. The sign will be installed about 3 feet above grade and is approximately 2.25 square feet.

An additional directional sign that was not accounted for previously was also acknowledged during the public hearing. The directional signage provides a visual cue on the best way to access the drive through pharmacy.

All other directional signage remains as is on the property.

The reduction in the size of the main signage, the lighting upgrade, and the addition of the Do Not Enter sign are improvements to the property.

**H. EDR-8 Special Features**

There are no changes to the building or the site that would cause any adverse impacts on light, air and water resources, or on noise and temperature levels.

**I. EDR-9 Safety**

There are no changes to the building or the site that would cause any safety or accessibility concerns.

**J. EDR-10 Heritage**

The CVS building is not located on any local or State historic property listing. The adjacent Atwood House is identified as a significant building per Title VI, Article 6 of the Town Bylaw. The signage rebranding does not impact the Atwood House and the 2009 Decision retains jurisdiction over future plans for the structure as does the Historical Commission. The Redevelopment Board requests that the property owner attend the December 16, 2019 meeting to discuss the future of the Atwood House.

**K. EDR-11 Microclimate**

The signage rebranding will not impact the microclimate.

**L. EDR-12 Sustainable Building and Site Design**

The signage rebranding will support sustainable building and site design through the usage of LED fixtures to illuminate the signage.

**The project must adhere to the following general conditions:**

1. The final plans and specifications for signage shall be subject to final approval by the Department of Planning and Community Development (DPCD).
2. Any substantial or material deviation during construction from the approved plans and specifications is subject to the written approval of the Arlington Redevelopment Board.
3. The conditions of the 2009 Special Permit decision are still in force. The Board maintains continuing jurisdiction over this permit and may, after a duly advertised public hearing, attach other conditions or modify these conditions as it deems appropriate in order to protect the public interest and welfare.

**The project must adhere to the following special conditions:**

1. The Applicant and the property owner will work with the Department of Planning and Community Development to come to a reasonable solution that reflects the requirement of the 2009 Decision to install landscaping between the front of the building and the Massachusetts Avenue sidewalk.
2. The Applicant and property owner appear at the December 16, 2019, Redevelopment Board hearing to discuss the ongoing compliance with the 2009 Decision, with special attention to the Atwood House.





Corporate Office  
125 Samuel Barnett Boulevard  
New Bedford, MA 02745

Northern New England  
3 Bud Way #19  
Nashua, NH 03063

September 5, 2019

Attn: Erin Zwirko  
Town of Arlington

Re: CVS Pharmacy 833 Mass Ave: Continuance Request

Dean Erin Zwirko / Town of Arlington,

Please use this letter as formal request for a continuance to October 7<sup>th</sup>, 2019 for the  
Special Permit for CVS Pharmacy, 833 Mass Ave. Arlington MA.

Sincerely,

Gary McCoy  
Account Executive  
Poyant Signs  
508-328-1457



**Town of Arlington, Massachusetts**  
Department of Planning & Community Development  
730 Massachusetts Avenue, Arlington, Massachusetts 02476

---

## **Public Hearing Memorandum**

*The purpose of this memorandum is to provide the Arlington Redevelopment Board and public with technical information and a planning analysis to assist with the regulatory decision-making process.*

**To:** Arlington Redevelopment Board

**From:** Jennifer Raitt, Secretary Ex Officio

**Subject:** Environmental Design Review, 833 Massachusetts Ave, Arlington, MA  
Docket #3348

**Date:** August 6, 2019

---

### **I. Docket Summary**

This is an application by Gary McCoy, Poyant Signs, for CVS, at 833 Massachusetts Avenue, to re-open Special Permit Docket #3348. This is in accordance with the provisions of M.G.L. Chapter 40A Section 11, and the Town of Arlington Zoning Bylaw Section 3.4 Environmental Design Review. The applicant proposes to install new signage consistent with CVS rebranding. The re-opening of the Special Permit is to allow the Board to review and approve the signage, under Section 6.2, Signs.

Materials submitted for consideration of this application:

1. Environmental Design Review Special Permit Application dated June 27, 2019.

### **II. Application of Special Permit Criteria (Arlington Zoning Bylaw, Section 3.3)**

#### **1. Section 3.3.3.A.**

**The use requested is listed as a Special Permit in the use regulations for the applicable district or is so designated elsewhere in this Bylaw.**

The retail pharmacy is allowed in the B-4 Vehicular Oriented Business District. The Board can find that this condition is met.

**2. Section 3.3.3.B.**

**The requested use is essential or desirable to the public convenience or welfare.**

The retail pharmacy has operated in this location for many years. The Board can find that this condition is met.

**3. Section 3.3.3.C.**

**The requested use will not create undue traffic congestion or unduly impair pedestrian safety.**

There are no exterior alterations other than signage. The Board can find that this condition is met.

**4. Section 3.3.3.D.**

**The requested use will not overload any public water, drainage or sewer system or any other municipal system to such an extent that the requested use or any developed use in the immediate area or in any other area of the Town will be unduly subjected to hazards affecting health, safety, or the general welfare.**

This retail pharmacy has operated in this location for many years without overloading any public utilities. The Board can find that this condition is met.

**5. Section 3.3.3.E.**

**Any special regulations for the use as may be provided in the Bylaw are fulfilled.**

All such regulations are fulfilled.

**6. Section 3.3.3.F.**

**The requested use will not impair the integrity or character of the district or adjoining districts, nor be detrimental to the health or welfare.**

The use does not impair the integrity or character of the neighborhood. The Board can find that this condition is met.

**7. Section 3.3.3.G.**

**The requested use will not, by its addition to a neighborhood, cause an excess of the use that could be detrimental to the character of said neighborhood.**

The use will not be in excess or detrimental to the character of the neighborhood. The Board can find that this condition is met.

III. Environmental Design Review Standards (Arlington Zoning Bylaw, Section 3.4)

A. EDR-1 Preservation of Landscape

**The landscape shall be preserved in its natural state, insofar as practicable, by minimizing tree and soil removal, and any grade changes shall be in keeping with the general appearance of neighboring developed areas.**

There are no changes to the landscape as there are no proposed exterior alterations. The Board can find that this condition is met.

B. EDR-2 Relation of the Building to the Environment

**Proposed development shall be related harmoniously to the terrain and to the use, scale, and architecture of the existing buildings in the vicinity that have functional or visible relationship to the proposed buildings. The Arlington Redevelopment Board may require a modification in massing so as to reduce the effect of shadows on the abutting property in an R0, R1 or R2 district or on public open space.**

There are no changes to the exterior of the building other than the installation of new signage replacing the existing signage. The Board can find that this condition is met.

C. EDR-3 Open Space

**All open space (landscaped and usable) shall be so designed as to add to the visual amenities of the vicinity by maximizing its visibility for persons passing by the site or overlooking it from nearby properties. The location and configuration of usable open space shall be so designed as to encourage social interaction, maximize its utility and facilitate maintenance.**

There are no changes to open space. The Board can find that this condition is met.

D. EDR-4 Circulation

**With respect to vehicular and pedestrian and bicycle circulation, including entrances, ramps, walkways, drives, and parking, special attention shall be given to location and number of access points to the public streets (especially in relation to existing traffic controls and mass transit facilities), width of interior drives and access points, general interior circulation, separation of pedestrian and vehicular traffic, access to community facilities, and arrangement of vehicle parking and bicycle parking areas, including bicycle parking spaces required by Section 6.1.12 that are safe and convenient and, insofar as practicable, do not detract from the use and enjoyment of proposed buildings and structures and the neighboring properties.**

The existing circulation does not change; however, the addition of a Do Not Enter sign will help ensure that circulation occurs as it is intended. The Board can find that this condition is met.



**E. EDR-5 Surface Water Drainage**

Special attention shall be given to proper site surface drainage so that removal of surface waters will not adversely affect neighboring properties or the public storm drainage system. Available Best Management Practices for the site should be employed, and include site planning to minimize impervious surface and reduce clearing and re-grading. Best Management Practices may include erosion control and stormwater treatment by means of swales, filters, plantings, roof gardens, native vegetation, and leaching catch basins. Stormwater should be treated at least minimally on the development site; that which cannot be handled on site shall be removed from all roofs, canopies, paved and pooling areas and carried away in an underground drainage system. Surface water in all paved areas shall be collected in intervals so that it will not obstruct the flow of vehicular or pedestrian traffic and will not create puddles in the paved areas.

In accordance with Section 3.3.4., the Board may require from any applicant, after consultation with the Director of Public Works, security satisfactory to the Board to insure the maintenance of all stormwater facilities such as catch basins, leaching catch basins, detention basins, swales, etc. within the site. The Board may use funds provided by such security to conduct maintenance that the applicant fails to do.

The Board may adjust in its sole discretion the amount and type of financial security such that it is satisfied that the amount is sufficient to provide for any future maintenance needs.

There will be no changes to the exterior of the building or surface water run-off as a result of this proposal. The Board can find that this condition is met.

**F. EDR-6 Utilities Service**

Electric, telephone, cable TV, and other such lines of equipment shall be underground. The proposed method of sanitary sewage disposal and solid waste disposal from all buildings shall be indicated.

There will be no changes to the utility service as a result of this proposal. The Board can find that this condition is met.

**G. EDR-7 Advertising Features**

The size, location, design, color, texture, lighting and materials of all permanent signs and outdoor advertising structures or features shall not detract from the use and enjoyment of proposed buildings and structures and the surrounding properties.

The existing CVS signage includes a slash, and appears as CVS/pharmacy. The proposed rebranding eliminates the slash and includes a heart in front of the words CVS pharmacy. The heart rebranding retains the typical red color associated with CVS.

The scope of work for the new signage includes removing the large signage above the main entrance of the building and other plaques, and replacing it with updated signage. A new Do Not Enter sign will be installed. All other directional signage will be retained.

The signage on the Massachusetts Avenue frontage is currently 75.18 square feet and will be replaced with signage that measures approximately 33.08 square feet. The reason for the reduction is the size of the letters. The existing letters are approximately 36 inches and the proposed letters are 22.5 inches. Additionally, the new signage will include channel LED illumination.

The main signage facing the parking lot is currently 33.41 square feet and will be replaced with signage that measures approximately 33.08 square feet. The existing letters are approximately 24 inches and the proposed letters are 22.5 inches. Additionally, the new signage will include channel LED illumination.

Three plaques on the property will be updated. A plaque at the main entrance will be replaced. This plaque conveys information regarding the opening hours, the store manager, and the pharmacy manager. The plaque will remain but the CVS/pharmacy will be replaced with the heart branding. The receiving entrance plaque will be replaced with a 3 square foot plaque. A directional sign will be replaced at the drive-thru pharmacy that indicates both lanes offer full service. It is approximately 4.17 square feet.

A new Do Not Enter sign will be installed at the end of the main drive aisle in the parking lot. At the rear of the site, the circulation is one way in order to access the drive-thru pharmacy. The Do Not Enter sign will reinforce the circulation pattern. The sign will be installed about 3 feet above grade and is approximately 2.25 square feet. All other directional signage remains as is on the property.

The reduction in the size of the main signage, the lighting upgrade, and the addition of the Do Not Enter sign are improvements to the property. The Board can find that this condition is met.

#### **H. EDR-8 Special Features**

**Exposed storage areas, exposed machinery installations, service areas, truck loading areas, utility buildings and structures, and similar accessory areas and structures shall be subject to such setbacks, screen plantings or other screening methods as shall reasonably be required to prevent their being incongruous with the existing or contemplated environment and the surrounding properties.**

No changes are proposed. The Board can find that this condition is met.

**I. EDR-9 Safety**

**With respect to personal safety, all open and enclosed spaces shall be designed to facilitate building evacuation and maximize accessibility by fire, police and other emergency personnel and equipment. Insofar as practicable, all exterior spaces and interior public and semi-public spaces shall be so designed to minimize the fear and probability of personal harm or injury by increasing the potential surveillance by neighboring residents and passersby of any accident or attempted criminal act.**

No changes are proposed. The Board can find that this condition is met.

**J. EDR-10 Heritage**

**With respect to Arlington's heritage, removal or disruption of historic, traditional or significant uses, structures or architectural elements shall be minimized insofar as practical whether these exist on the site or on adjacent properties.**

The CVS building is not located on any local or State historic property listing. However, the adjacent Atwood House is identified as a significant building per Title VI, Article 6 of the Town Bylaw. The signage rebranding does not impact the Atwood House and the Special Permit Decision retains jurisdiction over future plans for the structure as does the Historical Commission. The Board can find that this condition is met.

**K. EDR-11 Microclimate**

**With respect to the localized climatic characteristics of a given area, any development which proposes new structures, new hard surface, ground coverage or the installation of machinery which emits heat, vapor or fumes shall endeavor to minimize insofar as practicable, any adverse impacts on light, air and water resources or on noise and temperature levels of the immediate environment.**

No changes are proposed. The Board can find that this condition is met.

**L. EDR-12 Sustainable Building and Site Design**

**Projects are encouraged to incorporate best practices related to sustainable sites, water efficiency, energy and atmosphere, materials and resources, and indoor environmental quality. Applicants must submit a current Green Building Council Leadership in Energy and Environmental Design (LEED) checklist, appropriate to the type of development, annotated with narrative description that indicates how the LEED performance objectives will be incorporated into the project.**

No changes are proposed. The Board can find that this condition is met.

**IV. Conditions**

1. The final plans and specifications for signage shall be subject to final approval by the Department of Planning and Community Development (DPCD).

2. Any substantial or material deviation during construction from the approved plans and specifications is subject to the written approval of the Arlington Redevelopment Board.
3. The conditions of the original Special Permit decision are still in force. The Board maintains continuing jurisdiction over this permit and may, after a duly advertised public hearing, attach other conditions or modify these conditions as it deems appropriate in order to protect the public interest and welfare.





**ARLINGTON REDEVELOPMENT BOARD**

TOWN HALL ARLINGTON, MASSACHUSETTS 02476

TELEPHONE 781-316-3090

August 15, 2019

Geoffrey Noyes  
208 Beacon Street  
Marblehead, MA 01945

Re: Environmental Design Review Special Permit 3348 - CVS

Dear Mr. Noyes,

I am writing regarding the condition of property you own located at 833 Massachusetts Avenue in Arlington, MA. This property has a Special Permit 3348 recorded at the Middlesex South Registry of Deeds Book 131350 and Page 69 on August 18, 2009. Pursuant to this Decision, as the property owner you are required to abide by all permit conditions. The Arlington Redevelopment Board is specifically concerned about Special Condition #5 with regard to your responsibility to maintain the "Atwood House". This condition requires you to make reasonable and diligent efforts to prevent any damage from the elements or otherwise, until the property is redeveloped. Currently, the property is partially boarded and partially exposed to the elements. The partial exposure is not compliant with the Decision.

Your tenant CVS Health recently requested an amendment to Special Permit 3348 in order to increase the number of signs on the property. At the opening of the public hearing on August 12<sup>th</sup>, the Board quickly indicated that they were concerned about broader property issues referenced above and subsequently continued the hearing to September 9<sup>th</sup> at 7:30 p.m. in the Town Hall, 730 Massachusetts Avenue, second floor conference room. The Board requests your attendance at this meeting in order to provide a property update regarding the conditions and your ability to meet permit requirements.

Please contact me at your earliest convenience should you wish to discuss these matters. Otherwise, your presence is expected at the hearing date and time noted above.

Sincerely,

  
Jennifer Raitt  
Secretary Ex-Officio

cc: Andrew Bunnell, Chair  
Robert J. Annese, Esquire



June 27, 2019

Town of Arlington  
Planning & Community Development  
730 Mass Ave. Annex  
Arlington, MA 02476

Re: 833 Mass Ave. CVS Pharmacy Signage Rebrand

Dear Town of Arlington,

Please find enclosed special sign permit application for Environmental Design Review and supporting documents as needed.

2 copies of:

- Application & Plans
- Owner Approval Letter from CVS
- COI & Worker's Comp.
- Construction Supervisor License
- Original Sign Permit Application
- \$500 Permit Fee (Check)
- Dimensional & Parking Info Form
- Site Plan
- Drawing of Existing Conditions and Drawing of Proposed
- Photographs
- Impact Statement

An electronic copy of these materials have been sent to Erin Zwirko.

If you have any questions, please call me at 508-328-1457.

Sincerely,

**Gary McCoy**  
Account Executive  
125 Samuel Barnet Boulevard  
New Bedford, MA 02745  
508.328.1457



TOWN OF ARLINGTON  
REDEVELOPMENT BOARD

Application for Special Permit In Accordance with Environmental Design  
Review Procedures (Section 3.4 of the Zoning Bylaw)

Docket No. \_\_\_\_\_

1. Property Address 833 Massachusetts Ave Arlington, MA 02476  
Name of Record Owner(s) CVS Pharmacy Phone 401-770-3047  
Address of Owner One CVS Drive, MC 1190, Woonsocket, RI 02895  
Street City, State, Zip
2. Name of Applicant(s) (if different than above) Poyant Signs  
Address 125 Samuel Barnet Blvd. New Bedford MA 02745 Phone 508-328-1457  
Status Relative to Property (occupant, purchaser, etc.) Sign Contractor
3. Location of Property 52-1-1  
Assessor's Block Plan, Block, Lot No.
4. Deed recorded in the Registry of deeds, Book \_\_\_\_\_, Page \_\_\_\_\_;  
-or- registered in Land Registration Office, Cert. No. \_\_\_\_\_, in Book \_\_\_\_\_, Page \_\_\_\_\_.
5. Present Use of Property (include # of dwelling units, if any) Pharmacy/Commercial
6. Proposed Use of Property (include # of dwelling units, if any) Pharmacy/Commercial (Like for Like)
7. Permit applied for in accordance with the following Zoning Bylaw section(s)

<u>3.3</u>	<u>Special Permits</u>
<u>3.4</u>	<u>Environmental Design Review</u>
<u>6.2</u>	<u>Signs</u>
_____	_____

section(s) title(s)
8. Please attach a statement that describes your project and provide any additional information that may aid the ARB in understanding the permits you request. Include any reasons that you feel you should be granted the requested permission.  
  
see attached statement

(In the statement below, strike out the words that do not apply)

The applicant states that CVS Pharmacy is the ~~owner -or- occupant -or-~~ purchaser under agreement of the property in Arlington located at 833 Massachusetts Ave which is the subject of this application; and that unfavorable action -or- no unfavorable action has been taken by the Zoning Board of Appeals on a similar application regarding this property within the last two years. The applicant expressly agrees to comply with any and all conditions and qualifications imposed upon this permission, either by the Zoning Bylaw or by the Redevelopment Board, should the permit be granted.

Signature of Applicant(s)

125 Samuel Barnet Blvd. New Bedford MA 02745

Address

508-328-1457

Phone

# TOWN OF ARLINGTON

Dimensional and Parking Information  
for Application to  
The Arlington Redevelopment Board

Docket No. \_\_\_\_\_

Property Location 833 Massachusetts Ave

Zoning District B4

Owner: CVS Pharmacy

Address: One CVS Drive, MC 1190 Woonsocket, RI 02895

Present Use/Occupancy: No. of Dwelling Units:

Uses and their gross square feet:

Pharmacy

Proposed Use/Occupancy: No. of Dwelling Units:

Uses and their gross square feet:

Pharmacy

	<u>Present Conditions</u>	<u>Proposed Conditions</u>	<u>Min. or Max. Required by Zoning for Proposed Use</u>
Lot Size	79715 SF	79715 SF	min.
Frontage			min.
Floor Area Ratio			max.
Lot Coverage (%), where applicable			max.
Lot Area per Dwelling Unit (square feet)	NA	NA	min. NA
Front Yard Depth (feet)			min.
Side Yard Width (feet) right side			min.
left side			min.
Rear Yard Depth (feet)			min.
Height			min.
Stories	1	1	stories
Feet			feet
Open Space (% of G.F.A.)			min.
Landscaped (square feet)			(s.f.)
Usable (square feet)			(s.f.)
Parking Spaces (No.)			min.
Parking Area Setbacks (feet), where applicable			min.
Loading Spaces (No.)			min.
Type of Construction	Signage		
Distance to Nearest Building			min.





June 27, 2019

Town of Arlington  
Planning & Community Development  
730 Mass Ave. Annex  
Arlington, MA 02476

Re: Impact Statement

Dear Town of Arlington,

Poyant has been contracted with CVS pharmacy to rebrand their existing signage at their location at 833 Massachusetts Avenue, Arlington MA. Due to the scope of work, we feel that there will be no negative impact as it relates to the Arlington Environmental Design Review Standards. The existing signage that is currently at this location include lighting systems that can be more hazardous to the environment due to the amount of energy used. We will be replacing this existing lighting system with new, energy-efficient LEDs. The following is the scope of work for this proposed signage project:

- Remove existing (2) 24" Main ID Channel Lettersets and plaques.
- Replace with (2) 22" Channel Letters (LED Illuminated) and new regulatory plaques.
- Install Do Not Enter sign.
- All directional signage will remain as is.

Please review the attached signage plans for more details (install locations, etc.).

If you have any questions, please call me at 508-328-1457.

Sincerely,

Gary McCoy  
Account Executive  
125 Samuel Barnet Boulevard  
New Bedford, MA 02745  
508.328.1457

June 19, 2019

To: Building Inspector

Re: CVS pharmacy contracted sign vendor

To Whom It May Concern:

CVS pharmacy has awarded Poyant Signs Inc. as the exterior sign vendor throughout New England and New York. Poyant Signs Inc. has a term contract in place with CVS pharmacy.

Please accept this letter to allow Poyant Signs Inc. & their respective agents to act as our agent, pull permits and install signs as per code.

Sincerely,

*Karen Rezukey*

Karen Rezukey

Properties Programs and Strategy Administration, Sr. Consultant Exterior Branding





## 2019 THH BIR Upgrade



Existing Location Overview



Proposed Signage Overview

**Loc#: 00278**

833 Massachusetts Ave.  
Arlington, MA 02476

### SITE SIGNAGE SUMMARY

#### EXISTING

- (1) 24" Main ID Channel Letterset
- (2) 24" Main ID Channel Letterset
- (3) Drive-Thru Information Panel
- (4) Pharmacy Pick-Up/Drop-off Plaque
- (5) Missing Do Not Enter Sign
- (6) Receiving Entrance Plaque
- (7) Hours Plaque
- (8) Directional Signs (Qty:4)

#### PROPOSED

- (1) IL-22-CL-L LED Heart
- (2) IL-22-CL-L LED Heart
- (3) Full Service Regulatory Plaque
- (4) Remove and Dispose
- (5) Do Not Enter Sign
- (6) Receiving Entrance Plaque
- (7) Hours Plaque
- (8) Leave As Is





Drawing prepared by:	<b>SITE PLAN</b>	
Location:	833 Massachusetts Ave.	Proj #:
	Arlington, MA 02476	16189
File Path:		278
ACCOUNTS\CVS pharmacy\Locations 2016\3018_MC\3018_0000_City_ST		



Rev#:	Req#:	Date:	Req. By:	Drawn By:	Revision Description:	Rev#:	Req#:	Date:	Req. By:	Drawn By:
Original	000000	4/16/19	GM	CC		Rev 7	000000	00/00/00	XXX	XXX
Rev 1	000000	6/13/19	GM	CC	Notes	Rev 8	000000	00/00/00	XXX	XXX
Rev 2	000000	00/00/00	XXX	XXX		Rev 9	000000	00/00/00	XXX	XXX
Rev 3	000000	00/00/00	XXX	XXX		Rev 10	000000	00/00/00	XXX	XXX
Rev 4	000000	00/00/00	XXX	XXX						
Rev 5	000000	00/00/00	XXX	XXX						
Rev 6	000000	00/00/00	XXX	XXX						





#### NOTES:

Drawing prepared by: **SITE PLAN**

Location: 833 Massachusetts Ave.  
Arlington, MA 02476

Proj #: 16189  
Loc #: 278

File Path: ACCOUNTS\C\CVS pharmacy\Locations 2016\3018\_MC\3018\_0000\_City\_ST



Drawing prepared for:

Rev#:	Req#:	Date:	Req. By:	Drawn By:	Revision Description:	Rev#:	Req#:	Date:	Req. By:	Drawn By:
Original	000000	4/16/19	GM	CC		Rev 7	000000	00/00/00	XXX	XXX
Rev 1	000000	6/13/19	GM	CC	Notes	Rev 8	000000	00/00/00	XXX	XXX
Rev 2	000000	00/00/00	XXX	XXX		Rev 9	000000	00/00/00	XXX	XXX
Rev 3	000000	00/00/00	XXX	XXX		Rev 10	000000	00/00/00	XXX	XXX
Rev 4	000000	00/00/00	XXX	XXX						
Rev 5	000000	00/00/00	XXX	XXX						
Rev 6	000000	00/00/00	XXX	XXX						

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**NOTES:**

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**SITE PLAN**

Location: 833 Massachusetts Ave.  
Arlington, MA 02476  
Proj #: 16189  
Loc #: 278

File Path: ACCOUNTS\C\CVS pharmacy\Locations 2016\3018\_MC\3018\_0000\_City\_ST

Drawing prepared for:



Rev#:	Req#:	Date:	Req. By:	Drawn By:	Revision Description:	Rev#:	Req#:	Date:	Req. By:	Drawn By:
Original	000000	4/16/19	GM	CC		Rev 7	000000	00/00/00	XXX	XXX
Rev 1	000000	6/13/19	GM	CC	Notes	Rev 8	000000	00/00/00	XXX	XXX
Rev 2	000000	00/00/00	XXX	XXX		Rev 9	000000	00/00/00	XXX	XXX
Rev 3	000000	00/00/00	XXX	XXX		Rev 10	000000	00/00/00	XXX	XXX
Rev 4	000000	00/00/00	XXX	XXX						
Rev 5	000000	00/00/00	XXX	XXX						
Rev 6	000000	00/00/00	XXX	XXX						




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**Poyant**  
Building Your Brand

Drawing prepared by:  	<b>SITE PLAN</b>	
	<u>Location:</u>	<u>Proj #:</u>
	833 Massachusetts Ave. Arlington, MA 02476	16189
	<u>File Path:</u>	<u>Loc #:</u>
		278
ACCOUNTS\C\CVS pharmacy\Locations 2016\3018_MC\3018_0000_City_ST		



Drawing prepared for:

Rev#:	Req#:	Date:	Req. By:	Drawn By:	Revision Description:					
Original	000000	4/16/19	GM	CC						
Rev1	000000	6/13/19	GM	CC	Notes	Rev#:	Req#:	Date:	Req. By:	Drawn By:
Rev2	000000	00/00/00	XXX	XXX		Rev 7	000000	00/00/00	XXX	XXX
Rev3	000000	00/00/00	XXX	XXX		Rev8	000000	00/00/00	XXX	XXX
Rev4	000000	00/00/00	XXX	XXX		Rev9	000000	00/00/00	XXX	XXX
Rev5	000000	00/00/00	XXX	XXX		Rev10	000000	00/00/00	XXX	XXX
Rev6	000000	00/00/00	XXX	XXX		Pg. 4				





NOTES:

Drawing prepared by: **SITE PLAN**

Location: 833 Massachusetts Ave.  
Arlington, MA 02476

Proj #: 16189  
Loc #: 278

File Path: ACCOUNTS\C\CVS pharmacy\Locations 2016\3018\_MC\3018\_0000\_City\_ST



Rev#:	Req#:	Date:	Req. By:	Drawn By:	Revision Description:	Rev#:	Req#:	Date:	Req. By:	Drawn By:
Original	000000	4/16/19	GM	CC	Notes	Rev 7	000000	00/00/00	XXX	XXX
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Rev 2	000000	00/00/00	XXX	XXX		Rev 9	000000	00/00/00	XXX	XXX
Rev 3	000000	00/00/00	XXX	XXX		Rev 10	000000	00/00/00	XXX	XXX
Rev 4	000000	00/00/00	XXX	XXX						
Rev 5	000000	00/00/00	XXX	XXX						
Rev 6	000000	00/00/00	XXX	XXX						





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NOTES: Approach photos

Drawing prepared by: **OVERVIEW PHOTOS**



Location: 833 Massachusetts Ave.  
Arlington, MA 02476  
Proj #: 16189  
Loc #: 278

File Path: ACCOUNTS\CVS pharmacy\Locations 2016\3018\_MC\3018\_0000\_City\_ST



Drawing prepared for:

Rev#:	Req#:	Date:	Req. By:	Drawn By:	Revision Description:	Rev#:	Req#:	Date:	Req. By:	Drawn By:
Original	000000	4/16/19	GM	CC		Rev 7	000000	00/00/00	XXX	XXX
Rev 1	000000	6/13/19	GM	CC	Notes	Rev 8	000000	00/00/00	XXX	XXX
Rev 2	000000	00/00/00	XXX	XXX		Rev 9	000000	00/00/00	XXX	XXX
Rev 3	000000	00/00/00	XXX	XXX		Rev 10	000000	00/00/00	XXX	XXX
Rev 4	000000	00/00/00	XXX	XXX						
Rev 5	000000	00/00/00	XXX	XXX						
Rev 6	000000	00/00/00	XXX	XXX						

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Existing Signage - West Elevation

**Existing Sign #1 (33.41 SQ.FT.)**

24" Main ID Channel Letterset  
Illuminated

S/F ☒ D/F ☐  
Yes ☒ No ☐



Proposed Signage - West Elevation

Proposed Signage

Not To Scale Unless Noted



IL-33-CL-L (33.08 SqFt)

1

Drawing prepared by:

**RECOMMENDATION**

**Poyant**  
Building Your Brand

Location: 833 Massachusetts Ave.  
Arlington, MA 02476  
Proj #: 16189  
Loc #: 278

File Path: ACCOUNTS\CVS pharmacy\Locations 2016\3018\_MC\3018\_0000\_City\_ST

Drawing prepared for:



Rev#:	Req#:	Date:	Req.By:	Drawn By:	Revision Description:	Rev#:	Req#:	Date:	Req.By:	Drawn By:
Original	000000	4/16/19	GM	CC	Notes	Rev 7	000000	00/00/00	XXX	XXX
Rev 1	000000	6/13/19	GM	CC		Rev 8	000000	00/00/00	XXX	XXX
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Rev 3	000000	00/00/00	XXX	XXX		Rev 10	000000	00/00/00	XXX	XXX
Rev 4	000000	00/00/00	XXX	XXX						
Rev 5	000000	00/00/00	XXX	XXX						
Rev 6	000000	00/00/00	XXX	XXX						

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Existing Signage - South Elevation

<b>Existing Sign #2 (75.18 SQ.FT.)</b>			
36" Main ID Channel Letters			
Illuminated	S/F <input checked="" type="checkbox"/>	D/F <input type="checkbox"/>	
	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	
<b>Existing Sign #7 (2 SQ.FT.)</b>			
Hours Plaque			
Illuminated	S/F <input checked="" type="checkbox"/>	D/F <input type="checkbox"/>	
	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	



Proposed Signage - South Elevation

Proposed Signage

**CVS pharmacy**

IL-22-CL-L LED Heart (33.08 SqFt)

2



7

Reface Header  
Only with new  
Vinyl Graphics

Not To Scale Unless Noted

Drawing prepared by:

**RECOMMENDATION**

**Poyant**  
Building Your Brand

Location: 833 Massachusetts Ave.  
Arlington, MA 02476  
File Path: 278

Proj #: 16189  
Loc #: 278

Drawing prepared for:

**CVS pharmacy**

ACCOUNTS\C\CVS pharmacy\Locations 2016\3018\_MC\3018\_0000\_City\_ST

Rev#:	Req#:	Date:	Req. By:	Drawn By:	Revision Description:	Rev#:	Req#:	Date:	Req. By:	Drawn By:
Original	000000	4/16/19	GM	CC		Rev 7	000000	00/00/00	XXX	XXX
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Rev 3	000000	00/00/00	XXX	XXX		Rev 10	000000	00/00/00	XXX	XXX
Rev 4	000000	00/00/00	XXX	XXX						
Rev 5	000000	00/00/00	XXX	XXX						
Rev 6	000000	00/00/00	XXX	XXX						

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Existing Signage - North Elevation

<b>Existing Sign #3 (12 SQ.FT.)</b>	
Drive-Thru Information Panel	S/F <input checked="" type="checkbox"/> D/F <input type="checkbox"/>
Illuminated	Yes <input type="checkbox"/> No <input checked="" type="checkbox"/>
<b>Existing Sign #4 (1 SQ.FT.)</b>	
Pharmacy Pick-Up/Drop-Off Plaque	S/F <input checked="" type="checkbox"/> D/F <input type="checkbox"/>
Illuminated	Yes <input type="checkbox"/> No <input checked="" type="checkbox"/>



Proposed Signage - North Elevation

Proposed Signage



(4.17 SqFt)

3

Not To Scale Unless Noted

Drawing prepared by:		<b>RECOMMENDATION</b>	
Location:	833 Massachusetts Ave.	Proj #:	16189
	Arlington, MA 02476	Loc #:	278
File Path:	ACCOUNTS\CVS pharmacy\Locations 2016\3018_MC\3018_0000_City_ST		



Rev #:	Req #:	Date:	Req. By:	Drawn By:	Revision Description:	Rev #:	Req #:	Date:	Req. By:	Drawn By:
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Rev 2	000000	00/00/00	XXX	XXX		Rev 9	000000	00/00/00	XXX	XXX
Rev 3	000000	00/00/00	XXX	XXX		Rev 10	000000	00/00/00	XXX	XXX
Rev 4	000000	00/00/00	XXX	XXX						
Rev 5	000000	00/00/00	XXX	XXX						
Rev 6	000000	00/00/00	XXX	XXX						

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Existing Signage - North Elevation

Existing Sign #6 (3 SQ.FT.)  
Receiving Entrance Plaque  
Illuminated

S/F ☒ D/F ☐  
Yes ☐ No ☒



Proposed Signage - North Elevation

## Proposed Signage



(3 SqFt)

6

Not To Scale Unless Noted

Drawing prepared by:

## RECOMMENDATION

Drawing prepared for:

**Poyant**  
Building Your Brand

Location: 833 Massachusetts Ave.  
Arlington, MA 02476  
Proj #: 16189  
Loc #: 278

File Path: ACCOUNTS\CVS pharmacy\Locations 2016\3018\_MC\3018\_0000\_City\_ST



Rev#:	Req#:	Date:	Req. By:	Drawn By:	Revision Description:	Rev#:	Req#:	Date:	Req. By:	Drawn By:
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Rev 2	000000	00/00/00	XXX	XXX		Rev 9	000000	00/00/00	XXX	XXX
Rev 3	000000	00/00/00	XXX	XXX		Rev 10	000000	00/00/00	XXX	XXX
Rev 4	000000	00/00/00	XXX	XXX						
Rev 5	000000	00/00/00	XXX	XXX						
Rev 6	000000	00/00/00	XXX	XXX						

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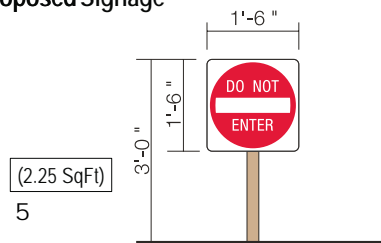


Existing Signage - West Elevation



Proposed Signage - West Elevation

## Proposed Signage



Not To Scale Unless Noted

Drawing prepared by:

## RECOMMENDATION

Drawing prepared for:

**Poyant**  
Building Your Brand

Location: 833 Massachusetts Ave.  
Arlington, MA 02476  
Proj #: 16189  
Loc #: 278

File Path: ACCOUNTS\CVS pharmacy\Locations 2016\3018\_MC\3018\_0000\_City\_ST

**CVS**  
pharmacy

Rev#:	Req#:	Date:	Req. By:	Drawn By:	Revision Description:	Rev#:	Req#:	Date:	Req. By:	Drawn By:
Original	000000					Rev 7	000000	00/00/00	XXX	XXX
Rev 1	000000	6/13/19	GM	CC	Notes	Rev 8	000000	00/00/00	XXX	XXX
Rev 2	000000	00/00/00	XXX	XXX		Rev 9	000000	00/00/00	XXX	XXX
Rev 3	000000	00/00/00	XXX	XXX		Rev 10	000000	00/00/00	XXX	XXX
Rev 4	000000	00/00/00	XXX	XXX						
Rev 5	000000	00/00/00	XXX	XXX						
Rev 6	000000	00/00/00	XXX	XXX						

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QTY: 2 Sets



CVS pharmacy

One CVS Drive  
Woonsocket, RI

Specifications

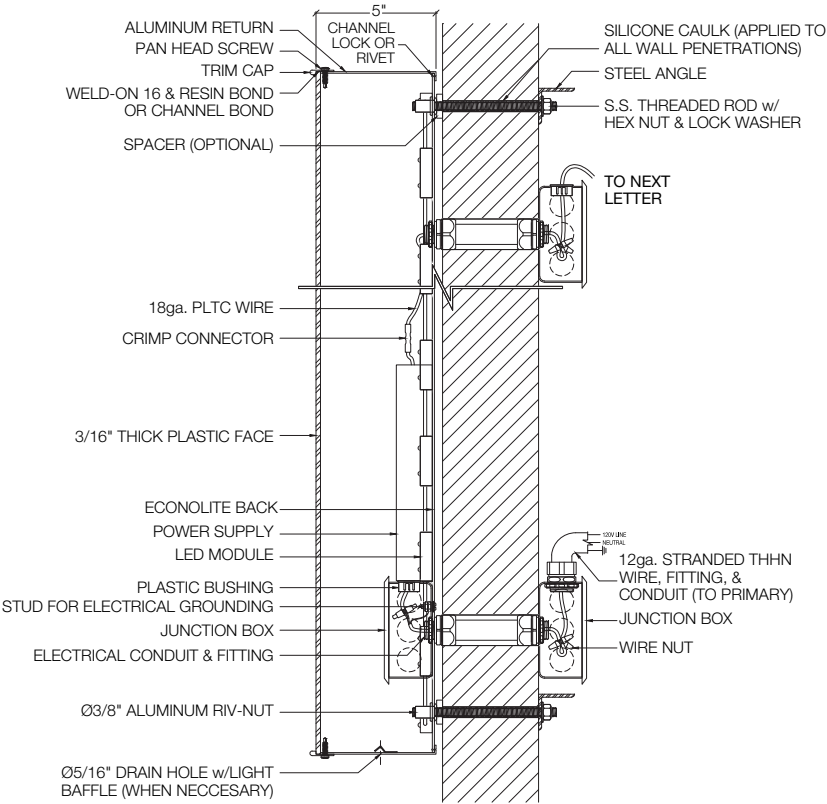
Qty:TBD 33.08 SqFt (Each)

Internally Illuminated Channel Letters (Horizontal)

- Chemcast 3/16" Red acrylic faces #2793
- 5" Deep pre-finished Hunter Red returns
- 3/4" Jewelite True Red trim cap
- Red LED illumination, GEMXRD-W1, 132 mod
- 120 Volt Power supplies, (Qty:1) GEPS12-60U-NA, (Qty:1) GEPS12-25U-NA
- 0.9 Amps, 77.88 Watts
- Mounted to existing fascia as required

Colors & Materials

- Chemcast Red Acrylic #2793
- Jewelite True Red Trim Cap
- Pre-Finished Hunter Red Returns



Drawn By: L. Rogers

File Name: Main ID\_  
Channel Letters\_Horizontal

Revisions:

UL  
This sign is intended to be installed in accordance with the requirements of Article 600 of the National Electrical Code and / or other applicable local codes. This includes proper grounding and bonding of the sign.

Approved By:  
Approved for Construction

Martin B Higgins III  
Date:

Main ID Channel Letters  
Horizontal - IL-22-CL-L

CHANNEL LETTERS

Existing Channel Letters

QTY: 2 Sets



Specifications

Qty = 1 33.41 Sq Ft

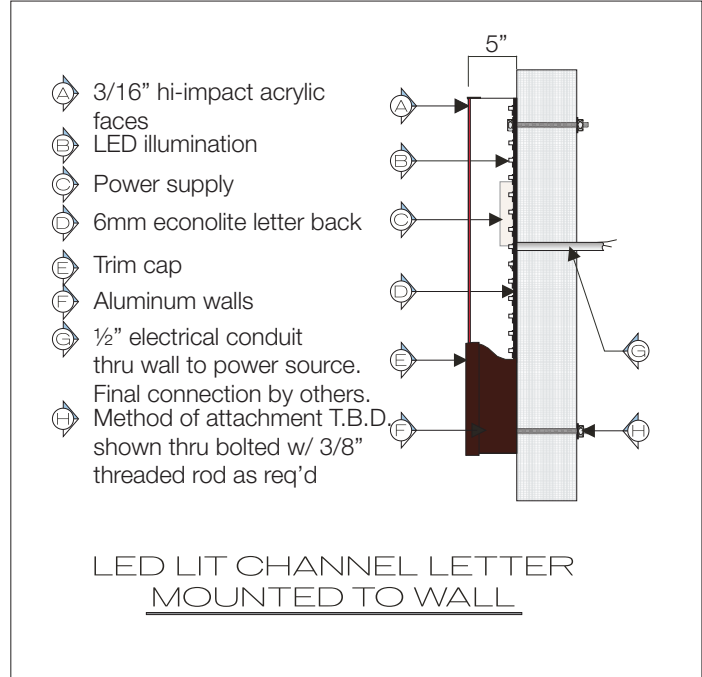
24" CVS/pharmacy Channel Letters

-3/16" acrylic faces - #2793 red (typical)  
-5" deep .040 aluminum returns alliance pre-painted (dark bronze)  
-3/4" bronze trim cap  
-U.L. Listed  
-(1) 20 amp circuit

-LED Specifications: Tetra MAX Red LED Strip  
-LED Module: GERDMXS6  
-# Of Modules#: 166  
-# Of Feet: 56  
-Power Supply: GEPS12-60  
-Quantity: 2  
-Total amps: 1.7

Colors & Materials

- Red acrylic #2793
- Dark Bronze trim cap & returns



LED LIT CHANNEL LETTER  
MOUNTED TO WALL



125 Samuel Barnet Boulevard  
New Bedford, MA 02745  
800.544.0961 | poyantsigns.com



Main Street  
Town, State

Project: 12264  
CVS/pharmacy

Sales: Gary McCoy  
Date: 06.23.09  
Designer: LB

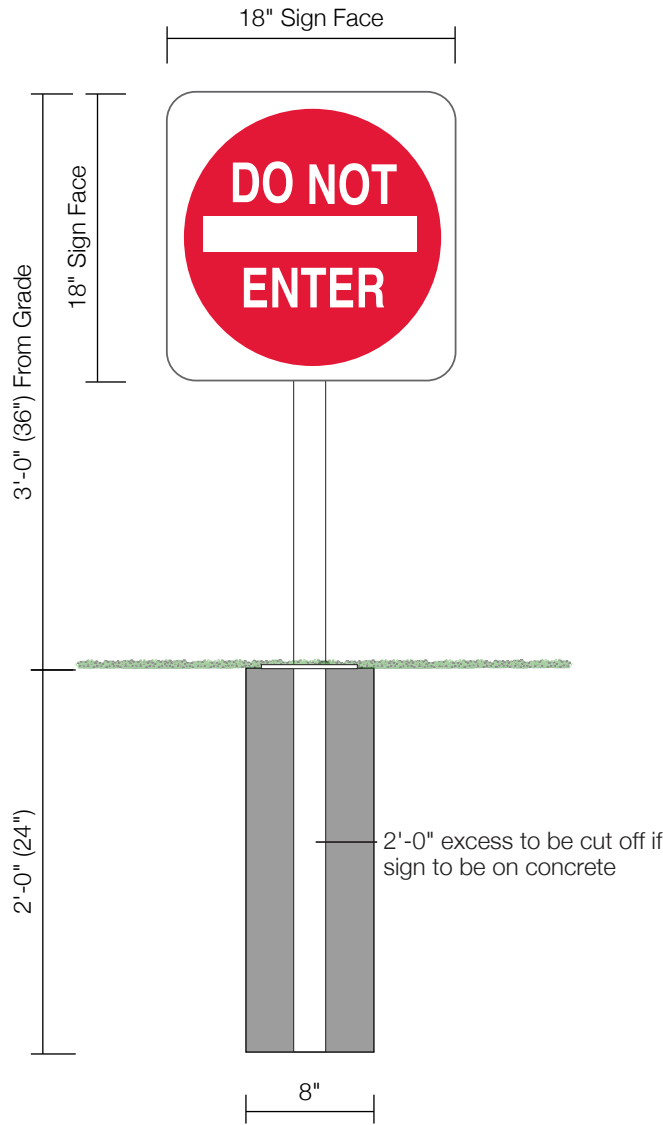
Note:  
This is an original unpublished drawing created by Poyant Signs, Inc. It is submitted for your personal use in connection with a project being planned for you by Poyant Signs, Inc. It is not to be shown to anyone outside your organization, nor is it to be reproduced, copied or exhibited in any fashion until transferred.

Revisions:

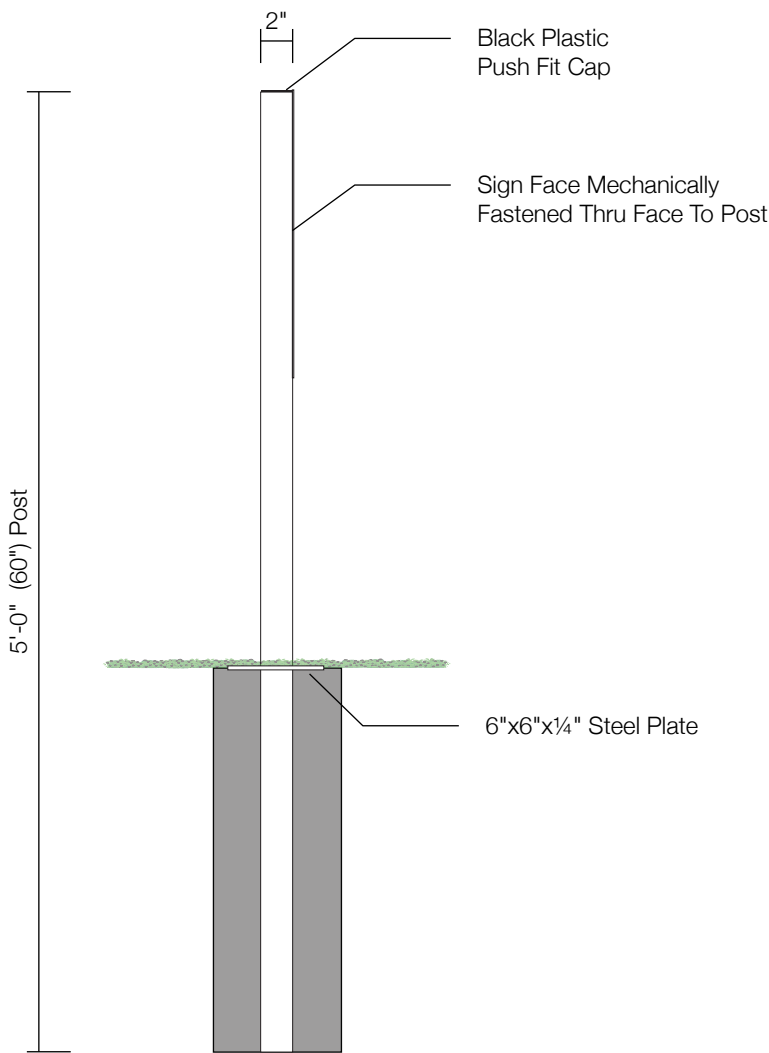

Approved By:

Date:

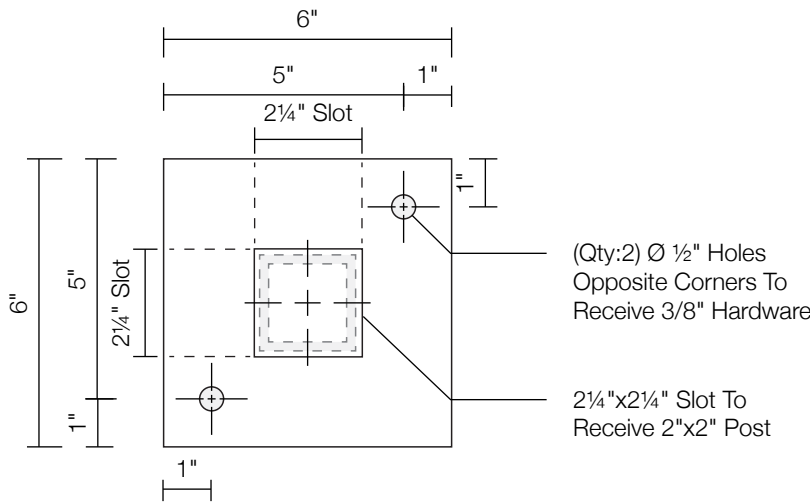
24" Channel Letters



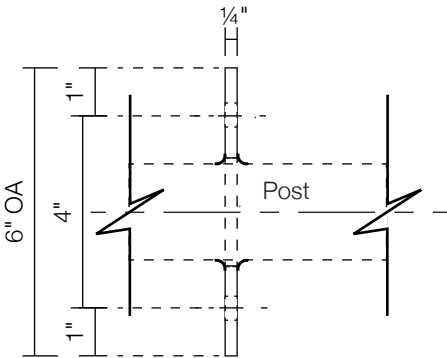
A Sign Elevation - Front View  
Scale: 1"=1'-0"



B Sign Elevation - Side View  
Scale: 1"=1'-0"



C Plate Detail - Front View  
Scale: 3"=1'-0"



D Plate Detail - Side View  
Scale: 3"=1'-0"

Specifications

Qty = 1 2.25 Sq Ft

Single Faced Non Illuminated Regulatory Sign

- Single sided stock regulatory sign face w/ engineer grade reflective white background & red graphics
- Sign face mechanically fastened thru face to post
- (Qty:1) 2" x 2" x 5'-0" steel post to be painted colonial white
- Black plastic push fit cap for top of post
- (Qty:1) 6" x 6" x 1/4" steel plate welded 3'-0" down from top of pole painted white

\* If installation is on concrete: cut pole from bottom of plate down & mount plate to concrete

\* If installation is to be direct burial: bury 2'-0" below grade To be in direct burial foundation

- Foundation to be 2'-0"D x 8" Dia. with (Qty:1) Bags of pea stone per hole

\*Foundation varies per location

Colors & Materials

Paint

Colonial White; Satin Finish

Drawn By: L. Rogers

Revisions:

3.23.17 LR Rev changed to pea stone fill T2

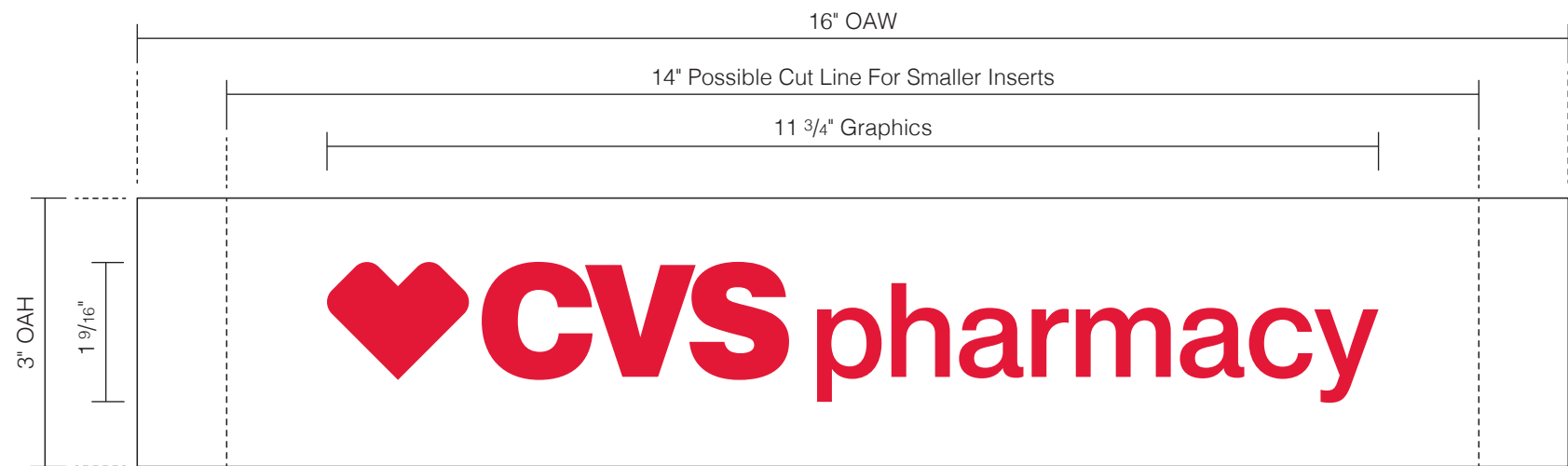


This sign is intended to be installed in accordance with the requirements of Article 600 of the National Electrical Code and / or other applicable local codes. This includes proper grounding and bonding of the sign.

Approved By:

Date:

Do Not Enter\_  
Regulatory Sign



A Sign Elevation - Front View  
Scale: 6"=1'-0"

Specifications  
Qty = 60 Sq Ft Varies

Replacement Single Face Header Insert For Rebrand Hours Plaques

\*Remove & Dispose Of Existing Header Inserts w/ Old Logo

- .080 Styrene direct print inserts
- Red to match PMS 186c
- Some locations may need to be trimmed in length to fit
- Install new insert in top section of existing hours plaques

Colors & Materials

Direct Print

 (Qty:60) 3"H x 16"W .080 Styrene Direct Printed w/ CVS Red; PMS 186C Trimmed To Finished Size

**Poyant**  
Building Your Brand

125 Samuel Barnet Boulevard  
New Bedford, MA 02745  
800.544.0961 | poyantsigns.com



Corporate

Project: 14840  
CVS Pharmacy

Sales: Gary McCoy  
Date: 05.07.18  
Designer: LR

Note:  
This is an original unpublished drawing created by Poyant Signs, Inc. It is submitted for your personal use in connection with a project being planned for you by Poyant Signs, Inc. It is not to be shown to anyone outside your organization, nor is it to be reproduced, copied or exhibited in any fashion until transferred.

Revisions:

5.7.18 LR T2



This sign is intended to be installed in accordance with the requirements of Article 600 of the National Electrical Code and / or other applicable local codes. This includes proper grounding and bonding of the sign.

Approved By:  
**Approved for Construction**

*Martin B Higgins III*  
Date:

Pylon Sign

Option A

Sign Type 14840.1A

1A.1



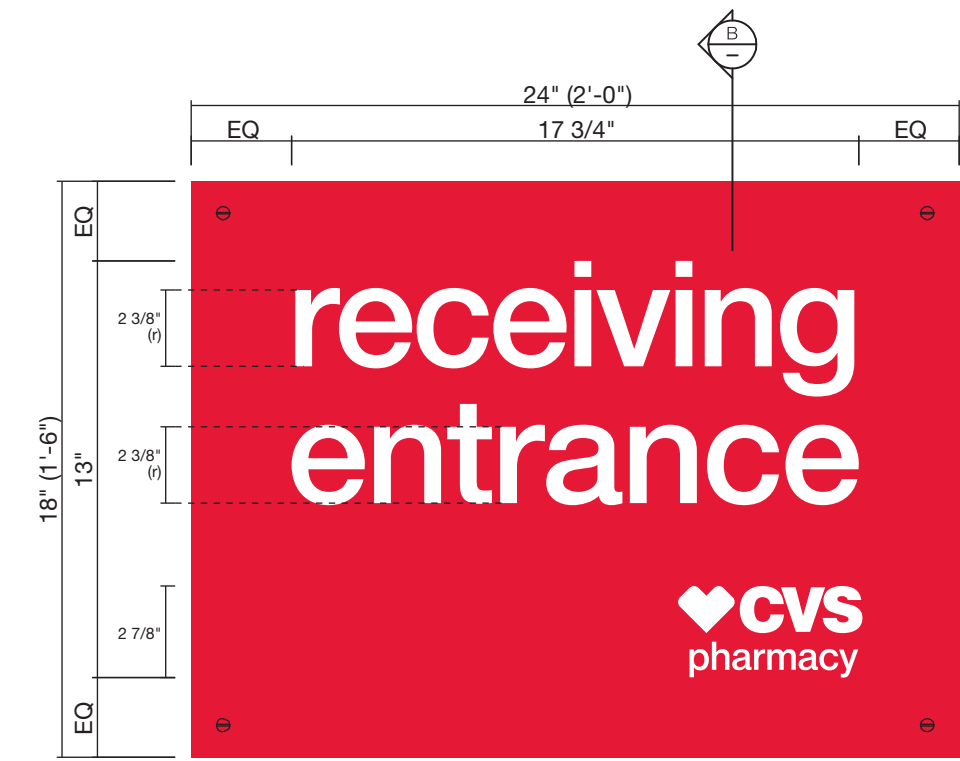
B Photo Comp - Existing Example  
Not To Scale



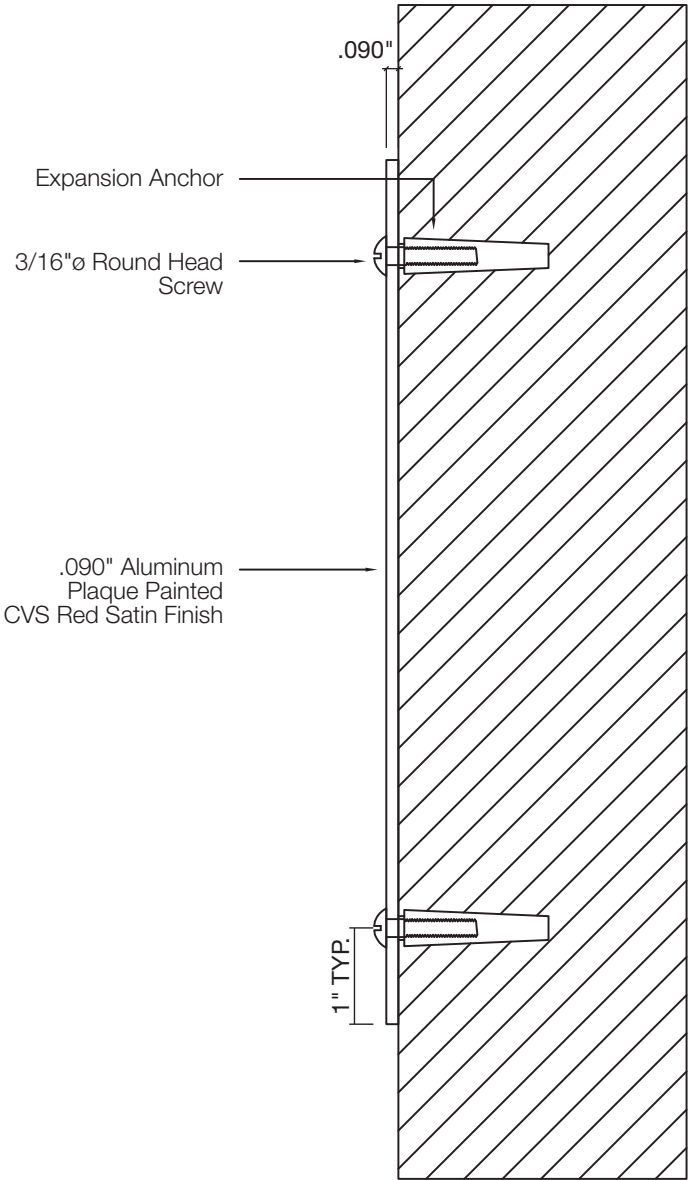
C Photo Comp - Proposed Example  
Not To Scale



# Receiving Entrance Plaque



A Sign Elevation - Front View  
Scale: 2"=1'-0"



B Section

## Specifications

S/F Non-Illuminated Door Plaque  
(3.0 SQ.FT.)

Background  
- 0.090" Aluminum Plaque Painted CVS Red

Graphics  
- White Vinyl Graphics

Hardware  
- 3/16"ø Round Head Screw  
- Expansion Anchor

## Colors & Materials

- Paint to Match PMS #186 (Satin Finish)
- White Opaque Vinyl



Drawn By: N. Pazdan

File Name: Receiving Entrance Plaque

## Revisions:



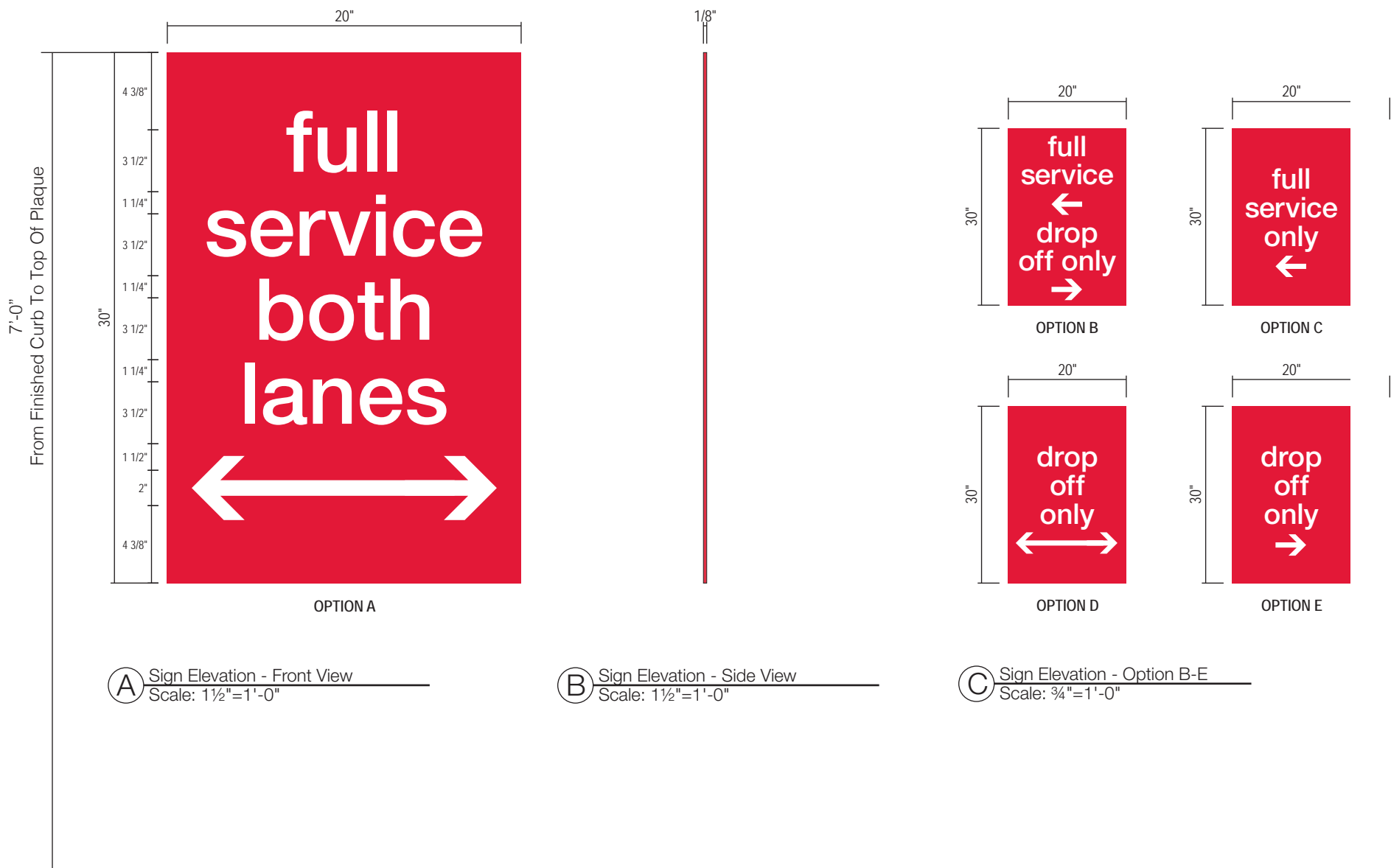

This sign is intended to be installed in accordance with the requirements of Article 600 of the National Electrical Code and / or other applicable local codes. This includes proper grounding and bonding of the sign.

Approved By:

Date:

Receiving Entrance Plaque

# Drive-Thru Regulatory Plaques



## Specifications

S/F Non-Illuminated Full Service Plaque  
(4.17 SQ.FT.)

Background  
- 1/8" Aluminum Plaque Painted CVS Cardinal Red

Graphics  
- White Reflective Vinyl Graphics

Install  
- Mount to drive-thru canopy columns as required in field

## Colors & Materials

CVS Cardinal Red (Satin Finish)

White Reflective; 3M 680-10



125 Samuel Barnet Boulevard  
New Bedford, MA 02745  
800.544.0961 | poyantsigns.com



Corporate

Project: 14866  
Fairhaven Commons

Sales: Gary McCoy  
Date: 05.16.18  
Designer: LR

Note:  
This is an original unpublished drawing created by Poyant Signs, Inc. It is submitted for your personal use in connection with a project being planned for you by Poyant Signs, Inc. It is not to be shown to anyone outside your organization, nor is it to be reproduced, copied or exhibited in any fashion until transferred.

## Revisions:

5.16.18 LR T2



This sign is intended to be installed in accordance with the requirements of Article 600 of the National Electrical Code and / or other applicable local codes. This includes proper grounding and bonding of the sign.

Approved By:  
**Approved for Construction**  
*Martin B Higgins III*  
Date:

Drive-Thru Regulatory Plaque  
Standard

Option A



03.26.2019 301 of 347



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CVS/pharmacy

RECEIVING  
ENTRANCE

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NOTES

1. HANDICAP ACCESSIBLE PARKING SPACES SHALL CONFORM TO THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT (A.D.A.) OF JANUARY, 1992 AND ANY AMENDMENTS THERETO AND THE COMMONWEALTH OF MASSACHUSETTS ARCHITECTURAL ACCESS BOARD, 521 CMR, WHICHEVER IS MORE STRINGENT.
2. ACCESSIBLE PARKING SPACES DESIGNATED WITH A "V" SHALL BE SIGNED AS "VAN ACCESSIBLE" PER A.D.A. 4.1.2.5B.
3. ALL NEW CURBING INSTALLED ON MASSACHUSETTS AVENUE SHALL BE VERTICAL GRANITE CURB (VGC), TYPE VA3 PER MHD SPECIFICATION M 9.04.1
4. ALL LIMITS OF PAVEMENT SHALL BE CURBED UNLESS NOTED OR DETAILED OTHERWISE.
5. ALL STANDARD PARKING SPACES SHALL BE 9 FEET IN WIDTH BY 18 FEET IN LENGTH, WITH 24 FOOT AISLES UNLESS NOTED OR DETAILED OTHERWISE.
6. ALL PAVEMENT STRIPING SHALL BE PAINTED WITH 2 COATS OF PAINT.
7. CONTRACTOR SHALL REFER TO BUILDING PLANS FOR CONSTRUCTION OF COMPACTOR, DUMPSTER AND DRIVE THRU AREAS.
8. CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATIONS OF VESTIBULE, EXIT PORCHES, BUILDING SIDEWALK, RAMPS, LOADING AREA, BUILDING DIMENSIONS, BUILDING UTILITIES, ENTRANCE LOCATIONS, COMPACTOR AREA, ETC.
9. ALL SIDEWALK RAMPS SHALL BE CONSTRUCTED OF CONCRETE AND COMPLY WITH A.D.A. REQUIREMENTS. PRIOR TO INSTALLING HANDICAP RAMPS, CONTRACTOR SHALL SUBMIT A DETAILED DESIGN OF EACH HANDICAP RAMP FOR THE ENGINEER'S REVIEW.

PLAN LEGEND

EXISTING	PROPOSED	DESCRIPTION
		CURBED LANDSCAPED ISLAND
		HANDICAP PARKING STALL
		PARKING COUNT
		HANDICAP SYMBOL
		MOUNTED TRAFFIC SIGN
		WHEELCHAIR RAMP
		WHEELCHAIR RAMP
		SIGNALIZED INTERSECTION
		EXTRUDED CONG. CURB TYPE
		VERTICAL GRANITE CURB TYPE
		BOLLARD
		STOP LINE
		SOLID WHITE CHANNELIZING LINE
		SOLID YELLOW CHANNELIZING LINE
		BROKEN YELLOW CENTERLINE
		DOUBLE YELLOW CENTERLINE
		SOLID YELLOW CENTERLINE
		SOLID YELLOW EDGE LINE
		SOLID WHITE EDGE LINE
		SOLID WHITE LANE LINE
		TRANSFORMER PAD
		CONCRETE PAVEMENT
		CHAIN LINK FENCE
		CONCRETE WHEEL STOP
		GUARDRAIL
		VERTICAL GRANITE CURB
		WOODEN STOCKADE FENCE

ZONING DISTRICT: VEHICULAR ORIENTED BUSINESS DISTRICT (B4)  
RETAIL: SPECIAL PERMIT FROM ARLINGTON REDEVELOPMENT BOARD (ARB) FOR GENERAL RETAIL USE.

CVS Zoning Summary Chart

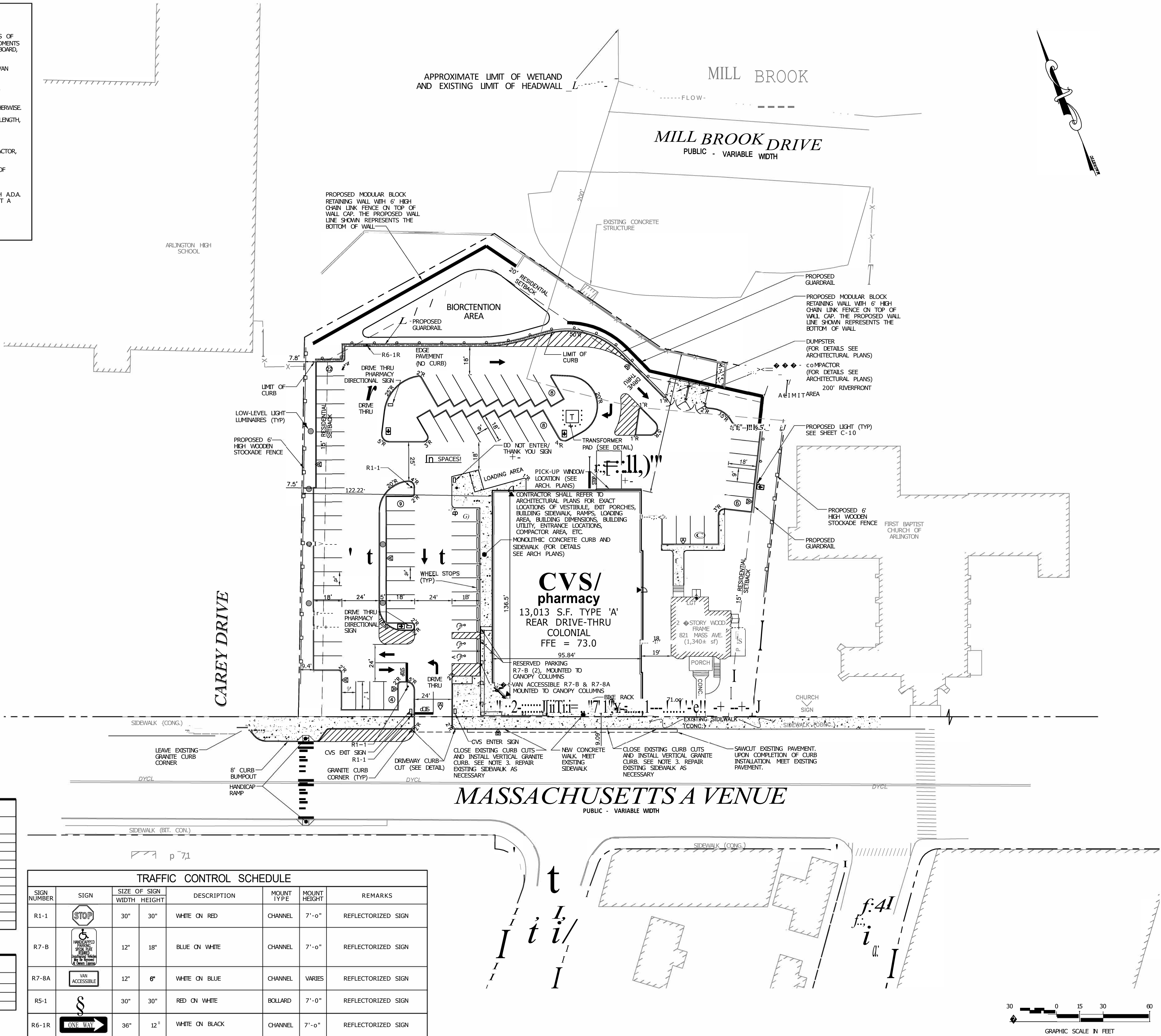
Zoning Regulation Requirements	Required	Proposed
MINIMUM LOT SIZE	NONE	1.83 AC
FRONT SETBACK	NONE	0 FT
SIDE SETBACK	NONE	71 FT
REAR SETBACK	20 FT	92 FT
MAX. BUILDING HEIGHT	35 FT	32 FT
MIN. LOT FRONTAGE	50 FT	291.5 FT
F.A.R.	1.0	0.18
MIN LANDSCAPE AREA	10%	32.5%
MIN INTERIOR LANDSCAPE	8%	8.6%
RESIDENTIAL BUFFER	15 FT	7.5 FT
BUILDING SEPARATION	13 FT	18 FT

Parking Summary Chart

Description	Required	Proposed
STANDARD SPACES (9'X18')	42	69
NO. OF ACCESSIBLE SPACES*	2	3
TOTAL SPACES*	44	72

\*4. SPACE PER 300 SF OF GROSS FLOOR AREA  
\*\*ADA REQUIREMENT - MINIMUM NUMBER OF ACCESSIBLE SPACES FOR A PARKING LOT TOTALING 50 TO 75 SPACES - 3 TOTAL HANDICAP SPACES  
ZONING DISTRICT: VEHICULAR ORIENTED BUSINESS DISTRICT (B4)

TRAFFIC CONTROL SCHEDULE						
SIGN NUMBER	SIGN	SIZE OF SIGN WIDTH HEIGHT	DESCRIPTION	MOUNT TYPE	MOUNT HEIGHT	REMARKS
R1-1		30" 30"	WHITE ON RED	CHANNEL	7'-0"	REFLECTORIZED SIGN
R7-B		12" 18"	BLUE ON WHITE	CHANNEL	7'-0"	REFLECTORIZED SIGN
R7-8A		12" 6"	WHITE ON BLUE	CHANNEL	VARIES	REFLECTORIZED SIGN
R5-1		30" 30"	RED ON WHITE	BOLLARD	7'-0"	REFLECTORIZED SIGN
R6-1R		36" 12"	WHITE ON BLACK	CHANNEL	7'-0"	REFLECTORIZED SIGN



CIVIL ENGINEER:

**Rf O'CONNELL**  
**& ASSOCIATES, INC**  
Civil Engineers & Land Planners

80 MONTVALE AVE., SUITE 201  
STONEHAM, MA 02180  
781-279-0180  
FAX: 781-279-0173

CONSULTANT:

SEAL:

**CVS/**  
**pharmacy**

13,013 TYPE A PROTOTYPE

STORE NUMBER= 75672

837-821 MASSACHUSETTS AVENUE  
ARLINGTON, MA

DEAL TYPE: H

DEVELOPER:

**GBC**

GERSHMAN BROWN CROWLEY INC.

14 BREAKNECK RD  
LINCOLN, RI 02865

REVISIONS:

ISSUED TO ARLINGTON REVIEW BOARD

REVISED PER ARB COMMENTS 03/23/2009

DRAWING BY: AKC/KPM

DATE: 03/02/2009

JOB NUMBER: 07150

TITLE:

**PARKING & TRAFFIC  
CONTROL PLAN**

SHEET NUMBER:

**C-2**

COMMENTS:



RECEIVED

TOWN CLERK'S OFFICE  
ARLINGTON, MA 02174  
2009 APR 20 PM 1:10

## ARLINGTON REDEVELOPMENT BOARD

Arlington, Massachusetts  
Middlesex, ss

DOCKET NO. 3348

REQUEST FOR SPECIAL PERMIT  
Subject to  
ENVIRONMENTAL DESIGN REVIEW

Applicant CVS

Date of Hearings October 20, 2008, November 17, 2008,  
December 22, 2008, February 23, 2009,  
March 9, 2009, March 30, 2009,  
April 6, 2009, April 13, 2009

Date of Decision April 13, 2009

Date of Filing April 28, 2009

### Members

Approved

Andrew P. Neri chair  
[Signature]  
Edward Tsai  
[Signature]  
[Signature]

Opposed

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_



## TOWN OF ARLINGTON

MASSACHUSETTS 02476

781 - 316 - 3090

### DEPARTMENT OF PLANNING and COMMUNITY DEVELOPMENT

#### OPINION OF THE BOARD

This application by G. B. New England 2, LLC seeks a special permit to construct a CVS drugstore at the subject address. The property has been the site for an automobile dealer and a small office building (formerly a residence) for many years. The applicant originally proposed to construct a 12,900-square-foot retail store on a part of the site that did not include the former residence at 821 Mass. Ave., known as the Atwood House. Prior to the public hearing, the applicant notified the Town that it wished to modify its proposal. It had arranged to include the property on which the Atwood house is located. It now proposed to demolish both buildings, construct the same CVS drug store, and construct an automated bank teller machine in a freestanding, 70-square-foot building. The applicant requested more time to modify its application. Accordingly, the hearing scheduled for October 20, 2008 was opened and immediately continued to November 17, 2008 with no discussion of the project. The hearing was advertised in the Arlington Advocate on October 2 and October 9, 2008.

When it was questioned if the drive-thru pharmacy could be permitted, the Inspector of Buildings determined that the proposed drive-thru for the pharmacy could be permitted as use number 8.17, which requires a special permit. The public hearing for that special permit use was advertised in the Arlington Advocate on December 4 and December 11, 2008, and scheduled for December 22, 2008 which coincided with the continuation date (from November 17, 2008) for the original permit application. Subsequently, hearings have been held for all proposed uses on January 26, 2009, February 23, 2009, March 9, 2009, Mar 30, 2009, April 6, 2009, and April 13, 2009.

The proposal has changed in response to the discussion at these hearings. The Atwood House will not be demolished, but will remain. There has been a great deal of discussion about using the house as a multi-family residence. However, at this time, there is no specific proposal for the use of the Atwood House. The applicant has indicated that it wishes to complete the permitting of the CVS drugstore, and will return to modify the special permit when the use of the Atwood House is determined. The proposed site plan includes the Atwood House, parking spaces that are dedicated to it, and space for an addition to the rear of the structure.

#### FINDINGS OF FACT

**Section 10.11a-1** The uses requested are listed in the Table of Use Regulations as a Special Permit in the district for which application is made or is so designated elsewhere in this Bylaw.

The applicant seeks a special permit to operate a retail store having more than 3,000 square feet of gross floor area. The use, number 6.16 in the Table of Use Regulations (Section 5.04 of the Zoning Bylaw), is a special permit use in the B4 zoning district. The fact that the proposed development also requires a building permit and is located on Massachusetts Avenue means that the special permit is subject to environmental design review (Section 11.06 of the Zoning Bylaw). The applicant also seeks special permits for signs under Section 7.09, and for parking and loading space standards under Section 8.12 of the Zoning Bylaw. The proposal includes two drive-thru pharmacy

windows, which the Inspector of Buildings has said can be permitted special permit under accessory use number 8.17. The Board finds that the proposal meets this standard.

**Section 10.11a-2      The requested use is essential or desirable to the public convenience or welfare.**

The town is now served by four large drugstores (two of which are CVS, one of which does not have a pharmacy) and two additional pharmacies. The proposed use of the site will establish the fifth large drugstore in Arlington (and the third CVS store) and the sixth pharmacy. The proposed store will be the only pharmacy with a drive-thru. Public input at the public hearing has been mixed, but some clearly want a convenient, large drugstore with a drive-thru pharmacy. The Board finds that the proposed use is desirable to the public convenience or welfare.

**Section 10.11a-3      The requested use will not create undue traffic congestion, or unduly impair pedestrian safety.**

The applicant submitted a traffic impact report, and then modified it several times to include the impact of the project on pedestrians, the impacts when the abutting high school is in session, the re-positioning of the retail store, and the preservation of the Atwood House. At each step of the way, the Board's traffic consultant has reviewed the reports. The Town's Transportation Advisory Committee has also reviewed the traffic impact studies and made recommendations to the Board.

The following is the sequence of documents regarding traffic impacts:

1. Traffic Impact Study by GEOD (for CVS), August 18, 2008
2. Traffic Impact Study by GEOD (for CVS), November 17, 2008 – This study reflected an altered site plan because CVS had arranged to control more of the property and proposed to demolish the Atwood House and add more parking and an ATM on the site.
3. Memo from BSC Group (ARB's consultant), December 4, 2008 – This memo asked for clarification of parts of the proposal and asked for some technical corrections.
4. Revised Traffic Impact Study by GEOD (for CVS), January 19, 2009 – This study responded to comments from BSC and those made at the December 22 hearing. It also reflected a second change to the site plan: the Atwood house is to remain and put to a new use (as yet undetermined). It also recommended a new crosswalk near Carey Drive and improvements that could be made to the Jason and Mill Streets intersection.
5. Comments by Jeff Maxtutis (TAC), January 19, 2009 – The comments asked for minor changes in the impact analysis.
6. Memo from BSC Group (ARB's consultant), January 22, 2009 – The memo expressed general agreement with the responses in the January 19 report and suggested some refinements.
7. Memo from GEOD regarding pedestrian movements, February 4, 2009 – This report provided more detail about pedestrian movements and studied alternative crosswalk locations.
8. Comments by Jeff Maxtutis (TAC), February 6, 2009 – These comments evaluated the proposed improvements to the Jason and Mill Streets intersection and the proposed crosswalk. It also expressed concern about the site entrance and exit being close to Carey Drive.
9. Memo from GEOD summarizing comments, February 20, 2009 – This memo summarized the recent months' studies, comments, and responses.
10. Memo from TAC, February 26, 2009 – This memo indicated general satisfaction with the impact studies and the changes made to the project. It listed items it still thought were



unresolved: 1) the proximity of the access to Carey Drive, 2) the need for bump-outs at Bartlett Street, 3) the need for bump-outs at the new crosswalk near Carey Drive, and 4) a commitment to make improvements to the Jason and Mill Streets intersection.

11. Email from Chris Emelius (GEOD), March 4, 2009 – Clarified distance from Carey Drive to proposed site entrance.
12. Local CVS traffic counts by Ed Starr (TAC), March 5, 2009 – Counts of pedestrians and vehicles were made at Walgreens in East Arlington for comparison purposes.
13. Memo from BSC Group. March 5, 2009 – This memo concurred with the new crosswalk location and recommended bump-outs. It also made a number of recommendations regarding circulation on the site (parking, drive-thru, delivery).
14. Local traffic counts (various) from Ed Starr (TAC), March 9, 2009 – Additional local counts for comparison purposes.
15. Hours of operation, local drug stores, by E. Carr-Jones (TAC), March 10, 2009 – Hours of operation of local drug stores for comparison purposes.
16. Report on meeting with TAC by Bruce Fitzsimmons (ARB), March 12, 2009 – TAC was pleased with bump-outs, thought \$5,000 offer to mitigate Jason and Mill Streets intersection was too low, offered compromise on site entrance location, and expressed concern over the trip generation numbers.
17. Memo from BSC Group. March 20, 2009 – This essentially endorsed the TAC comments of March 12.
18. Memo from TAC, March 23, 2009 – Reiteration of concerns and proposal of \$50,000 mitigation fund for unforeseen traffic impacts.
19. Memo from GEOD, March 26, 2009 – This is a discussion of TAC and BSC concerns, and acceptance of the crosswalk bump-outs, and of the site entrance drive 113 feet from Carey Drive.
20. Memo from TAC, March 30, 2009 – This is a defense of TAC's March 23rd memo.
21. Email from Sam Offei-Addo (BSC Group), April 2, 2009 – This listed recommended improvements to signage and traffic lines on the site and at one of the bump-outs.
22. Questions on the Permitting Process for the Proposed CVS Pharmacy at 837 Massachusetts Avenue, Arlington Citizens for Responsible Development, April 6, 2009 by David Wright – This paper expressed concern about the intersection at Jason and Mill Streets, traffic congestion at the high school, the validity of pedestrian counts, traffic generation figures, and traffic impacts on neighboring streets.
23. Traffic and Safety issues Relating to the CVS Special Permit Application, Arlington Citizens for Responsible Development, April 6, 2009 by Dorothy Nash Webber – This paper made comparison of the proposal to the Osco proposal, which was denied some ten years earlier, and reiterated the concerns made in David Wright's paper, above.

The Board considered the traffic safety issues very carefully and asked its traffic consultant and TAC to do likewise. The trip generation numbers were discussed in great detail, with general agreement on the PM numbers and the feeling that the AM numbers may be low. The effects of the traffic on intersection performance were assessed using the PM numbers which corresponded to the TAC's AM counts. The Board therefore felt it had adequate indication of the impacts. Because of the potential impacts at the site entrance, the Board felt compelled to create the ability through an escrow fund to mitigate unexpected vehicle activity near the site. Should mitigation near the site not be deemed necessary, the escrow may be used at the Jason and Mill Streets intersection, which is expected to require mitigation regardless of whether or not the CVS is built.

As a result of the discussions about pedestrian safety and traffic congestion, the applicant has moved the driveway away from Carey Drive to lessen the impact on pedestrians and vehicles entering the high school, and will install a new crosswalk between Carey Drive and the CVS driveway, and will install crosswalk bump-outs on Mass. Ave. at that crosswalk and at Bartlett Avenue. The bump-outs will shorten the crossing distance, and help prevent illegal parking in the crosswalk. The applicant has agreed to contribute funds to help mitigate the impact of increased traffic along Mass. Ave., including at the Jason Street and Mill Street intersection. Based on the data and reports submitted by the applicant's consultant, as revised, and the materials and comments submitted by the Board's consultant and TAC, the mitigation measures agreed to by the applicant as part of this special permit, and the funding of future mitigation measures as required, the Board finds that this standard is met.

**Section 10.11a-4      The requested use will not overload any public water, drainage or sewer system or any other municipal system to such an extent that the requested use or any developed use in the immediate area or in any other area of the Town will be unduly subjected to hazards affecting health, safety, or the general welfare.**

The impact of the proposed development on public water and sewer will be minimal, but the Town Engineer has given the applicant instructions for making such connections. The applicant has submitted a very detailed stormwater management plan. The stormwater management plan has been revised to conform to the significant changes that have been made in the site plan, but the system remains essentially the same, with most of the stormwater filtered through a large rain garden at the rear of the site. The Board finds that the proposal meets this standard.

**Section 10.11a-5      Any special regulations for the use, set forth in Article 11 are fulfilled.**  
The environmental design review standards of Section 11.06 are evaluated below.

**EDR-1 Preservation of Landscape: The landscape shall be preserved in its natural state insofar as practicable, by minimizing tree and soil removal and any grade changes shall be in keeping with the general appearance of neighboring developed areas.**

The current site is covered almost entirely by building or paving. There is some lawn area in front and to the right of the Atwood House, and minimal other landscaping. With the Atwood House remaining on the site, it is possible to preserve a 22-inch-diameter pine tree in its front yard. The northern side of the lot slopes steeply down, and is covered with scrub growth, including trees. The proposed development will retain most of the treed area to the north, and introduce significantly more landscaping on the remaining three sides, as well as some landscaped areas within the parking lot. Besides that mentioned above, there is no existing landscaping to be preserved; the site is either paved or covered by building. The proposed plan will replace some of the impermeable surface with landscape, and the total landscaped area exceeds the amount required by the Zoning Bylaw. The Board finds that the proposal meets this standard.

**EDR-2 Relation of the Building to the Environment: Proposed development shall be related harmoniously to the terrain and to the use, scale and architecture of the existing buildings in the vicinity that have functional or visible relationship to the proposed buildings. The Arlington Redevelopment Board may require a modification in massing so as to reduce the effect of shadows on the abutting property in an R-1 or R-2 district or on public open space.**

The current proposal is much improved from the original application. The proposed store building has been moved up to the front of the lot, consistent with business uses along Mass. Ave. The Atwood House is to remain. It is important that the current design retains the Atwood House in its

current location on the site, and accommodates the possible future expansion at the rear of the structure. The Atwood House, and the current design of the CVS building itself, present an appropriate streetscape for Mass. Ave. in this area. The Board finds that the proposal meets this standard.

**EDR-3 Open Space:** All open space (landscaped and usable) shall be so designed as to add to the visual amenities of the vicinity by maximizing its visibility for persons passing by the site or overlooking it from nearby properties. The location and configuration of usable open space shall be so designed as to encourage social interaction, maximize its utility and facilitate maintenance.

The open space provided on the site is appropriately and attractively landscaped, and exceeds the amount of landscaped space required by the Zoning Bylaw. The changes to the parking lot configuration result in the proposal meeting the required open space within the parking lot. In addition, the applicant has agreed to provide landscaping between the setback at the front of the new building and the sidewalk. The spaces will be attractively planted and placed to provide a pleasant view or screening as needed. The Board finds that the proposal meets this standard.

**EDR-4 Circulation:** With respect to vehicular and pedestrian and bicycle circulation, including entrances, ramps, walkways, drives, and parking, special attention shall be given to location and number of access points to the public streets (especially in relation to existing traffic controls and mass transit facilities), width of interior drives and access points, general interior circulation, separation of pedestrian and vehicular traffic, access to community facilities, and arrangement of vehicle parking and bicycle parking areas, including bicycle parking spaces required by Section 8.13 that are safe and convenient and, insofar as practicable, do not detract from the use and enjoyment of proposed buildings and structures, and the neighboring properties.

The traffic circulation on the site is designed to accommodate large delivery trucks and the pharmacy drive-thru, and to provide parking for customers. The evolution of the site plan is such that the current proposal meets the standard. Some minor changes to the directional signage have been suggested. There is bike parking provided near the store entrance, and extensive changes involving a crosswalk; and curb bump-outs are proposed near the vehicle entrance to the site, helping to protect pedestrian traffic.

**EDR-5 Surface Water Drainage:** Special attention shall be given to proper site surface drainage so that removal of surface waters will not adversely affect neighboring properties or the public storm drainage system. Available Best Management Practices for the site should be employed, and include site planning to minimize impervious surface and reduce clearing and re-grading. Best Management Practices may include erosion control and stormwater treatment by means of swales, filters, plantings, roof gardens, native vegetation, and leaching catchbasins. Stormwater should be treated at least minimally on the development site; that which cannot be handled on site shall be removed from all roofs, canopies, paved and pooling areas and carried away in an underground drainage system. Surface water in all paved areas shall be collected in intervals so that it will not obstruct the flow of vehicular or pedestrian traffic and will not create puddles in the paved areas.

In accordance with Section 10.11.b, the Board may require from any applicant, after consultation with the Director of Public Works, security satisfactory to the Board to insure the maintenance of all stormwater facilities, such as catch basins, leaching catch basins,

detention basins, swales, etc., within the site. The Board may use funds provided by such security to conduct maintenance that the applicant fails to do.

**The Board may adjust in its sole discretion the amount and type of financial security such that it is satisfied that the amount is sufficient to provide for any future maintenance needs.** The applicant has submitted a very detailed stormwater management plan, which was revised to match the current plan. It has been reviewed by the Town Engineer, and the applicant has responded to the comments. The storm drain system discharges storm flow in the same location as the flow is directed today. The permeable surface on the site has been reduced, and the system includes an underground detention and infiltration chamber and a rain garden to reduce, clean, and slow the flow of storm water. The Board finds that the proposal meets this standard.

**EDR-6 Utilities Service: Electric, telephone, cable, TV, and other such lines and equipment shall be underground. The proposed method of sanitary sewage disposal and solid waste disposal from all buildings shall be indicated.**

The plans indicate adequate underground utility connections; they also show the location of an electric transformer in a landscaped island in the parking lot. The Town Engineer made some modifications to the plans relative to the hook-ups in Mass. Ave. The applicant has moved the transformer location to a less visible location. The Board finds that the proposal meets this standard.

**EDR-7 Advertising Features: The size, location, design, color, texture, lighting, and materials of all permanent signs and outdoor advertising structures or features shall not detract from the use and enjoyment of proposed buildings and structures and the surrounding properties.**

The main signage on the building consists of two wall signs, one facing Mass. Ave., and one facing the parking lot on the west side of the building. The two signs meet the bylaw standards. Several signs are located within the parking lot area to direct traffic. These signs exceed the one-square-foot area that is allowed. The directional signs are helpful and important in helping vehicles navigate a fairly complicated parking lot. The signs are slightly larger than three square feet each, and the Board has determined that the larger size is in the public interest, and is allowed by special permit. Other directional signs are posted on, and identify, the pharmacy drive-thru. These also are larger than one square foot, and the Board has determined that they are allowed by special permit.

**EDR-8 Special Features: Exposed storage areas, exposed machinery installations, service areas, truck loading areas, utility buildings and structures, and similar accessory areas and structures shall be subject to such setbacks, screen plantings, or other screening methods as shall reasonably be required to prevent their being incongruous with the existing or contemplated environment and the surrounding properties.**

The proposal includes two dumpsters at the rear of the parking lot, which are visible from the street. The sides of the dumpster are screened by plantings, and the front is stockade fence. Planting has been sited to the rear of the Atwood House to effectively screen the dumpster area from the street and from the Atwood House. There is a large electrical transformer in a landscaped island in the parking lot. It was moved to a less visible location, and is appropriately screened with vegetation. The proposal locates rooftop HVAC and refrigeration units behind the screen of the slanted roof surfaces; this equipment will not be visible from the ground. The Board finds that the proposal meets this standard.



**EDR-9 Safety:** With respect to personal safety, all open and enclosed spaces shall be designed to facilitate building evacuation and maximize accessibility by fire, police, and other emergency personnel and equipment. Insofar as practicable, all exterior spaces and interior public and semi-public spaces shall be so designed to minimize the fear and probability of personal harm or injury by increasing the potential surveillance by neighboring residents and passersby of any accident or attempted criminal act.

The plan appears to be generally safe, with all accessible spaces open to the public view. The parking lot is well lighted to serve the parking lot users well. The Board requested reduced lighting on the Atwood House side of the proposed building; the plan calls for some light in this area for safety. The source of lighting on the site will not be visible from off the site. The Board finds that the proposal meets this standard.

**EDR-10 Heritage:** With respect to Arlington's heritage, removal or disruption of historic, traditional, or significant uses, structures or architectural elements shall be minimized insofar as practical, whether these exist on the site or on adjacent properties.

The site has no historical structure, and the site has no historical significance. Before it became an auto dealership, there were three or four houses on the site, including the Atwood House, which remains today. The Atwood House is listed as a significant building under Arlington Town Bylaws, as is the Baptist Church next door. The applicant has stated that the Atwood House will be retained on the site, and the proposed plan reflects that. Any addition or modification of the Atwood House would have to respect Town bylaws regarding significant structures. Any modification of the Atwood House will require an amendment of this special permit. The Board finds that the proposal meets this standard.

**EDR-11 Microclimate:** With respect to the localized climatic characteristics of a given area, any development which proposes new structures, new hard surface, ground coverage, or the installation of machinery which emits heat, vapor, or fumes, shall endeavor to minimize, insofar as practicable, any adverse impacts on light, air, and water resources, or on noise and temperature levels of the immediate environment.

The proposal will reduce the amount of impermeable area on the site. The HVAC and refrigeration equipment are located on the roof of the CVS building in a well, behind slanted roofs on all four sides. The site is large relative to the amount of equipment, and the heat, light, vapor, or fumes will not be detectable. The Board finds that the proposal meets this standard.

**EDR-12 Sustainable Building and Site Design:** Projects are encouraged to incorporate best practices related to sustainable sites, water efficiency, energy and atmosphere, materials and resources, and indoor environmental quality. Applicants must submit a current Green Building Council Leadership in Energy and Environmental Design (LEED) checklist, appropriate to the type of development, annotated with narrative description that indicates how the LEED performance objectives will be incorporated into the project.

The applicant has submitted the LEED checklist, and the narrative required by this standard. The plan shows the methods to control soil erosion and sedimentation of storm sewers. The plan increases the amount of permeable surface, and exceeds the Town's open space requirement. The planned lighting is designed to prevent up lighting, and to minimize light trespassing onto abutting properties. Low-flow toilet fixtures will be used, and the performance of the proposed energy systems in the building has been optimized.

The project site has certain characteristics that help make it sustainable. The project uses an already-built site with existing infrastructure, and is accessible to public transportation. The developer has provided a landscaped rain garden and bio-retention area at the rear of the site to help reduce water runoff. The applicant considered permeable paving for portions of the parking lot, but it was determined that the potential pollutant load created by a commercial parking lot made such paving environmentally unfriendly. The Board finds that the proposal meets this standard.

**Section 10.11a-6      The requested use will not impair the integrity or character of the district or adjoining districts, nor be detrimental to the health, morals, or welfare.**

The retail drugstore use located right next to the high school is convenient for students; school officials have endorsed the use. The site is zoned for commercial use, and has been used in that manner for many decades. The retention of the Atwood House and the siting of the CVS building near the sidewalk have improved the presence the development makes on the avenue. The store obviously provides a convenience to consumers, and is more of a community use than the auto dealership that existed there for decades. The Board finds that the proposal meets this standard.

**Section 10.11a-7      The requested use will not, by its addition to a neighborhood, cause an excess of that particular use that could be detrimental to the character of said neighborhood.**

As pointed out above, there are several pharmacies and drug stores in Arlington, but few near the site of the proposed CVS. The nearest is a small pharmacy located in a Stop & Shop supermarket 2/10 of a mile away. The nearest comparable store (a Walgreens east of Arlington Center) is almost 9/10 of a mile away; a Walgreens in Arlington Heights is 1.5 miles away; and the CVS in East Arlington is 1.6 miles away. In addition, the proposed development improves upon the character of the neighborhood by replacing a closed auto dealership. The building design has been changed to be much more in keeping with the appearance of the neighborhood. The site is appropriate for retail use. The Board finds that the proposal meets this standard.

## **DECISION**

The Board finds that the proposal is an appropriate re-use of the property, and grants the following special permits, subject to the following general and special conditions:

- Uses 6.16 and 8.17 from the Table of Use Regulations (Sect. 5.04 of the Zoning Bylaw);
- special permit for signs (Sect. 7.09 of the Zoning Bylaw); and,
- special permit for parking (Sect. 8.12 of the Zoning Bylaw).

### **General Conditions**

1. The final plans and specifications for the site; including all buildings, signs, exterior lighting, and landscaping, shall be subject to the approval of the Arlington Redevelopment Board. The Board shall maintain its jurisdiction over plans and specifications by approving them at 50% and 100% of completion.

At the time of submission of the 50% drawings, the applicant shall submit for approval samples of exterior materials proposed for the building, and the specifics of the location, type, and noise levels of all HVAC and refrigeration machinery.

Final plans and specifications shall include complete information concerning colors, materials, lighting, and other features that comprise the details of the final design. The applicant shall provide a statement from the Town Engineer that all proposed utility services have adequate capacity to serve the development.

2. The final plans and specifications approved by the Board for this permit shall be the final plans and specifications submitted to the Building Inspector of the Town of Arlington in connection with the application for building permits. There shall be no substantial or material deviation during construction from the approved plans and specifications without the express written approval of the Arlington Redevelopment Board.

3. No building permit shall be issued until the Board has received evidence that the special permit has been recorded at the registry of deeds.

4. The Board maintains continuing jurisdiction over this permit, and may, after a duly advertised public hearing, attach other conditions, including, but not limited to, restricting the store opening hours, or it may modify these conditions as it deems reasonably appropriate to protect the public interest and welfare. Such modifications shall not require the applicant to modify the size or dimensions of the retail building shown on the approved plan, nor restrict the opening hour to any time later than 8:00 AM.

5. Snow removal from all parts of the site, as well as from any abutting public sidewalks, shall be the responsibility of the owner or occupant, and shall be accomplished in accordance with the Town bylaws.

6. All exterior trash and storage areas on the property, if any, shall be properly screened and maintained in accordance with Title V, Article 9, of the Bylaws of the Town of Arlington.

7. Trash shall be picked up only between the hours of 7:00 a.m. and 6:00 p.m., Monday through Friday.

8. No final or permanent Certificate of Occupancy shall issue on this project until the project is completed in its final form, and approved by the Redevelopment Board as being in compliance with the final plans and specifications, including the landscape plan.

9. The Building Inspector is hereby notified that he is to monitor the site, and should proceed with appropriate enforcement procedures at any time he determines that violations are present. The Inspector of Buildings shall proceed under Section 10.09 of the Zoning Bylaw, pursuant to the provisions of MGL c. 40, s. 21D, and institute non-criminal complaints. If necessary, the Inspector of Buildings may institute appropriate criminal action also, in accordance with Section 10.09.

#### **Special Conditions**

1. All utilities serving or traversing the site (including electric, telephone, cable, and other such lines and equipment) shall be underground.

2. Upon installation of landscaping materials and other site improvements, the applicant shall remain responsible for such materials and improvement, and shall replace and repair such as necessary, to remain in compliance with the approved site plan.

3. All utility work off site in public rights-of-way of the Town of Arlington shall be undertaken in accordance with the provisions of the Town bylaws.
4. Upon the issuance of the building permit, the applicant shall file with the Building Inspector and the Department of Community Safety the names and telephone numbers of contact personnel who may be reached 24 hours each day during the construction period.
5. The Atwood House shall remain at its present location on the site, and reasonable and diligent efforts shall be used to maintain its present condition to prevent any damage from the elements or otherwise, until it is redeveloped. It is acknowledged that ten parking spaces behind the Atwood House are reserved for its use. It is further acknowledged that the plan of the site leaves space behind the Atwood House to accommodate a possible future expansion of the structure, and that no use of that portion of the site will preclude such an expansion. Redevelopment of the house will require the amendment of this special permit, regardless of whether the proposed use of the structure is allowed by right or by special permit (as such are listed in the Arlington Zoning Bylaw). No requests to move or demolish the house by amending this special permit will be made within 24 months of the date of issuance of this permit.
6. The applicant shall install bump-outs and thermo-plastic crosswalks on Mass. Ave. at Carey Drive and at Bartlett Avenue. Bump-outs shall be installed on both sides of Mass. Ave. The design and construction of the bump-outs and crosswalks shall be approved by the Town Engineer, and shall take into account drainage at those locations.
7. Post construction monitoring: The Town will measure traffic volume at the CVS driveway six months, and again twelve months, after the opening of the CVS, and when school is in session, to compare with the analyzed volume data. Driveway traffic volumes will be recorded during the weekday AM (7-9 AM) and PM (4-6 PM) peak periods. Based upon this data, and the safety and performance of the area at least 6 months after opening, the Town will decide what, if any, mitigation is needed on roadways near the site. Possible mitigation may include addition of a left turn lane, or other measures, to improve safety and operations along Mass. Ave. between Carey Drive and the intersection of Jason and Mill Streets, at the Town's discretion. The funding for the mitigation shall be paid from the traffic mitigation escrow account referred to in Condition No. 8 below.
8. CVS will contribute the total sum of \$50,000 to a traffic mitigation escrow account, prior to receipt of an occupancy permit. These funds would first be used for mitigations around the site if it were determined during post-construction monitoring that further mitigation is needed. If it is not needed at the site, it may also be used as a contribution toward improvements at Jason/Mass/Mill Streets. Said escrow account will be closed, and unspent monies returned to CVS, five years after the date of the occupancy permit. All disbursements from the CVS escrow account will be subject to the approval of the ARB.
9. Prior to receiving a building permit, the applicant shall submit to the ARB for its review a plan for reduction of energy use, including use of energy-efficient lighting and appliances, to be incorporated into the plans and specifications.



10. Changes to signage, such as wording, color, or material of construction, but not changes in the number, location, or size of signs, may be deemed by the Planning Director to be consistent with the existing special permit, and such changes may be made by sign permit.

11. In accordance with Standard EDR-5, the applicant is required to post a bond in the amount of \$1,500 as security that the storm drain system will be maintained in good working order. The ARB may use the funds to conduct cleaning and maintenance of the system if the applicant fails to do so. Town personnel, or the Town's agents, may enter upon the property to perform such cleaning and maintenance.

12. This permit is contingent upon the applicant receiving an Order of Conditions from the Arlington Conservation Commission for the project essentially as approved by the Arlington Redevelopment Board.

13. The drive-thru pharmacy shall be open only between the hours of 8:00 AM and 10:00 PM, and only when the main store is open, and only pharmacy and pharmacy-related items (but not general merchandise) may be sold through the drive-thru window. Bicyclists will be allowed to use the drive-thru pharmacy, and "No Idling" signs will be posted for vehicles using the drive-thru. Pedestrian walkup business will not be allowed.

14. Aside from the shutters described in the approved plans, first floor windows shall not be covered or obscured in any way that prevents a clear view into the store, without the prior written permission of the ARB. No film, paper, or other material, including advertisements, may be used to cover any windows.

15. The applicant shall maintain a clean site at all times, and the landscaped area on the north side of the site, extending down the hill to the property below, shall be cleaned at least once in the spring and once in the fall. Litter and fallen branches and such shall be removed, and trees and shrubs shall be pruned as necessary.



## Town of Arlington, Massachusetts

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### ARB Committee Appointments for ARB Members and Designees

#### Summary:

9:15 p.m. *Board will review and approve ARB representation on various Town committees and vote to approve appointments of Board members and designees on various Town committees*

#### ATTACHMENTS:

	Type	File Name	Description
▢	Reference Material	Agenda_Item_2_-_ARB_Committee_appointments_2021.pdf	ARB Committee appointments 2021

Redevelopment Representatives on Committees/ Other Appointments

1. Envision Arlington Standing Committee— Alex Bagnall
2. Open Space Committee—Wendy Richter
3. Zoning Bylaw Working Group – Eugene Benson
4. Master Plan Implementation – Melisa Tintocalis
5. Housing Plan Implementation – Steve Revilak
6. Community Preservation Committee—Kin Lau
7. Remote Participation Study Committee – Rachel Zsemlery



## Town of Arlington, Massachusetts

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### Draft FY22 ARB Updated Goals

#### Summary:

9:20 p.m. *Board will review and approve goals*

#### ATTACHMENTS:

Type	File Name	Description
Reference Material	Agenda_Item_3_-_ARB_FY_22_Draft_Amended_Goals.pdf	ARB FY22 Draft Amended Goals





## ARLINGTON REDEVELOPMENT BOARD

TOWN HALL, 730 MASSACHUSETTS AVE., ARLINGTON, MASSACHUSETTS 02476

TELEPHONE 781-316-3090

### **ARLINGTON REDEVELOPMENT BOARD GOALS**

**September 2021 through June 2022**

#### **I. ZONING BYLAW AMENDMENTS**

##### **1) Advance Zoning Bylaw amendments to future Town Meetings**

- i. *Action – Encourage development and redevelopment opportunities to generate a full range of housing options for all incomes and housing types and encourage mixed-use development, and new commercial development*
  - a) *Complete and adopt Housing Production Plan update. Advance discussions about zoning amendments.*
  - b) *Action - Consider zoning amendments to encourage commercial development along the Massachusetts Ave and Broadway corridors.*
  - c) *Action – Review and amend zoning in Arlington Heights business districts to align with recommendations in Arlington Heights Neighborhood Action Plan as appropriate.*
- ii. *Action - Review Net Zero Action Plan zoning recommendations*
- iii. *Action - Review Connect Arlington (Long-Range Transportation Plan) zoning recommendations*
- iv. *Action – Review forthcoming stormwater management plan and consider potential zoning recommendations*

#### **II. LONG-RANGE PLANNING**

##### **1) Review progress on implementation of the Master Plan**

- i. *Action – Adopt formal amendments to Master Plan based upon recent completion of Connect Arlington and provide amended Master Plan to DHCD accordingly.*
- ii. *Action – Direct Master Plan Implementation Committee to provide ARB with an update on implementation status, status of all Working/ Study Groups, and make recommendations on modifications/ updates that might be needed*
- iii. *Action – Participate on Town Economic Development Working Group (to be formed) to advance economic development goals and objectives.*

##### **2) Ensure transparent, welcoming, and efficient permit review and delivery system**

- i. *Action – In alignment with Town effort to modernize permitting processes, convene representatives from the Select Board, Conservation Commission, Historical Commission, Historic Districts Commission, Zoning Board of Appeals, and Board of Health*

##### **3) Ensure that Master Plan economic development goals and recommendations are achieved in Arlington's business districts**

- i. *Action – DPCD Director will provide quarterly updates on progress meeting goals, including planning goals, business retention and attraction goals, and new mixed-used development*

##### **4) Request appropriation to hire consultant to update Arlington's Design Standards**

#### **III. URBAN RENEWAL PLANS AND POWERS**

##### **1) Consider transitioning management of ARB-managed properties back to the Town**

- i. *Action -Work with Town Counsel on filing appropriate warrant article to transition properties.*

##### **2) Utilize Urban Renewal Authority powers**

- i. *Action: Work with Town Counsel to explore urban renewal options in Arlington Center and at site-specific locations along main commercial corridors*
- ii. *Action: Provide updates to Board on Symmes Urban Renewal Plan and property when*

appropriate (Arlington 360 and Brightview and adjacent conservation properties)

#### **IV. SUPPORT COMMUNITY PLANNING GOALS**

- 1) Participate in range of Town committees and initiatives that advance community planning goals**
  - i. Action - Appoint/ re-appoint committee members serving on ARB committees and ARB designees to committees.
  - ii. Action - Collaborate with committee implementing Arlington Heights Action Plan
  - iii. *Action – Receive updates from ARB designees to Envision Arlington Standing Committee, Open Space Committee, Housing Plan Implementation Committee, Community Preservation Committee, Master Plan Implementation Committee, Remote Participation Committee, and others on an ongoing basis.*



## Town of Arlington, Massachusetts

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### Zoning Warrant Article filing process/ coordination with Redevelopment Board

#### Summary:

9:35 p.m. *Board will review their approved Warrant Article filing process discussed in February 2021 and update to prepare for the next Town Meeting*

#### ATTACHMENTS:

	Type	File Name	Description
▢	Reference Material	Agenda_Item_4_-_ARB_2021_Town_Meeting_Process.pdf	ARB 2021 Town Meeting Process



## TOWN OF ARLINGTON

DEPARTMENT OF PLANNING and  
COMMUNITY DEVELOPMENT

TOWN HALL, 730 MASSACHUSETTS AVENUE  
ARLINGTON, MASSACHUSETTS 02476  
TELEPHONE 781-316-3090

### Redevelopment Board Review Process and Schedule for 2021 Annual Town Meeting

Date	Process	Description and Action Items
January 29, 2021	Warrant Closes	<p>This is the final date for submission of articles to the Town Meeting warrant.</p> <p><b>ARB Action Item:</b> The ARB will submit any zoning articles to the warrant.</p> <p><b>Petitioner Action Item:</b> Members of the public will submit any zoning articles to the warrant. The ARB recommends that petitioners reach out to the Director and Assistant Director for Planning and Community Development to discuss their article and to learn more about the review timeline. The Planning and Community Development staff is available to provide technical assistance to petitioners throughout the process.</p> <p><b>General Public Action Item:</b> This is the due date for topics (articles on zoning or other topics) to be submitted to warrant for consideration by Town Meeting beginning on April 26, 2021.</p>
February 11, 2021	Legal Notice Published/ Zoning Warrant Articles Posted	<p>As required by M.G.L. Chapter 40A, The Zoning Act, a legal notice will be placed in a local newspaper that identifies the hearing dates, location (all virtual for Town Meeting 2021), and the topics. At the same time, a document will be published that compiles all of the zoning warrant articles and amendment text (known as a main motion) if available.</p> <p><b>ARB Action Item:</b> On behalf of the ARB, DPCD staff submits the legal notice and the compiled zoning warrant articles. Additionally, the DPCD staff will communicate the hearing schedule to all petitioners and other interested parties.</p> <p><b>Petitioner Action Item:</b> At this time, it is important for petitioners to be in touch with DPCD Director and Assistant Director to review the amendment text (main motion) and other relative resources and documents. The DPCD staff is available for technical assistance to any petitioner.</p>



Date	Process	Description and Action Items
		<p><b>General Public Action Item:</b> Once the legal notice is published, the hearing schedule will be published. If you are interested in any of the zoning topics, you can save the date to attend a virtual public hearing with the ARB.</p>
<p>March 1, 2021 March 15, 2021 March 29, 2021 April 5, 2021</p>	<p>ARB Public Hearings</p>	<p>M.G.L. Chapter 40A requires that the ARB hold public hearings to obtain feedback on all of the zoning warrant articles proceeding to Town Meeting.</p> <p><b>ARB Action Item:</b> The ARB will hold public hearings on most Mondays in March to obtain feedback. On behalf of the ARB, the DPCD Director and Assistant Director will communicate the hearing schedule and protocol to all petitioners and coordinate obtaining materials from petitioners for inclusion in the ARB’s meeting packet which is publically accessible.</p> <p><b>Petitioner Action Item:</b> At least a week in advance of a petitioners hearing date, petitioners should provide to the DPCD Director and Assistant Director your amendment text (main motion) and other relevant materials. The petitioner should prepare to make a short presentation (no more than 3-5 minutes) at the public hearing and answer questions from the ARB members regarding the petition.</p> <p><b>General Public Action Item:</b> All public hearings are open for attendance by the general public. You may join and provide feedback based on the information presented or just listen. Written comments are also welcome. All materials will be posted online at <a href="http://arlingtonma.gov/arb">arlingtonma.gov/arb</a>.</p>
<p>Late March/Early April (Actual Dates to be determined)</p>	<p>ARB Votes on Zoning Articles</p>	<p>M.G.L. Chapter 40A requires that the ARB vote on each article and prepare a report for Town Meeting. The ARB can vote to recommend action or recommend no action to Town Meeting. The Report to Town Meeting, which outlines the votes taken and why, is typically written, but can also be given verbally at Town Meeting.</p> <p><b>ARB Action Item:</b> After hearing from all petitioners and interested parties, the ARB will vote on each article and outline their reasons for each vote. The Report to Town Meeting will be finalized and voted on and submitted to Town Meeting Members and posted online for review.</p> <p><b>Petitioner Action Item:</b> Prior to the ARB’s vote, each Petitioner should work with the DPCD Director and Assistant Director to finalize their amendment text (main motion) for consideration by the ARB. Any other relevant information should be provided by the petitioner.</p> <p><b>General Public Action Item:</b> At this stage, all feedback has been obtained by the ARB either verbally or written, the public hearing has closed, and the ARB will vote on each article individually.</p>
<p>April 2021 (Actual Dates to</p>	<p>Public Information</p>	<p>Typically in April 2021, Arlington Town Meeting Members will hold precinct meetings. Additionally,</p>

Date	Process	Description and Action Items
be determined)	Sessions	<p>the ARB may hold public information sessions on the zoning warrant articles.</p> <p><b>ARB Action Item:</b> On behalf of the ARB, DPCD Staff and ARB members will hold virtual public information session(s) to provide an overview the zoning articles to be considered by Town Meeting. The public information session will include time for questions and answers.</p> <p><b>Petitioner Action Item:</b> Petitioners may want to attend the precinct meetings and ARB public information sessions to provide information about their zoning warrant articles.</p> <p><b>General Public Action Item:</b> Attendance at the precinct meetings and at public information sessions will provide a venue to seek additional information and to let your Town Meeting Members know your opinion on any article.</p>
April 26, 2021	Town Meeting Begins	<p>Town Meeting begins on April 26, 2021 and continues on Mondays and Wednesdays until completed. The format for the 2021 Annual Town Meeting will be virtual. The Town Meeting Members will vote on each article. In most cases, zoning articles require an affirmative vote of two-thirds majority of Town Meeting.</p> <p><b>ARB Action Item:</b> The ARB Chair will provide a presentation of the warrant article. DPCD staff may be on hand to provide additional information to respond to Town Meeting Member questions.</p> <p><b>Petitioner Action Item:</b> If the petitioner is not the ARB, the petitioner will be given a chance to present and answer any questions from Town Meeting Members.</p> <p><b>General Public Action Item:</b> The general public can watch the Town Meeting proceedings through ACMi.</p>
Post Town Meeting	Submission to the Attorney General	<p>Towns are required to submit to the Attorney General's Municipal Law Unit information about zoning articles, the process, and the votes taken. Unless otherwise described in the article, the effective date of a zoning amendment is the date Town Meeting voted.</p> <p><b>ARB Action Item:</b> On behalf of the ARB, the DPCD staff work with the Town Clerk and Town Counsel to prepare and submit the appropriate forms and update the Zoning Bylaw.</p> <p><b>There are no action items for Petitioners and the General Public.</b></p>

If you have any questions about this timeline and process, please contact Erin Zwirko, Assistant Director, Department of Planning and Community Development at 781-316-3091 or [ezwirko@town.arlington.ma.us](mailto:ezwirko@town.arlington.ma.us).



## Town of Arlington, Massachusetts

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### Meeting Minutes (8/30/21, 9/11/21)

#### Summary:

9:50 p.m. *Board will review and approve meeting minutes*

#### ATTACHMENTS:

Type	File Name	Description
▢ Reference Material	Agenda_Item_5A_-_08302021_Draft_ARB_Minutes.pdf	08302021 Draft Minutes Redevelopment Board
▢ Reference Material	Agenda_Item_5B_-_09112021_Draft_ARB_Minutes.pdf	09112021 Draft Minutes Redevelopment Board

**Arlington Redevelopment Board**  
**Monday, August 30, 2021, 7:30 PM**  
**Meeting Conducted Remotely via Zoom**  
**Meeting Minutes**

This meeting was recorded by ACMi.

**PRESENT:** Rachel Zsembery (Chair), Eugene Benson, Kin Lau, Melisa Tintocalis

**STAFF:** Jennifer Raitt, Director of Planning and Community Development and Kelly Lynema, Senior Planner

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The Chair called the meeting to order and notified all attending that the meeting is being recorded by ACMi.

The Chair explained that this meeting is being held remotely in accordance with the Governor's March 12, 2020 order suspending certain provisions of the Open Meeting Law G.L. c. 30A, Section 20. This order from Governor Baker allows for meetings to be held remotely during this time to avoid public gatherings.

The Chair introduced the first agenda item, Public Hearings, the first hearing is for 29 Mill Street Docket #3662. Mr. Jason Parillo from Bluebird Graphic solutions said that the requested sign is larger than that allowed by the Town sign bylaw. Mr. Parillo said that the size of the requested sign would fit best based on the size of the building and the size of the lot. Ms. Raitt said that the sign is larger than allowed but in this case this request should proceed. Mr. Benson asked about the disparity between the stated sign size between the application forms and the memo. Mr. Parillo said that the size description on the memo is correct. Mr. Parillo said that a smaller sign would make the type on the sign difficult to read. Mr. Benson said that he does not think that approving a sign larger than allowed by the sign bylaw guidelines would be in the public interest. The Chair asked about the bylaw sign measurement with regard to irregularly shaped signs. Ms. Tintocalis asked about the hours that the sign would be illuminated and asked if the sign will be lit using solar power. Mr. Parillo said that Great Sky Solar will be filing a request to install solar panels in the future. Ms. Raitt said that the sign size calculation, considering the irregular shape, would bring the sign into size compliance. The Chair asked to clarify how that the sign is non-compliant with the sign bylaws, Ms. Raitt confirmed the sign is still non-conforming due to the height and area of the signage.

The Chair opened the floor to public comment.

Chris Loreti stated that there is no sign on the building at this time and he does not believe that this sign should be illuminated. Mr. Loreti said that he does not believe that there are other illuminated signs in this area. Mr. Loreti said that the sign is too big, too high, and should only be illuminated during office hours.

With no other members of the public wishing to speak the Chair closed the floor to public comment.

Mr. Lau said he found no issues with this sign. Mr. Lau asked what hours the sign would be illuminated, Mr. Parillo said that they expect to have the sign lit until 8:00 p.m. or 9:00 p.m. in the evenings. Ms. Tintocalis said that she would like the operating hours for the illumined light included with the conditions and would like to see the sign powered by solar in the future. Ms. Raitt confirmed that the bylaw does include rules for the hours of illumination. Mr. Benson said he would like the sign size limited as a condition of the permit since there is a size discrepancy on the application.

Mr. Lau moved to approve Docket #3662 for signage on 29 Mill Street with the following conditions, Ms. Tintocalis seconded, approved 4-0.

Conditions:

No illumination between 9:00 p.m. to 6:00 a.m.

Confirm the approved square footage of the sign



The Chair introduced the next public hearing for Docket #3665, 645 Massachusetts Avenue. Mark Sides from Core States Group representing JP Morgan Chase gave an overview of the application. Ms. Raitt said that the applicant should comply with the bike parking bylaw and to make some changes to the façade and the signage. Ms. Raitt said that the changes being proposed would create challenges to accessibility. Ms. Raitt said that there are several banks and lending institutions in Arlington Center and it would be nice to have a restaurant replace another restaurant. Ms. Raitt asked the Board to consider the liveliness and activity of the streets in Arlington Center. Mr. Lau asked about long term bike parking if JP Morgan Chase will also be leasing the basement space. Mr. Lau said he is concerned about the type of windows shown in the plans for the Mass Ave. façade. Mr. Lau said that the planned blade sign does not fit the character of Mass Ave., he asked if a flat sign could be placed on the driveway side of the building. Mr. Lau asked if the ATM could be relocated to accommodate handicapped access. Mr. Sides said that a ramp will not fit in that space without impeding parking or the sidewalk. Mr. Benson said that he agrees with Mr. Lau regarding the signage and with Ms. Raitt regarding the bicycle parking. Mr. Benson said there has to be a way to make the back door ADA compliant. Mr. Benson said that he agrees with Ms. Raitt's concern of a bank taking the place of a restaurant. Ms. Tintocalis asked Mr. Sides about the decision to locate a bank of this size in Arlington Center. Mr. Sides said that Chase Bank is looking for representation within this community, can host community events, training sessions for the community, and in that respect Chase feels that they offer a unique perspective on banking that is not currently present in the community. Ms. Tintocalis asked how Chase determined they would like representation in the community. Ms. Tintocalis said that there seems to be a growing trend of banks locating in Arlington. Ms. Tintocalis said she is concerned about the excess in use with the aggregation of banks, especially along main streets in this location. The Chair asked Mr. Sides if the bank would consider adding awnings and street seating, which is regularly used by the public. Mr. Sides said Chase was looking to stand out and not use awnings like neighbors and competitors. Ms. Raitt said that the benches are the Town's and part of the right of way and Ms. Raitt asked if the existing street planters could be kept by Chase. The Chair opened the floor to public comment.

Chris Loreti said he appreciates the discussion regarding the number of banks in the area, Mr. Loreti said that this use is detrimental in this location. Mr. Loreti said that B5 zoning district is limited, there are certain uses only allowed in that district. Mr. Loreti suggested that the applicant consider another location outside the immediate town center.

Don Seltzer asked the applicant if this business is just a bank or does it have more of a function beyond that. The Chair said that the applicant already answered this question earlier. Mr. Seltzer asked if the bank would also be a financial center that will draw foot traffic and how many employees are expected to be working at this location.

Steve Revilak said that having this vacant space filled so quickly is a nice surprise. Mr. Revilak said the Arlington just seems to attract a lot of banks, restaurants, and preschools. Mr. Revilak said that the applicant should meet the bike parking requirements.

Colleen Cunningham said that this site is located on the bike path by a busy intersection with a lot of foot traffic. Ms. Cunningham said that this location is suited for a restaurant and not a national bank, next to another national bank.

With no other members of the public wishing to speak the Chair closed the floor to public comment and opened the floor to the Board for comment. Mr. Lau said that there are many empty storefronts on Mass. Ave. that used to be restaurants. Mr. Lau said that wishing this location to be a restaurant is unfair. Mr. Lau said that without businesses Arlington will become a bedroom community. Mr. Benson said that Arlington should welcome Chase but not in a location with so many other banks nearby. Mr. Benson said that a bank will not replicate the foot traffic or vibrancy that was there when it was a restaurant. Ms. Tintocalis said that she agrees with Mr. Benson regarding the change in use. The number of banks, the proximity to each other, linear square footage, and the total square footage. Ms. Tintocalis said that she does not believe that this use

supports the goals of the Master Plan to create a walkable town. Ms. Tintocalis said it is critical to understand what Arlington is looking for, what are our metrics for foot traffic and walkability. Ms. Tintocalis said that she does not support this change at this time. The Chair asked Mr. Sides asked if this location would be a financial center with hours outside of the typical banking hours. Mr. Sides said that this will be a retail banking center with relatively standard banking hours. The Chair said that she is aligned with Mr. Lau and is concerned about turning away what is considered a strong business that is amenable to investing in a building in the center of town. The Chair said she is also concerned about the number of financial institutions in this area. The Chair said that without clear guidance she does not want to make what would be an arbitrary decision based on what is considered to be too many financial institutions on the Mass. Ave. corridor. Mr. Benson asked the Staff to do some research to see if there have been studies in other areas regarding this issue and what the results were. Mr. Lau said that a bank would not open a new branch in an area that is already completely saturated. Mr. Lau said that competition will ultimately create better service for Town residents. Ms. Raitt reminded the Board of the full list of criteria for denying a permit, that use alone is not enough to deny a permit. Mr. Sides asked what would happen in the event of a deadlock since there are currently only four board members. The Chair said that the application would not be approved if there is a deadlock.

Mr. Benson moved to continue this hearing to the September 13, 2021 meeting, Ms. Tintocalis seconded, approved 4-0.

The Chair introduced the public hearing for Docket #3348, 833 Massachusetts Avenue. Ms. Raitt reviewed the Board's decision made in 2009 for CVS, which includes the neighboring property at 821 Mass. Ave., known as the Atwood House. Ms. Raitt said that the terms of the decision included that the Atwood House would be maintained, would not be moved, or have a demolition request submitted within 24 months of the issuance of the 2009 decision. Ms. Raitt said that it has been more than a decade and the Atwood House is now in very bad condition. Ms. Raitt reviewed the memo sent in 2020 to the property owner, Mr. Noyes, listing the Board's requirements. Ms. Raitt said that the applicant was working to file a demolition order but proceeded to start demolition with asbestos remediation, including the exterior siding, before obtaining a building permit. Ms. Raitt said she reported that work had begun without a permit and the Town was able to file a stop work order. Ms. Raitt said that Mr. Noyes was sent a memo listing the bylaw violations and fines. Ms. Raitt said that the applicant then attended the Historical Commission's hearing and the continuation of that hearing is scheduled for the Historical Commission's September 7, 2021 meeting. Ms. Raitt said that the Board should have a conversation regarding what the Board's expectations and discuss what should happen next. Ms. Raitt said that Bob Annese, the property owner Mr. Noyes, the Attorney representing CVS, and Joanne Preston, the Chair of the Historical Commission, are all on the Zoom call this evening to discuss this property with the Board. Mr. Lau said he would like to hear what happened from ownership and what they plan to do with the building. Mr. Annese said there was a miscommunication with Mr. Noyes' employee regarding pursuing the demolition application. Mr. Annese that Mr. Noyes followed the Historical Commission's request to wrap the building and to install a security system. Mr. Annese said that restoring the exterior of the building could cost up to \$150,000.00. Mr. Annese said that Mr. Noyes will continue to pursue the demolition application and would like to come back before the Board with a new proposal that includes demolition of the Atwood House. Ms. Raitt said it is up to the Historical Commission to determine what the next steps should be. Mr. Lau asked if the Board had other options to create some affordable housing instead of waste the \$150,000.00 for a restoration that will be demolished. Ms. Raitt said that Mr. Noyes has several options including donating the property to the Arlington Affordable Housing Trust Fund but they have not disclosed what they would like to do with the property. Mr. Benson asked what the Board has the ability to do since the special permit terms from the 2009 decision were violated and continue to be violated. Ms. Raitt said that the more extreme action could be to revoke the special permit or there may be opportunity for other discussions. Ms. Raitt said that the current condition erodes the long term lease agreement with neighboring CVS and Arlington does not want to discourage keeping and attracting large retailers. Ms. Raitt said that the responsibility for something to happen with that property lies with the Board. Ms. Raitt said that the Board has to make a decision from a spectrum of options: doing nothing, keep having discussions with the property owner, or revoking the special permit. Mr. Annese suggested proposing a new development plan after the hearing with the Historical Commission. Mr. Benson said that he would like to have Mr. Noyes and Mr. Annese appear once a month to update

the Board. Mr. Benson said he would like to see a proposal that keeps the original façade. The Chair said she would like to discuss the timing of permits and the violation of public trust. The Chair said that this has gone on so long and does not want a vacant lot left after the demolition permit has been issued. Ms. Raitt said that there is a demolition delay that will be determined by the Historical Commission that can be up to two years. Mr. Robinson said that the first order of business for the Historical Commission was to restore the exterior of the building. Ms. Raitt notified the Board that Mary Winstanley O'Connor, who represented CVS, is also present. Ms. Tintocalis asked if the Board can question the property owner to find out what his goal is for the building. Mr. Noyes said he would like to develop the space as a mixed use building with residential units in the back. Ms. Tintocalis asked what the implications are if the special permit is revoked. Ms. Raitt said that revoking the permit would be a more challenging pathway to choose and would impact future use. Ms. Raitt said that the bottom line is that the Town would like something done with the property. Ms. Winstanley O'Connor said that CVS is also deeply concerned about the condition of the property. Ms. Winstanley O'Connor said that CVS is putting the landlord on notice that it is reserving its rights and will perhaps take action if something is not done. Mr. Lau said he would also like to see Mr. Noyes monthly for an update.

The Chair opened the floor to public comment.

John Worden said the Noyes family let the Atwood House fall into disrepair so the cost of repairing the exterior is Mr. Noyes' responsibility. Mr. Worden said that the 2019 proposal to move the house forward was worthwhile to save the house.

Steve Revilak asked if the Board, as a redevelopment authority, could determine that this site is blighted and implement an urban renewal plan to remediate it.

Don Seltzer said that one of the provisions of the 2009 Special Permit ruling there was supposed to be some landscaping in front of CVS that is still not there.

With no other members of the public wishing to speak the Chair closed the floor to public comment. The Chair agreed that a monthly check in with Mr. Noyes would assist with next steps and to move this process along.

Mr. Lau moved to continue this hearing for Docket #3348, 833 Massachusetts Avenue to September 27, 2021, Mr. Benson seconded, approved 4-0.

The Chair introduced the second agenda item, ARB Committee Appointments for ARB Members and Designees. The Chair suggested meeting the Standing Committee nominee, who is present, the other appointment introductions will be rescheduled. Ms. Raitt introduced Jagat Adhiya who said it is a pleasure to represent Envision Arlington. Mr. Adhiya said Arlington is changing so it is great to have this opportunity to build up this community. Mr. Adhiya asked what the Board's expectations are for Envision Arlington. Mr. Benson said that Envision Arlington brings a lot of new voices to table, the subcommittees can do important work, and people can choose what they think are the right ways to move the town ahead. The Chair said that there are some interesting plans and studies being conducted and The Chair said she looks forward to how the recommendations are integrated into our community. Mr. Lau said that it can encourage diversity, growth, and the ability to reach out to other people to get diverse opinions.

Mr. Lau moved to approve Mr. Adhiya's nomination to the Envision Arlington Standing Committee, Mr. Benson seconded, approved 4-0.

The Chair introduced the third agenda item, Housing Plan, and Open Space and Recreation Plan updates. Ms. Raitt said she provided a memo and will be happy to answer any questions. Mr. Benson asked if the Housing Production Plan will include a proposal to increase inclusionary zoning. Ms. Raitt said that that should be included with the next phase of the plan.

The Chair introduced the fourth agenda item, Business development update and opportunities. Ms. Raitt put this item on the agenda at Mr. Lau's request. Ms. Raitt said that she would like to collect questions to discuss at the Board's retreat. Mr. Lau asked for a review of the vacant storefronts. Mr. Lau said he would like to hear from commercial brokers to get an overall sense why

the spaces are not being leased so the Board can make any necessary changes. Ms. Raitt said that Ali Carter, Economic Development Coordinator, is preparing to present to the Select Board in October.

The Chair introduced the fifth agenda item, Meeting Minutes.

Mr. Benson moved to accept the June 7, 2021 meeting minutes as submitted, Mr. Lau seconded, approved 4-0.

Mr. Lau moved to accept the June 21, 2021 meeting minutes as submitted, Ms. Tintocalis seconded, approved 4-0.

The Chair opened the floor for the Open Forum portion of the meeting. With no members of the public in queue waiting to speak the Chair closed the Open Forum portion of the meeting.

Mr. Lau moved to adjourn, Ms. Tintocalis seconded, approved 4-0.

Meeting adjourned.

DRAFT



**Arlington Redevelopment Board**  
**September 11, 2021, 8:00 AM**  
**Arlington Police Department, Community Room**  
**Meeting Minutes**

**PRESENT:** Rachel Zsembery (Chair), Eugene Benson, Kin Lau, Melisa Tintocalis, Steve Revilak

**STAFF:** Jennifer Raitt, Director of Planning and Community Development

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The Chair called the meeting to order.

Raitt welcomed Revilak, the Board's new gubernatorial designee. Raitt reviewed first agenda item and discussed Department of Planning and Community Development objectives for this fiscal year as well as accomplishments from last fiscal year. She reviewed the Town's Annual Budget and Financial Plan documents related to the Department and Redevelopment Board. The Board is interested in weighing in on a plan in progress, especially when zoning recommendations are discussed. The Board requested that Raitt add Net Zero Action Plan and Connect Arlington to a future ARB agenda for the Board to endorse the Net Zero Action Plan and to adopt the Connect Arlington plan. The Board also requested that Raitt add Working Group reports to future meeting agendas. They also suggested creating a calendar with specific times for updates. Benson requested a focus on the inclusionary zoning study. Raitt explained that it would not be prudent to hire a consultant at this time and wondered if the Town would learn anything new about the bylaw versus the need to produce more housing to trigger any inclusionary requirement. Raitt suggested that there are amendments that the Board could make to the bylaw that would not require a consultant, including amending the payment in lieu of housing section and the related calculation for payment to the Town. Lau agreed. The Board agreed not to move forward with hiring a consultant at this time.

Lau introduced the importance of focusing on economic development and zoning that can help address commercial uses along the Mass Ave and Broadway corridors. He suggested focusing on amendments to the bylaw to increase floor-area-ratios (FAR) in business districts. Other suggestions included amending parking requirements for residential uses, a bylaw amendment to help create starter homes, and an update to existing design standards. The Board agreed that it was important to pursue an appropriation at next Town Meeting to update the Arlington Design Standards relative to commercial areas. The new industrial zoning incorporated design standards into the bylaw. The Board also agreed to focus on Housing Plan goals and implementation of strategies related to zoning once the plan is complete. Raitt outlined a Town-led application modernization project, which will move the Town to a completely technology-based application format for all Town boards and commissions engaged in project/ application reviews and approvals. Revilak agreed to serve as a rep from the Board on this project.

Given the Board's interest in economic development issues, Raitt mentioned that a new working group will be formed to focus on economic development. The group would include representatives from many groups working on economic development efforts across town. The working group will not be solely focused on zoning, but on multiple issues facing the business districts. The Board discussed several other potential zoning recommendations for the commercial areas, including establishing a minimum percentage of commercial in mixed-use projects and addressing setbacks. There may also be opportunities to amend the zoning map and reduce the number of business districts. This zoning work may occur because of the MBTA Communities planning work. However, municipalities across the state are awaiting guidance from the MBTA and DHCD regarding the substantive changes that will be required to be compliant as an MBTA community. The timeline for releasing that guidance is unclear. Other potential zoning amendments included a suggestion from a member of the Zoning Bylaw Working Group regarding addressing buildings in floodplains by allowing for taller buildings to elevate basements.

Revilak suggested that this and other resiliency zoning efforts are important for the Board to consider.

For a future meeting, the Board would like to have a discussion with Town Counsel regarding their statutory powers and authority around MGL 40A and MGL 121B. At this meeting, it would also be helpful to discuss urban renewal powers in relation to moving forward with redevelopment proposals in the town.

Additionally, the Board would like to discuss the Town Meeting warrant article process at the next meeting. The Board will also make committee appointments at the next meeting and devise a calendar for approximate times when Board members will be required to provide regular reports from committee work. Lau also requested that additional time be added to future public hearings for special permit applications under consideration. He often feels that there is not enough time provided for a full discussion. The Board will also discuss an update to their goals for this current fiscal year.

Mr. Lau moved to adjourn, Ms. Tintocalis seconded, all voted in favor (5-0).

Meeting adjourned at 11:00 a.m.



## Town of Arlington, Massachusetts

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### Correspondence Received:

#### Summary:

S. Mraz 09082021  
S. Tuttle 09122021  
M. Guyton 09142021  
D. Seltzer 09212021  
J. Spiller 09232021

#### ATTACHMENTS:

	Type	File Name	Description
▢	Reference Material	Correspondence_received_from_S._Mraz__09-08-2021.pdf	Correspondence from S. Mraz received 09082021
▢	Reference Material	Correspondence_received_from__S._Tuttle_09-12-2021.pdf	Correspondence from S. Tuttle 09122021
▢	Reference Material	Correspondence_received_from_M._Guyton_09-14-2021.pdf	Correspondence from M. Guyton 09142021
▢	Reference Material	Correspondence_received_from_D._Seltzer_09212021.pdf	Correspondence from D. Seltzer received 09212021
▢	Reference Material	Correspondence_received_from_J._Spiller_9-23-2021.pdf	Correspondence from J. Spiller 09232021

----- Forwarded message -----

From: **Sarah Mraz** <[sarahmraz@gmail.com](mailto:sarahmraz@gmail.com)>

Date: Wed, Sep 8, 2021 at 5:30 PM

Subject: Chase Bank proposal objection

To: <[acarter@town.arlington.ma.us](mailto:acarter@town.arlington.ma.us)>

Dear Ali,

I am one of many residents deeply disappointed to learn that the beautiful spot formerly occupied by Not Your Average Joe's in Arlington Center is soon to become yet another bank. A big corporate bank- Chase. Bank of America is already in the same building. Furthermore a quick google search tells me there are already 29 physical banking locations (branches and/or ATM) in Arlington for a population of less than 50k in 2021, an online banking age.

Can't anything be done by town officials and the property owner to work together to select/attract a tenant that will contribute to the vibrancy of our community, especially in that key area?

The RDB hearing for this proposal is on 9/27 and I understand they will have very little power to oppose it.

I look forward to hearing from you on what to do next to advocate this tenant not move forward and other solutions be explored that will benefit the community and still provide the owner with rental income.

I understand Lexington changed its zoning laws in 2016 to limit banks and the like in their downtown areas. Is that being considered in Arlington?

Thank you,  
Sarah Mraz  
Chandler Street



From: Sarah Tuttle <s\_g\_tuttle@yahoo.com>  
To: "jraitt@town.arlington.ma.us" <jraitt@town.arlington.ma.us>  
Date: Sun, 12 Sep 2021 00:17:51 +0000 (UTC)  
Subject: Comment on Proposed Chase Branch in Arlington Center

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To Whom It May Concern on the Redevelopment Board,

I am writing as a resident of Arlington regarding the proposed branch of Chase Bank where Not Your Average Joe's used to be on MA Ave in Arlington Center. I sincerely hope that the location does not become a bank of any sort. To be frank-- in Arlington we have so many banks. I am very, very tired of seeing so many store fronts taken up with financial institutions. The location of the property is one of the best in Arlington-- right across from the library, in the heart of the center, with a huge space. It should be used for something that the community can engage with-- not yet another bank. There is already a large Citizen's Bank on one side of the block, and a large Bank of America property on the other side. Yet another bank in such a prime real estate location would be a waste of an opportunity to invite a business with real value to residents. We need restaurants, shops, and other businesses that invite folks to linger in the center. Businesses that the community wants to patronize. Locations that are fun to browse, or comfortable to gather. We don't need yet another reason to drive by our most accessible, valuable commercial real estate.

Thank you for taking these comments into consideration.

Sincerely,  
Sarah Tuttle

Resident-- Grove St. Arlington

**From:** "Matthew C. Guyton" <mcguyton@mit.edu>  
**Date:** September 14, 2021 at 1:51:43 PM EDT  
**To:** JRaitt@town.arlington.ma.us, ebenson@town.arlington.ma.us, KLau@town.arlington.ma.us, mtintocalis@town.arlington.ma.us, rzsembery@town.arlington.ma.us  
**Cc:** srevilak@town.arlington.ma.us  
**Subject:** Chase Bank - please deny

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Dear Redevelopment Board,

I am writing to ask you to deny the special permit for Chase Bank to replace Not Your Average Joe's.

As a frequent pedestrian in Arlington Center (especially around mealtimes), Not Your Average Joe's was what I consider an "anchor restaurant". You could walk out to the food destination of Arlington Center with the intention of patronizing one of the smaller restaurants, and if those restaurants were too busy, your backup plan could be to eat at Not Your Average Joe's, and vice versa. In this manner, the large restaurant helped drive business for the smaller restaurants. For this reason, I expect that if a restaurant does not soon replace NYAJ's, then Pasha and Thai Moon will soon go out of business. I realize that denying Chase's special permit won't bring NYAJ's back, but it will give up the opportunity for a new restaurant to open in this location before giving it a chance (and before the pandemic ends).

Frequently on our walks around the Center, my wife and I would complain about the prevalence of banks and how they ruin the walkable character of the town. The corner of Mass Ave and Pleasant has a plethora of financial institutions (listed going counterclockwise): Cambridge Bank, Coldwell Banker (not a bank, but sounds like one), Chase Bank requesting to go where NYAJ used to be, and Bank of America - leaving no good reason for pedestrians to walk to that part of the Center. Walking West up Mass Ave, the Citizen's Bank drive-thru exit creates a good place to get hit by a car, which makes me cautious when out with my young son. Walking further west up Mass Ave, the cluster of Brookline Bank, Leader Bank's lending center, and TD Bank creates another dead zone for pedestrians (made even worse by Lender Bank's large empty parking lot and crumbling brick planters that have sat unfixed for months). Keeping the Center a walkable area for pedestrians is important for businesses, because all customers are pedestrians. Banks violate this goal and are a poor use of space in the center of town.

Seeing a physical bank is like seeing a piece of history (and not in the good historical way that we strive for in Arlington). Does anyone still use physical bank locations? I have bought a house, refinanced it many times, and bought a car, all without ever visiting a physical bank location. I can't remember the last time I've physically been to a bank, and the last time I went, I was just there for a lollipop. I don't see how these locations are profitable for the banks, except that they serve as expensive billboards which would otherwise not be allowed in Arlington.

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Banks are ghost towns in the evening and make the area less walkable. I believe you should be trying to drive most of the banks out of the Center, not letting more banks in. If you let Chase bank replace NYAJ's, then you have essentially given up on having a vibrant welcoming town center in Arlington. (For the record, I'm not opposed to having Chase bank or any other bank in town - I just don't want to see them located at the main corner of Arlington Center).

-Matt Guyton

22 Irving St

From: Don Seltzer <timoneer@gmail.com>

To: Rachel Zsembery <rzsembery@town.arlington.ma.us>, KLau@town.arlington.ma.us, Eugene Benson <EBenson@town.arlington.ma.us>, MTIntocalis@town.arlington.ma.us, Steve Revilak <steve@srevilak.net>, Jenny Raitt <jrait@town.arlington.ma.us>

Date: Tue, 21 Sep 2021 11:08:47 -0400

Subject: Comments for Docket 3673 - 455 Mass Ave

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### **Critique of Proposal for Docket 3673 - 455 Mass Ave**

It is encouraging to see a mixed use proposal that attempts to retain a significant ground floor commercial component, particularly in the very heart of Arlington's largest business district. There are some problems however with this initial proposal, but they appear to be solvable with reasonable modifications to the plans.

I offer the Board these comments for your consideration.

#### **Existing Building**

The existing building is on a lot of 18,929 sf at the very heart of Arlington's largest business district. The first floor has eleven businesses in an area of 13,483 sf, all of it commercial. The second floor has 3,900 sf of office space.

The largest ground floor space is 3,269 sf for an eat-in restaurant (formerly Papa Gino's). The other large business is the Leader Bank branch office. There are four small eateries, primarily for takeout service. There are also three retail spaces and two salon businesses.

There are loading docks and driveway access to the rear of these businesses.

The second floor has multiple office spaces occupied by the owner of the property, as well as several tenants. It does not appear that any of these office businesses are of the type that would create foot traffic in the Center.

#### **Proposed Mixed Use Building**

The submitted plans show ten commercial spaces on the ground floor, plus a large lobby and bike storage area for the residential use. This is a 28% reduction in commercial floor area, from 13,483 sf to 9,711 sf. Much of it comes at the expense of the large eat-in restaurant space, which is being cut in half. Some of it is attributable to the residential lobby which replaces one of the

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existing retail spaces.

An option worth considering is shifting the residential lobby and entrance from the Mass Ave side to one of the Medford St storefronts, freeing up the Mass Ave space for commercial use.

The second floor replaces the existing office areas with 12 one bedroom apartments and a single studio. Two of the apartments are designated as affordable. The second floor is approximately 12,330 sf. There is an additional 1,128 sf of residential lobby and bike area on the first floor, for a total of 13,458 sf of residential gross floor area.

### **Calculation of Floor Area**

The application package is vague about how certain floor areas are calculated and provides some inconsistent figures. Most problematic is the second floor area. It can be inferred from some of the figures provided for open space that 10,211 sf is being referenced. This number is apparently obtained by simply summing up the interior floor spaces of the thirteen apartments, ignoring all hallways, stairwells, etc.

This is the third consecutive project in which the applicant has made the same miscalculation. Gross Floor Area is not the interior dimensions of rooms, but is instead defined in the ZBL as

*Gross Floor Area: The sum of the horizontal areas of all stories of a building or buildings on a lot, measured from the exterior faces of exterior walls...*

The actual second floor area is about 12,330 sf, to which must be added the 1,128 sf of the first floor lobby to obtain total residential gross floor area.

Without a detailed accounting of all of the floor areas it is difficult to determine exactly what the proposed FAR is for this project. It may be as high as 1.67.

### **Landscaping**

Most of the proposed landscaped open space is to be located in the two alley ways flanking the parking garage. They are each about 11'-12' wide and about 70' long, running from SW to NE.

The first 15' of these alley ways will be in total shade throughout the year.

Halfway down the alley, there will be a brief bit of afternoon sunlight each day for six months of the year, and complete shade for the other six months.

At the far end of the alley near the access driveway, the landscaped strip will receive about 36 minutes of mid afternoon sunlight per day for eight months of the year, from late February until late October.

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The proposed Wintergreen Arborvitae, Shadblow Serviceberry, Ivory Halo Dogwood, and Green Mountain Boxwood do best in full sun to partial shade. They will likely fare poorly in this location.

The June Hosta, however, does thrive in full shade.

It is recommended that only hardy, shade tolerant shrubs and trees be chosen for these areas.

### Usable Open Space

The application states that *“it would be the Petitioner’s position that the existing building is non-conforming with respect to the usable open space requirement contained in the zoning bylaw and the proposed changes to the building do not increase the non-conformity...”*.

The ZBL only requires usable open space as a percentage of the Residential floor area. The existing building has no residential component, so is currently conforming to this requirement. The proposed changes however would add more than 13,000 sf of residential use, requiring the creation of 2,600 sf of usable open space.

The applicant further claims “there is no ability to create usable open space.” That is also incorrect. The ZBL 5.3.18 specifically allows this Board to approve of balcony and roof spaces not more than 10’ above the lowest story used for dwelling purposes, to count toward usable open space. The current plans already include such a roof area, serving apartments A4 and A6. There are other potentially usable roof areas that can also serve this purpose as common open space areas for all of the residents.

### Logistics

Businesses typically need regular truck deliveries. The businesses at this location currently have good access. The rear entrances are all accessible to vehicles driving right up to their rear doors and there are two loading docks.

This access is sharply curtailed in the submitted plans. The vehicle access is replaced by narrow alley ways impassable to any vehicles. There is no mention of the required loading zone. It is possible that this can be provided in the garage but there is no mention of it in the plans.

There is also the question of garbage disposal for the ten businesses and the thirteen apartments. The dumpster shown in the plans may not have the capacity for all of these users. There should also be a provision for the collection of recyclables.

The parking in the garage is particularly tight, with the drive aisle four feet narrower than required by the ZBL and most design guidelines. This does not impact the general public; only the apartment residents will be inconvenienced.

This is potentially one of the better proposals for mixed use since the Broadway HCA project. There are a number of problems such as those outlined above, but none of these are insurmountable, and can be corrected with some compromise and redesign.

Don Seltzer



From: Jonathans064 <jonathans064@gmail.com>

To: Jenny Raitt <JRaitt@town.arlington.ma.us>

Date: Thu, 23 Sep 2021 14:40:36 -0400

Subject: Re: 645 Massachusetts Avenue Arlington , please don't let another bank move to Arlington

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Hi ok if I email you more comments ? it actually is hard for me to do public speaking on Zoom,

1.

645 Massachusetts Avenue Arlington, wish could be another restaurant or store, not a bank, too many banks in Arlington and read application that Chase Bank wants to expand farther then the zoning requires.

2. 2-14 Medford Street Arlington, don't want a tall building to be built there, what about the small businesses that are located on that block? Gail Ann's Coffee Shop been there a long time, There are too many tall high rises in Arlington, Arlington is a town, don't want it to be a city. All ready is two hotels in Arlington, one in the Heights, and one next to Menotomy Grille. Already will be a tall building across from Stop and Shop, retail and Apartments.  
Jonathan Spiller

Kindness makes the world a better place!

From: Jonathans064 <jonathans064@gmail.com>

To: jraitt@town.arlington.ma.us

Date: Wed, 8 Sep 2021 13:23:58 -0400

Subject: 645 Massachusetts Avenue Arlington , please don't let another bank move to Arlington

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Hi is too late to give feedback? please don't let another bank move in to Arlington, too ma h banks, bank on every corner, Leader Bank, Bank Of America, Citizens Bank, Cambridge Savings Bank.  
Another restaurant should move in there os bar and kitchen in space,  
where Not Your Average Joe's used to be,

[https://www.arlingtonma.gov/Home/Components/News/News/11402/16?cftype=News&fbclid=IwAR1H9CF7S2\\_gHZQujAI80iCtbSfaG9CfPw7JjH2nQ\\_qPXxONXZ2ZMJC8sGw](https://www.arlingtonma.gov/Home/Components/News/News/11402/16?cftype=News&fbclid=IwAR1H9CF7S2_gHZQujAI80iCtbSfaG9CfPw7JjH2nQ_qPXxONXZ2ZMJC8sGw)

Jonathan Spiller

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Kindness makes the world a better place!

| | |